

FEBRUARY 11, 2025

## Conditional use permit and modification of Miami-Dade County Resolution

REQUESTING APPROVAL OF A CONDITIONAL USE PERMIT AND MODIFICATION OF MIAMI-DADE COUNTY RESOLUTION 4-ZAB-68-79 AS LATER MODIFIED BY RESOLUTION Z-114-86 TO PERMIT AN INCREASE IN STUDENT ENROLLMENT AND OCCUPANCY FROM 325 STUDENTS TO 600 STUDENTS; TO PERMIT AN INCREASE IN HOURS OF OPERATION FROM 8:00 A.M. TO 4:30 P.M., TO 7:15 A.M. TO 6:00 P.M.; TO DELETE THE CONDITION REQUIRING DRIVEWAY ACCESS FROM SW 77 AVENUE TO BE CLOSED; TO DELETE THE LIMITATION ON THE NUMBER OF CLASSROOMS; TO DELETE THE LIMITATION ON THE NUMBER OF TEACHERS AND ADMINISTRATIVE PERSONNEL; TO ADD A NEW CONDITION ALLOWING THE REQUESTED INCREASE IN ENROLLMENT TO BE PHASED IN OVER THE COURSE OF 4 YEARS; AND TO ADD A NEW CONDITION ESTABLISHING PREFERENCE FOR ADMISSIONS FOR STUDENTS WHO LIVE WITHIN THE VILLAGE OF PINECREST.

OWNERS/APPLICANT(S):

Bet Shira Congregation, Inc.  
(Owner) and True North  
Classical Academy (Applicant)

LOCATION:

7500 SW 120 STREET,  
PINECREST, FL 33156,



PINECREST

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## PINECREST

Building and Planning Department

DATE: February 4, 2025

TO: Yocelyn Galiano, ICMA-CM  
Village Manager

FROM: Stephen R. Olmsted, AICP, LEED-GA  
Planning Director

RE: True North Classical Academy – Request to increase Student Enrollment from 325 Students to 600 Students; Conditional use permit and modification of Miami-Dade County Resolution 4-ZAB-68-79 as later modified by Resolution Z-114-86.

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### PETITION REQUEST/BACKGROUND

Bet Shira Congregation, Inc. (Owner) and True North Classical Academy (Applicant) are requesting approval of a conditional use permit and modification of Miami-Dade County Resolution 4-ZAB-68-79 as later modified by Resolution Z-114-86 to permit an increase in student enrollment and occupancy from 325 students to 600 students; to permit an increase in hours of operation from 8:00 a.m. to 4:30 p.m., to 7:15 a.m. to 6:00 p.m.; to delete the condition requiring driveway access from SW 77 Avenue to be closed; to delete the limitation on the number of classrooms; to delete the limitation on the number of teachers and administrative personnel; to add a new condition allowing the requested increase in enrollment to be phased in over the course of 4 years; and to add a new condition establishing preference for admissions for students who live within the Village of Pinecrest.

The applicant also seeks to change the number of school grades from kindergarten to 6<sup>th</sup> grade to kindergarten to 8<sup>th</sup> grade. However, this increase in the number of grades was approved by the Miami-Dade County Zoning Appeals Board by Resolution 4-ZAB131-80.

Resolution Z-114-86, Condition 2. Requires "That in the approval of the site plan, the same be substantially in accordance with that submitted for the hearing entitled "Bet Shira Congregation, as prepared by Zyscovich & Grafton Architects, and dated 10-3-85". The applicant is requesting that this be modified to reference the most recent site plan submitted

with this application. Staff recommends that the most recent site plan, if approved by the Village Council, be referenced in new condition 18.

Miami-Dade County Resolution Z-114-86 approved a variance for 158 parking spaces instead of 1,556 as otherwise required. The submitted plan includes parking along the southern portion of the property with an overall total of 197 parking spaces.

Additionally, since the initial application was submitted, the latest proposed plan limits ingress and egress from SW 120 Street, and so deletion of the condition requiring driveway access to SW 77 Avenue be closed may remain unchanged. The applicant proposes to modify Resolution Z-114-86 Conditions as follows:

1. That a detailed plot use plan be submitted to and meet with the approval of the Planning Director, said plan shall be drawn to scale and shall include dimensions to indicate lot size and location of building or buildings; said plan shall include among other things, but not limited thereto, off-street parking areas and driveways, walls, hedges, and fences, landscaping, etc. The plot plan shall include a title block identifying the project, the name and title of the person preparing the plan, the date of preparation of the plan, and the scale of the drawing.
2. That the proposed structure, or the addition to the existing structure, be of a residential type and character and meet with the approval of the Planning Director.
3. That the driveway to the existing residence for the day nursery on SW 77 Avenue be closed and that the area be appropriately landscaped.
4. That the use be made to conform to the requirements and/or recommendations of the Miami-Dade County Fire Chief and Miami-Dade County Department of Public Health and State Welfare Department.
5. That the use shall be established and maintained in accordance with the approved plan.
6. That the use shall be restricted to a maximum of ~~325~~ **600** children.
7. That the hours of operation shall be from ~~8:00 a.m. to 4:30 p.m.~~ **7:15 a.m. to 6:00 p.m.**
8. That a play area, if any, shall be enclosed with a fence of a type and at a location to be approved by the Planning Director.
9. That the number of grades will be from kindergarten to eighth grade.
- ~~10. That the maximum number of classrooms be 19.~~
- ~~11. That the maximum number of teachers will be 35. That the maximum number of administrative and clerical personnel (other than teachers) shall be 5.~~
12. That no new or additional signs shall be permitted.
13. That no transportation vehicles shall be stored on the premises at any time.

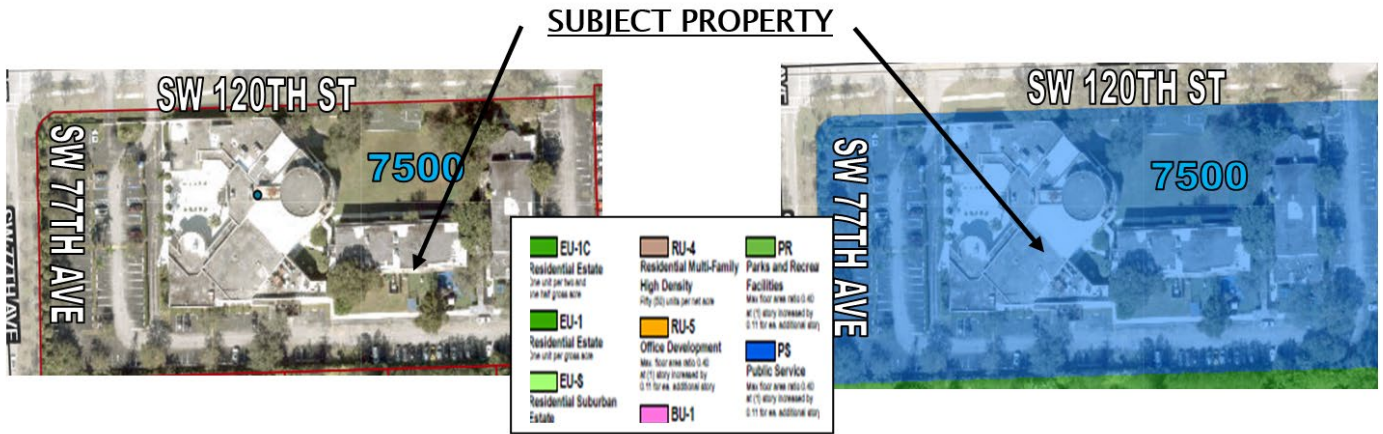
14. That the use shall be approved for pre-school, day nursery, kindergarten through 8<sup>th</sup> grade school and after school care.
15. That the Certificate of Use and Occupancy be automatically renewable annually by the Pinecrest Building and Planning Department upon compliance with all terms and conditions, and be subject to cancellation upon violation of any of the conditions or when, in the opinion of the Village Council, after public hearing, it is determined that the use is detrimental and/or incompatible to the surrounding neighborhood.
16. Increases in student enrollment shall be phased in over the course of time as follows:  
School Year 25-26 – 400 Students Maximum  
School Year 26-27 – 460 Students Maximum  
School Year 27-28 – 500 Students Maximum  
School Year 28-29 – 600 Students Maximum (Subject to administrative review of updated traffic study).
17. Preference for admissions shall be for students who live within the geographic boundaries of the Village of Pinecrest.
18. That in the approval of the site plan, the same be substantially in accordance with that submitted for the hearing entitled “True North Classical Academy Pinecrest Campus”, as prepared by CIVICA Architecture, and dated 11-08-2024.

The requested maximum number of students (600) includes the children attending pre-school activities. The applicant states that the number of children attending the pre-school shall never exceed 100 children.

The applicant proposes to remodel interior space within the existing one-story social hall and classroom building to add six (6) new classrooms. No new buildings are otherwise proposed to be constructed with this application. A copy of the applicant’s application and letter of intent are attached.

### **PROPERTY DESCRIPTION/HISTORY**

The subject property, True North Academy and Temple Bet Shira, is located at 7500 SW 20 Street, Pinecrest, FL 33156, within the PS, Public Service Zoning District. Temple Bet Shira is designed and built as a Temple and academic campus that includes a Temple, various classrooms, administrative offices, a basketball court and play areas, and well-established landscaping consisting of a variety of mature trees and shrubs throughout the campus. The total Temple Bet Shira property is 4.98 acres in area.



A “religious preschool” with a maximum enrollment of 300 children was initially approved as an unusual use by the Miami-Dade County Zoning Appeals Board in 1964. The school was subsequently expanded through approval of a “Special Exception” (conditional use) in 1979 allowing for a maximum of 325 students in grades K through 6. An increase in the number of grades allowing for grades K through 8 was approved in 1980 by the Miami-Dade County Resolution 4-ZAB-31-80.

### **OWNER/APPLICANT**

Bet Shira Congregation, Inc. (Owner); True North Classical Academy (Applicant)

### **PUBLIC COMMENT**

The Building and Planning Department has received one letter in opposition to the proposed increase in student enrollment. A copy of the email correspondence is attached. Additionally, an attorney representing a homeowner who lives immediately south of the property has indicated that his client is concerned about the noise that may be generated by the proposed increase.

### **COMPREHENSIVE DEVELOPMENT MASTER PLAN/LAND DEVELOPMENT REGULATIONS**

The subject property is designated as PS, Public and Semi-Public Services on the Future Land Use Map of the Village’s Comprehensive Development Master Plan and on the Village’s Official Zoning Map.

Division 1.4 of the Village’s Land Development Regulations requires all development to be consistent with the requirements of the Village’s Comprehensive Development Master Plan.

GOAL 1-1 (LAND USE) of the Village's Comprehensive Plan states that "The Village of Pinecrest Shall Maintain and Enhance the Extraordinary Character and Quality of Land Uses within the Village by:

- Balancing the Immediate and Long-Term Community Interests and Resources;
- Advancing the Aesthetic, Physical, Social, Cultural, and Economic conditions of its Residents;
- Ensuring that the Character and Location of Land Uses Incorporate Best Management Practices and Principles of Resource Conservation;
- Promoting Orderly Land Use Transition; and
- Protecting the Public Health, Safety, Welfare and rural aesthetic and Preventing Threats to Health, Safety, Welfare and rural aesthetic which may be caused by Incompatible Land Uses, Environmental Degradation, Hazards, and Nuisances".

Policy 1-2.4.1 of the Village's Comprehensive Plan requires lands zoned PS, Public and Semi-Public Services to "contain sufficient open space" – "Lands designated PS shall contain sufficient acreage and open space and shall be properly screened and buffered in order to minimize potential adverse impacts on adjacent land uses". The PS, Public and Semi-Public Services Zoning district requires a minimum of 35% green space. The Temple Bet Shira property has 35.13% green space and submitted plans indicate compliance with this requirement.

Implementation of the goals, objectives, and policies of the Village's Comprehensive Plan is achieved through application and enforcement of the requirements of the Village's Land Development Regulations. Policy 1-3.2.6 of the Village's Comprehensive Plan provides that the Village shall maintain Land Development Regulations for managing development in a manner consistent with goals, objectives and policies of the Comprehensive Plan. The purpose and intent are to ensure that potential community-wide impacts generated by future development are managed in a manner consistent with the Comprehensive Plan.

The current school enrollment of 325 students was approved by Miami-Dade County as a "Special Exception" (Conditional Use), consistent with the requirements of the County's Comprehensive Development Master Plan and Land Development Regulations. Since the time of approval of the existing school and enrollment, the Florida Legislature has enacted legislation, Florida Statutes, Chapter 1002.33 Charter Schools, Section (18), paragraph (a) which provides:

- (a) ".....The *local governing authority shall not adopt or impose any local building requirements or site-development restrictions, such as parking and site-size criteria,*

*student enrollment, and occupant load, that are addressed by and more stringent than those found in the State Requirements for Educational Facilities of the Florida Building Code....."*

Although Florida Statutes regulate site size criteria and student enrollment for public charter schools, all development in Florida is required to comply with a local municipality's Comprehensive Development Master Plan and applicable Land Development Regulations intended to implement the goals, objectives, and policies of the Comprehensive Plan. This requirement is further emphasized in "*The Amended and restated Interlocal Agreement for Public School Facility Planning in Miami-Dade County*", the adopted interlocal agreement with the Village of Pinecrest. Section 6 of the Interlocal Agreement states that the School Board will coordinate any and all proposed construction or expansion of public educational facilities with that City's adopted comprehensive plan and land development regulations.

The Land Development Regulations provide a regulatory framework which assists in resolving the following issues:

- Prevent loss of open space, landscaping, vegetation and tree canopies.

The proposed student enrollment increase will not result in the loss of any established open space, landscaping or tree canopy.

- Prevent encroachment of non-residential uses into residential areas.

With the exception of interior remodeling of the social hall to provide six new classrooms, no additional development or expansion of the campus or external facilities are proposed. Existing and proposed landscaping, the recommended increase in height of the existing wall, and proposed road improvements, curbing, sidewalk construction, and vehicle stacking plan are expected to mitigate existing and future impacts on the neighboring residential areas.

- Maintain and enhance infrastructure levels of service consistent with the goals, objectives, and policies of the Comprehensive Plan.

The submitted traffic study indicates that proposed road improvements to SW 77 Avenue, SW 120 Street, and internal queuing of vehicles will address transportation impacts and prevent a deterioration of existing and adopted levels of service for the adjoining roadways.

- Maintain and enhance traffic and pedestrian flow improvements.

Proposed road improvements to SW 77 Avenue and SW 120 Street, internal queuing of vehicles, construction of a new sidewalk adjacent to SW 77 Avenue, and enhancement of the sidewalk on the south side of SW 120 Street will address transportation impacts and prevent a deterioration of existing and adopted levels of service for the adjoining roadways.

- Require screening and/or buffer areas adjacent to areas of potentially incompatible land uses.

Existing and proposed landscaping, and the recommended increase in height of the existing wall will provide buffering in compliance with Village requirements.

### Division 3.3 Conditional Use Approval – General

When approved in 1979, the Temple Bet Shira School was a private school and regulated as a Conditional Use. With the exception of the Temple Bet Shira Pre-school, the elementary school has since become a Charter School, True North Academy, which is now considered to be a public school. Although the Village's Land Development regulations regulate private schools, and not public schools, the existing use was a private school and allowed as a conditional use in 1979; and the applicant now seeks to modify the conditions of approval of Miami-Dade County resolution 4-ZAB-68-79 that established the existing school and enrollment as a Special Exception (Conditional Use) at that time.

The purpose of Division 3.3 of the Village's Land Development Regulations is to "ensure that a conditional use or expansion of an existing conditional use shall only be permitted on specific sites where the proposed use or expansion may be adequately accommodated without generating adverse impacts on properties and land uses within the immediate vicinity". Criteria for approval or expansion of a Conditional Use include the following:

*1. LAND USE COMPATIBILITY: The Conditional Use, including its proposed scale and intensity, traffic generating characteristics, and off-site impacts shall be compatible and harmonious with adjacent land uses and shall not adversely impact land use activities in the immediate vicinity.*

Temple Bet Shira has existed since 1964. Compliance with the restrictions and requirements of the Land Development Regulations will help to ensure and maintain land use compatibility between the existing school and adjoining single-family residential uses. Substantial

landscaping will continue to buffer campus activities from adjoining residences located to the south and west.

A traffic study has been prepared for the applicant by Langan and has been reviewed by the Village's Traffic Engineering Consultant, Choice Engineering. The submitted traffic study indicates that the increase in enrollment from 325 students to 600 students will not cause levels of service on neighboring roads to deteriorate below existing or adopted levels of service with the completion of proposed road and traffic circulation improvements, and proposed staggered hours of operation. A copy of Choice Engineering's review of the revised study is attached.

Staff recommends that the traffic and queuing/stacking plan be submitted to Miami-Dade County for review.

Representatives of both Choice Engineering and Langan will be available at the scheduled public hearing on February 11, 2025 to discuss the findings and recommendations of the completed traffic study and traffic study review.

*2. SUFFICIENT SIZE, SITE SPECIFICATIONS AND INFRASTRUCTURE TO ACCOMMODATE THE PROPOSED USE. The size and shape of the site, the proposed access and internal circulation, and the urban design must be adequate to accommodate the proposed scale and intensity of conditional use requested. The site shall be of sufficient size to provide adequate screening, buffers, landscaping, open space, off-street parking, efficient internal traffic circulation, infrastructure and similar site plan improvements needed to mitigate against potential adverse impacts of the proposed use.*

The site, classrooms, and outdoor play area meet the requirements of the Land Development Regulations and are sufficient to accommodate the proposed 600 students.

*3. COMPLIANCE WITH THE COMPREHENSIVE DEVELOPMENT PLAN (CDMP) AND LAND DEVELOPMENT CODE. The conditional use and site plan shall comply with environmental, zoning and other applicable regulations of the Land Development Code, and shall be consistent with the CDMP.*

The existing school is consistent with the Future Land Use Element of the Village's Comprehensive Development Master Plan and was approved as a conditionally permitted use within the PS, Public Service zoning district.

## Land Development Regulations - Division 5.10 - Educational and childcare facilities, non-public

The applicant continues to comply with the following requirements of the Land Development Regulations that applied to the Temple Bet Shira School before it became a charter school.

### Classroom Size

The Land Development Regulations require that 25 square feet of classroom area be provided for each student. Classroom area in the amount of 15,000 square feet would be required to support a student enrollment of 600 students. The submitted "Classroom Summary Chart" shows a total of 24,752 square feet of classroom area is available at Temple Bet Shira and True North.

### Parking Spaces

Miami-Dade County Resolution Z-114-86 approved a variance for 158 parking spaces instead of 1,556 as otherwise required. The applicant proposes to convert 3,066 square feet of interior space to provide 6 new classrooms. For the additional Middle School classroom space, the Village's Land Development Regulations require one parking space for every 200 square feet of floor area. Based on the addition of 6 new classrooms (3,066 square feet), the Village's Land Development Regulations require an additional 16 parking spaces for a required total of 174 parking spaces. The submitted plan includes parking along the eastern, western, and southern boundaries of the property with an overall total of 197 parking spaces, in compliance with the parking variance approved by Miami-Dade County and demand created through the addition of 6 new classrooms.

*4. PROPER USE OF MITIGATIVE TECHNIQUES. The conditional use and site plan shall incorporate mitigative techniques needed to prevent adverse impacts to adjacent land uses. In addition, the design scheme shall appropriately address off-site impacts to ensure that land use activities in the immediate vicinity, including community infrastructure, are not burdened with adverse impacts detrimental to the general public health, safety and welfare.*

## **Neighborhood Impacts**

### Traffic

As indicated above, it is not expected that an increase in enrollment from 325 students to 600 students will cause levels of service on neighboring roads to deteriorate below existing

or adopted levels of service if the specified vehicle stacking plan, hours of operation and road and traffic circulation improvements are implemented as recommended by the Village's Traffic Engineering Consultant.

### Off-Campus Parking

During various site visits and completion of a recent drone surveillance video, it has become apparent that off-campus parking is occurring in the right-of-way, in the swales on both sides of SW 77 Avenue, and in the swales of other neighboring streets on the south side of SW 120 Street, by parents waiting to pick up students from both Miami-Palmetto Senior High School and True North in the afternoon.

In order to address and resolve these impacts, Temple Bet Shira and True North propose to provide internal stacking of vehicles during periods of morning drop-off of students and afternoon pick-up, to stagger hours of operation so as not to conflict with hours of operation and student pick-up and drop-off at Miami-Palmetto Senior High School, and to install concrete curbing along SW 120 Street and SW 77 Avenue. Staff further recommends that Temple Bet Shira and True North prepare and establish a parking policy that prohibits off-campus parking or standing of vehicles off-campus during hours of drop-off and pick-up.

### Noise

A neighboring property owner has expressed concern with existing noise generated on the playground on the south side of the property and is concerned that the proposed increase in enrollment will exacerbate the condition. Staff recommends that a noise wall or landscape buffer be installed adjacent to the playground to contain noise.

### Hours of Operation

Miami-Dade County Resolution 4-ZAB-68-79, Condition 7., limits hours of operation between 8:00 a.m. and 4:30 p.m. The applicants are requesting an increase in hours of operation from 8:00 a.m. to 4:30 p.m. to 7:15 a.m. to 6:00 p.m.

House Bill 733, enacted in 2023 and set to take effect on July 1, 2026, mandates that all public and charter middle schools in Florida must start no earlier than 8:00 AM, and high schools no earlier than 8:30 AM. Consequently, Miami-Palmetto Senior High School would be required to shift its start time to 8:30 AM. However, as of January 2025, there is ongoing debate among Florida lawmakers regarding this mandate. Some legislative efforts

are aiming to repeal or modify the bill due to concerns about transportation, scheduling, and logistical challenges.

Hours of operation are recommended as follows:

Scenario 1 (Before House Bill 733 takes effect):

- K through 1st grade: 7:45 a.m. to 2:45 p.m.
- 2nd grade through 5th grade: 8:15 a.m. to 3:15 p.m.
- 6th through 8th grade: 8:45 a.m. to 3:45 p.m.

Scenario 2 (If House Bill 733 takes effect on July 1, 2026, and Miami Palmetto Senior High adjusts dismissal time accordingly):

- K through 1st grade: 7:15 a.m. to 2:30 p.m.
- 2nd grade through 5th grade: 7:45 a.m. to 2:45 p.m.
- 6th grade through 8th grade: 8:15 a.m. to 3:15 p.m.

*5. HAZARDOUS WASTE. No conditional use which generates hazardous waste or uses hazardous materials shall be located in the Village unless the specific location is consistent with the CDMP, Land Development Code, and does not adversely impact well fields, aquifer recharge areas, or other conservation resources, as may be applicable now or in the future.*

This standard is not applicable. The proposed increase in enrollment is not expected to generate hazardous waste or require use of hazardous materials.

Division 4.4 of the Village's Land Development Regulations, states that the purpose of the PS, Public and Semi-Public Services zoning district is "to provide a management framework for implementing comprehensive development master plan development objectives and policies for existing and future public and semi-public services". Per Division 4.4 of the Village's Land Development Regulations, the location and distribution of specific types of public and semi-public services are required to be determined based on specified considerations 1 through 6 below:

1. Accommodating the land needs of existing public and semi-public land uses within the village.

No additional land is proposed to be developed with this application.

2. Provision, maintenance, and conservation of quality public services required to meet the needs of existing and projected future population magnitude, distribution, and characteristics.

There are 2,156 children school-aged in Pinecrest between the ages of 6 and 14 years old (2020 US Census). Current school capacity for children aged 6 to 14 years old in all Pinecrest schools is approximately 4,608 (St. Louis – 603; Temple Bet Shira – 325; Temple Beth Am – 700; Pinecrest Elementary 1,110; Palmetto Elementary – 668; Palmetto Middle – 1,202. Howard Drive Elementary has the capacity to accommodate 771 students.

Student enrollment reports provided by Miami-Dade County indicate that as of June 2024, there was available unused capacity (“Available Seats”) at the Village’s public elementary and middle schools as follows: Pinecrest Elementary – 240; Palmetto Elementary – 79; Palmetto Middle School – 469; Howard Drive Elementary School - 484.

This data would indicate that existing schools have the capacity to serve the needs of Pinecrest students.

3. Protecting established residential areas from encroachment by non-residential development.

The existing Temple has been established on the property in the PS, Public Services zoning district since 1964 and the current enrollment limit of 325 students was permitted in 1979. Although no new construction or expansion of existing facilities other than interior remodeling to create six new classrooms are proposed with this application, an increase in student enrollment will add to the existing activity on campus and will generate additional traffic. The existing wall along the southern boundary of the property along with the existing hedges and landscaping are in compliance with previous development orders. Additional landscaping and an increase in the height of the existing 5-foot wall to 6 feet in height are recommended to further buffer the impacts of any increase in student enrollment from neighboring residents. Staff further recommends that a 6-foot noise attenuating wall be constructed adjacent to the playground on the south side of the campus.

4. Ensuring compatibility with adjacent land uses to the greatest practical extent by providing landscaping and buffer areas to protect adjacent residential development from the off-site impacts of public and semi-public land uses.

Division 6.1 of the Village's Land Development Regulations requires that a *"Buffer between dissimilar land uses"* be provided as follows: "Where dissimilar land uses exist on adjacent properties, and where such areas will not be entirely visually screened by an intervening building or structure from abutting property, that portion of such area not so screened shall be provided with a buffer consisting of a six feet wall or fence with a life expectancy of at least ten years, together with shrubs which normally grow to a minimum height of six feet or more and shall be maintained at that height. Where chain link fencing is used, shrubs shall also be required to cover such fence. Shrubs used as a buffer shall be planted at a minimum of 24 inches on center and 30 inches in height at time of planting. The buffer shall form a continuous unbroken and solid visual screen between the dissimilar land uses within one year after planting. Buffers screening dissimilar uses shall include mahogany or oak trees, or similar hardwood canopy trees acceptable to the administrative official, planted at a maximum average spacing of 25 feet on center and shall have a height of at least 16 feet within the required setback. Shrubs required as a buffer shall not be trimmed shorter than 72 inches except where required in order to maintain a safe site distance".

There is an existing concrete block and stucco wall that exists along the southern boundary of the subject property adjacent to the established single family residential neighborhood to the south. The wall is approximately 5 feet in height. Staff recommends that the wall be increased to 6 feet in height as required.

The applicants propose to maintain and add additional landscaping along the north, south, and western boundaries of the property to include Orange Geiger, Pigeon Plum, Gumbo Limbo, and Live Oak Trees. Construction of the proposed right-turn lane on the east side of SW 77 Avenue will require removal and relocation of existing Crepe Myrtle and Sabal Palm trees. The applicant proposes replacing them with Orange Geiger and Pigeon Plum trees. Since the overhead power line will need to be removed with the addition of a right-turn lane, staff recommends that oak trees also be included in the variety of trees to be planted.

5. Providing and maintaining transportation and infrastructure system improvements.

A traffic study has been prepared for the applicant by Langan and has been reviewed by the Village's Traffic Engineering Consultant, Choice Engineering. The traffic study indicates that the increase in enrollment from 325 students to 600 students will not cause levels of service on neighboring roads to deteriorate below existing or adopted levels of service. Although the applicant is proposing to complete a traffic study for the Village's review and approval prior to an increase in enrollment from 500 to 600

students in School Year 28-29, the Village's transportation engineering consultant, Choice Engineering recommends that a new traffic study be prepared and submitted for review and approval prior to each proposed increase beyond 400 students in School Year 25-26. A copy of the applicant's traffic study and review by Choice Engineering are attached. Representatives of both Langan and Choice Engineering will be available at the scheduled public hearing on February 11, 2025 to discuss the findings of the traffic study and review comments.

Submitted plans show that access to the Temple and school will continue to be limited to SW 120 Street with right-turn only movements into and out of the east and west driveways. During pick-up and drop-off of students, vehicles will enter the east driveway (right-turn in) and exit the west driveway (right-turn out) with sufficient internal stacking for 95 vehicles.

Road improvements proposed to be engineered and constructed by the applicant include (1) Closure of one of the three existing driveways, the western driveway, leaving the center and eastern driveways for future ingress and egress; (2) Construction of a northbound right-turn lane on the east side of SW 77 Avenue, south of SW 120 Street, for improved traffic flow turning to the east to Miami-Palmetto High School and True North Classical Academy; (3) Construction of a southbound left turn lane on the east side of SW 77 Avenue, north of SW 120 Street, for improved traffic flow turning to the east to Miami-Palmetto High School and True North Classical Academy; (4) Construction of a pedestrian sidewalk on the east side of SW 77 Avenue; and (5) Construction of a right-turn lane on the south side of SW 120 Street between the two driveways to facilitate turning movements into the property for traffic approaching from the east. Review and approval of engineering and construction drawings by the Public Works Director is recommended prior to permitting and construction of these improvements.

Construction of the proposed right turn lane on the east side of SW 77 Avenue will require removal and relocation of an existing power pole at the southeast corner of SW 77 Avenue and SW 120 Street.

6. The public and semi-public service (PS) zoning district is intended to accommodate existing public and semi-public services including: governmental administration buildings; public schools and not-for-profit educational institutions; arts and cultural or civic facilities; essential public services, facilities, and utilities; fire and emergency operation facilities; places of worship; and accessory facilities; and other similar activities as identified in the land development code.

The existing Temple, pre-school, and K-8 school are established, consistent with the intent of the PS, Public and Semi-Public Service zoning district.

### STAFF RECOMMENDATION

The Building and Planning Department recommends approval of the proposed conditional use and modification of Miami-Dade County Resolution 4-ZAB-68-79 as later modified by Resolution Z-114-86 for an increase in student enrollment to 600 students subject to the following requirements and conditions:

- 1) Compliance with the submitted site plan entitled "True North Classical Academy Pinecrest Campus", as prepared by CIVICA Architecture, and dated November 8, 2024, including required revisions necessary to comply with the Village Council's Development Order and conditions of approval that may be imposed by the Village Council.
- 2) Compliance with all requirements of the Village of Pinecrest, including the Pinecrest Code of Ordinances, Comprehensive Development Master Plan, and recommendations of the Village's Traffic Engineering Consultant, Choice Engineering, as stipulated in their Traffic Study comments letter dated January 24, 2025.
- 3) Continued compliance with all requirements and conditions of Miami-Dade County Resolutions and Development Orders except for conditions of Resolution Z-114-86 which shall be modified as follows:
  1. through 5.- No change
  6. That the use shall be restricted to a maximum of ~~325~~ 600 children
  7. That the hours of operation shall be from ~~8:00 a.m. to 4:30 p.m.~~ 7:15 a.m. to 6:00 p.m. with class times to be regulated as follows:

Before House Bill 733 takes effect:

- K through 1st grade: 7:45 a.m. to 2:45 p.m.
- 2nd grade through 5th grade: 8:15 a.m. to 3:15 p.m.
- 6th through 8th grade: 8:45 a.m. to 3:45 p.m.

At the time House Bill 733 takes effect on July 1, 2026, and Miami-Palmetto Senior High School adjusts dismissal time accordingly:

- K through 1st grade: 7:15 a.m. to 2:30 p.m.

- 2nd grade through 5th grade: 7:45 a.m. to 2:45 p.m.
  - 6th grade through 8th grade: 8:15 a.m. to 3:15 p.m.
8. No Change.
9. That the number of grades will be from kindergarten to eighth grade.
- ~~10. That the maximum number of classrooms be 19.~~
- ~~11. That the maximum number of teachers will be 35. That the maximum number of administrative and clerical personnel (other than teachers) shall be 5.~~
2. through 15. – No Change.
- 16. Approved increases in student enrollment shall be phased in over the course of time as follows:**

School Year 25-26 – 400 Students Maximum.

School Year 26-27 – 460 Students Maximum, subject to administrative review and approval of an updated traffic study.

School Year 27-28 – 500 Students Maximum, subject to administrative review and approval of an updated traffic study.

School Year 28-29 – 600 Students Maximum, subject to administrative review and approval of an updated traffic study.

17. Preference for admissions shall be for students who live within the geographic boundaries of the Village of Pinecrest, subject to administrative review and approval of the methodology defining the geographic area of students, and implementing the preference requirement.

18. That in the approval of the site plan, the same be substantially in accordance with that submitted for the hearing entitled “True North Classical Academy Pinecrest Campus”, as prepared by CIVICA Architecture, and dated November 8, 2024.

- 4) Review and approval of engineering design and construction drawings for all public improvements by the Public Works Director, and submittal of a bond or letter of credit in the amount of 125% of an engineer’s estimated cost of public improvements prior to construction of proposed sidewalk and road improvements.
- 5) Issuance of required permits by the Village of Pinecrest prior to commencement of any landscaping, tree removal, construction or site alteration activities on the applicant’s property or within the public right-of-way.
- 6) Review of the site development plan and the submitted application by the Miami-Dade County Fire Rescue Department, and confirmation that the maximum number of students permitted by the Village Council is consistent with requirements of the Florida Fire Prevention Code and State Requirements for Educational Facilities.

- 7) Review and approval; of the application, site development plan, and traffic study by the Miami-Dade County Departments of Transportation and Public Works, Environmental Resources Management (DERM) and other applicable Departments.
- 8) Installation of curb and gutter along the south side of SW 120 Street and east side of SW 77 Avenue to restrict on-street parking.
- 9) Temple Bet Shira and True North shall prepare and establish a parking policy that prohibits off-campus parking or standing of vehicles off-campus during hours of drop-off and pick-up.
- 10) Construction of a 6-foot noise wall or landscape buffer adjacent to the playground on the south side of the property to attenuate playground noise.
- 11) Reconstruction of the existing wall along the southern property line to a height of six (6) feet.
- 12) Addition or substitution of trees adjacent to SW 77 Avenue to include four (4) Oak canopy trees.
- 13) Completion of required road, sidewalk, buffer wall, noise wall, landscaping, and pedestrian connectivity improvements, and the relocation of overhead utility lines along the eastern side of the SW 77 Avenue right-of-way adjacent to the subject property prior to any increase in enrollment beyond 400 students.
- 14) Certification of True North's submitted student enrollment figures by the Head of School and Board of Directors prior to renewal of the Certificate of Use and Occupancy and annually prior to October 1 of each year thereafter.
- 15) This conditional use does not grandfather, vest, or approve any use that violates this conditional use, these conditions, the Village Code of Ordinances, or other applicable law.
- 16) Conditions imposed by other development orders or resolutions affecting the property not expressly modified herein shall remain in full force and effect.
- 17) Review and approval of a Declaration of Covenants and Restrictions by the Village Attorney, to be recorded in the public records of Miami-Dade County, prior to an increase in student enrollment, prior to issuance of permits, and prior to issuance of a Certificate of Use and Occupancy.



# ENROLLMENT AT-A-GLANCE



Location Number: **2541** School Name: **HOWARD DRIVE ELEMENTARY**

Region: **SOUTH** Board District: **9** Feeder: **Miami Palmetto**

2023-24 School Boundary Student Counts EOY (June 2024)				
Capacity	TOTAL Boundary Student Count	Attending From In-Boundary	Attend Other Public Schools	Attend Charter
771				
Seats Available	246	98	118	30
484		39.8%	48.0%	12.2%

School Enrollment (June 2024)		
Total Enrollment	From within Boundary	From Outside of Boundary
287	98	189
	34.1%	65.9%

School Boundary Choice Data - Top 5 traditional & charter schools that boundary students are attending (data: Location Analytics 6/24)					
Other Traditional Public	Count	% Boundary	Charter	Count	% Boundary
Palmetto Elementary (4221)	61	22.10%	True North Classical Acad Pine (1019)	12	4.35%
Pinecrest Elementary (4421)	59	21.38%	Somerset Academy Elementary (2007)	4	1.45%
Coral Reef Elementary (1041)	7	2.54%	Somerset Academy Bay (5062)	4	1.45%
Sunset Elementary (5401)	4	1.45%	Archimedean Academy (0510)	1	0.36%
George Washington Carver Elem (0721)	3	1.09%	Somerset Academy(Silver Palms) (0332)	1	0.36%

## STUDENT MEMBERSHIP (FTE Survey Window)

Year	October Survey 2	February Survey 3	Difference
14-15	637	645	8
15-16	640	642	2
16-17	615	617	2
17-18	582	588	6
18-19	514	512	-2
19-20	455	457	2
20-21	383	387	4
21-22	348	360	12
22-23	361	376	15
23-24	336	340	4

FTE student membership data: Assessment, Research, and Data Analysis (ARDA)

UTILIZATION & CAPACITY	ENROLLMENT October Survey 2	Percent Utilization Capacity: 771
19-20	455	59.0%
20-21	383	49.7%
21-22	348	45.1%
22-23	361	46.8%
23-24	336	43.6%

Enrollment data: Assessment, Research, and Data Analysis (ARDA)  
PK included if applicable. Capacity data: FISH reports

## GRADE LEVEL COUNTS

GRADE LEVEL	2023-2024 (OCT - Survey 2)	2024-2025* (8/26/24)
K	44	43
1	46	44
2	50	54
3	55	46
4	44	57
5	53	47
TOTAL	292	291
Exiting Cohort	53	
Incoming Cohort	43	-10

\*2024-25 grade level enrollment data from FASCO

Enrollment	Capacity	% Utilization
334	771	43.32%

Enrollment data includes PrK (8/26/24 - FASCO)

2023-24 WITHDRAWALS	Private School	Out-of-State	Out-of-County
34	9	18	0

Withdrawal data source: FASCO June 2024

FEEDER PATTERN UTILIZATION	
Miami Palmetto	
Membership	12460
% Utilization	81%

Feeder data: ARDA October 2023 - Survey 2

2023-24 Accountability		
Grade	% pts	24-25 Tier
B	55%	T1

Data source: FLDOE & ETO

2023-24 Student Counts for School Grade Accountability Components						
	ELA	ELA LG	ELA L25	MATH	MA LG	MA L25
@ school	136	92	22	136	92	22
*home-zoned	0	0	0	0	0	0
	SCI	SS	MSA	GRAD	CCA	ELA 3rd
@ school	48	N/A	N/A	N/A	N/A	46
*home-zoned	0	N/A	N/A	N/A	N/A	0

\*Represents the number of students at alternative centers tied back to the school



# ENROLLMENT AT-A-GLANCE

Location Number: **6701** School Name: **PALMETTO MIDDLE SCHOOL**



Region: **SOUTH** Board District: **9** Feeder: **Miami Palmetto**

2023-24 School Boundary Student Counts EOY (June 2024)				
Capacity	TOTAL Boundary Student Count	Attending From In-Boundary	Attend Other Public Schools	Attend Charter
<b>1202</b>				
Seats Available	<b>991</b>	471	402	118
<b>469</b>		47.5%	40.6%	11.9%

School Enrollment (June 2024)		
Total Enrollment	From within Boundary	From Outside of Boundary
<b>733</b>	471	262
	64.3%	35.7%

School Boundary Choice Data - Top 5 traditional & charter schools that boundary students are attending (data: Location Analytics 6/24)						
Other Traditional Public			Count	% Boundary	Charter	
Vineland K-8 Center (5671)		92	9.94%	Somerset Academy Bay Middle (6128)	39	4.21%
George Washington Carver (6071)		82	8.86%	True North Classical Acad Pine (1019)	10	1.08%
Southwood Middle (6861)		68	7.34%	Somerset Acad Middle (S-Miami) (6053)	10	1.08%
Leewood K-8 Center (2881)		61	6.59%	Archimedean Conservatory (6006)	8	0.86%
Cutler Bay Middle (6111)		13	1.40%	Somerset Prep Academy Sunset (5002)	6	0.65%

## STUDENT MEMBERSHIP (FTE Survey Window)

Year	October Survey 2	February Survey 3	Difference
14-15	1079	1082	3
15-16	1063	1079	16
16-17	1026	1029	3
17-18	1029	1035	6
18-19	1027	1028	1
19-20	1066	1067	1
20-21	989	977	-12
21-22	819	837	18
22-23	837	832	-5
23-24	727	738	11

FTE student membership data: Assessment, Research, and Data Analysis (ARDA)

UTILIZATION & CAPACITY	ENROLLMENT October Survey 2	Percent Utilization Capacity: 1202
19-20	1066	88.7%
20-21	989	82.3%
21-22	819	68.1%
22-23	837	69.6%
23-24	727	60.5%

Enrollment data: Assessment, Research, and Data Analysis (ARDA)  
PK included if applicable. Capacity data: FISH reports

## GRADE LEVEL COUNTS

GRADE LEVEL	2023-2024 (OCT – Survey 2)	2024-2025* (8/26/24)
6	223	217
7	270	227
8	234	272
<b>TOTAL</b>	<b>727</b>	<b>716</b>
Exiting Cohort	234	<b>-17</b>
Incoming Cohort	217	

\*2024-25 grade level enrollment data from FASCO

Enrollment	Capacity	% Utilization
716	1202	59.57%

Enrollment data includes PrK (8/26/24 - FASCO)

2023-24 WITHDRAWALS	Private School	Out-of-State	Out-of-County
<b>21</b>	6	10	4

Withdrawal data source: FASCO June 2024

FEEDER PATTERN UTILIZATION	
Miami Palmetto	
Membership	12460
% Utilization	81%

Feeder data: ARDA October 2023 – Survey 2

2023-24 Accountability		
Grade	% pts	24-25 Tier
A	64%	T1

Data source: FLDOE & ETO

## 2023-24 Student Counts for School Grade Accountability Components

	ELA	ELA LG	ELA L25	MATH	MA LG	MA L25
@ school	675	663	171	674	662	171
*home-zoned	0	0	0	0	0	0
	SCI	SS	MSA	GRAD	CCA	ELA 3rd
@ school	222	246	145	N/A	N/A	N/A
*home-zoned	0	0	0	N/A	N/A	N/A

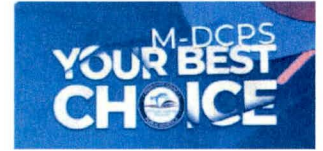
\*Represents the number of students at alternative centers tied back to the school



# ENROLLMENT AT-A-GLANCE

Location Number: 4221 School Name: PALMETTO ELEMENTARY

Region: SOUTH Board District: 9 Feeder: Miami Palmetto



2023-24 School Boundary Student Counts EOY (June 2024)				
Capacity	TOTAL Boundary Student Count	Attending From In-Boundary	Attend Other Public Schools	Attend Charter
668				
Seats Available	341	236	83	22
79		69.2%	24.3%	6.5%

School Enrollment (June 2024)		
Total Enrollment	From within Boundary	From Outside of Boundary
589	236	353
	40.1%	59.9%

School Boundary Choice Data - Top 5 traditional & charter schools that boundary students are attending (data: Location Analytics 6/24)							
Other Traditional Public			Count	% Boundary	Charter		
Pinecrest Elementary (4421)			63	17.60%	True North Classical Acad Pine (1019)	8	2.23%
Sunset Elementary (5401)			7	1.96%	True North Classical - Kendall (1024)	2	0.56%
Coral Reef Elementary (1041)			5	1.40%	Somerset Academy Elementary (2007)	2	0.56%
Howard Drive Elementary (2541)			2	0.56%	Pinecrest Cove Academy (5049)	2	0.56%
South Miami K-8 Center (5241)			2	0.56%	Archimedean Academy (0510)	1	0.28%

### STUDENT MEMBERSHIP (FTE Survey Window)

Year	October Survey 2	February Survey 3	Difference
14-15	642	645	3
15-16	617	611	-6
16-17	622	636	14
17-18	620	632	12
18-19	612	613	1
19-20	628	627	-1
20-21	592	586	-6
21-22	593	596	3
22-23	666	651	-15
23-24	631	636	5

FTE student membership data: Assessment, Research, and Data Analysis (ARDA)

UTILIZATION & CAPACITY	ENROLLMENT October Survey 2	Percent Utilization Capacity: 668
19-20	628	94.0%
20-21	592	88.6%
21-22	593	88.8%
22-23	666	99.7%
23-24	631	94.5%

Enrollment data: Assessment, Research, and Data Analysis (ARDA)  
PK included if applicable. Capacity data: FISH reports

### GRADE LEVEL COUNTS

GRADE LEVEL	2023-2024 (OCT - Survey 2)	2024-2025* (8/26/24)
K	91	83
1	108	107
2	102	106
3	100	111
4	97	98
5	91	100
<b>TOTAL</b>	<b>589</b>	<b>605</b>
Exiting Cohort	91	-8
Incoming Cohort	83	

\*2024-25 grade level enrollment data from FASCO

Enrollment	Capacity	% Utilization
652	668	97.60%

Enrollment data includes PrK (8/26/24 - FASCO)

2023-24 WITHDRAWALS	Private School	Out-of-State	Out-of-County
21	8	7	6

Withdrawal data source: FASCO June 2024

FEEDER PATTERN UTILIZATION	
Miami Palmetto	
Membership	12460
% Utilization	81%

Feeder data: ARDA October 2023 - Survey 2

2023-24 Accountability		
Grade	% pts	24-25 Tier
A	86%	T1

Data source: FLDOE & ETO

### 2023-24 Student Counts for School Grade Accountability Components

	ELA	ELA LG	ELA L25	MATH	MA LG	MA L25
@ school	279	180	46	279	180	45
*home-zoned	0	0	0	0	0	0
	SCI	SS	MSA	GRAD	CCA	ELA 3rd
@ school	86	N/A	N/A	N/A	N/A	97
*home-zoned	0	N/A	N/A	N/A	N/A	0

\*Represents the number of students at alternative centers tied back to the school



# ENROLLMENT AT-A-GLANCE



Location Number: 4421 School Name: PINECREST ELEMENTARY

Region: SOUTH Board District: 9 Feeder: Miami Palmetto

2023-24 School Boundary Student Counts EOY (June 2024)				
Capacity	TOTAL Boundary Student Count	Attending From In-Boundary	Attend Other Public Schools	Attend Charter
1110				
Seats Available	462	386	46	30
240		83.5%	10.0%	6.5%

School Enrollment (June 2024)		
Total Enrollment	From within Boundary	From Outside of Boundary
870	386	484
	44.4%	55.6%

School Boundary Choice Data - Top 5 traditional & charter schools that boundary students are attending (data: Location Analytics 6/24)					
Other Traditional Public			Charter		
Other Traditional Public	Count	% Boundary	Charter	Count	% Boundary
Sunset Elementary (5401)	18	3.79%	True North Classical Acad Pine (1019)	12	2.53%
Palmetto Elementary (4221)	16	3.37%	Somerset Academy Elementary (2007)	5	1.05%
Coral Reef Elementary (1041)	5	1.05%	Somerset Academy Continental (0402)	1	0.21%
Kenwood K-8 Center (2701)	2	0.42%	Pinecrest Cove Academy (5049)	1	0.21%
South Miami K-8 Center (5241)	2	0.42%	Bridgeprep Academy South (2003)	1	0.21%

### STUDENT MEMBERSHIP (FTE Survey Window)

Year	October Survey 2	February Survey 3	Difference
14-15	1042	1045	3
15-16	1032	1044	12
16-17	1021	1025	4
17-18	991	989	-2
18-19	1000	1012	12
19-20	962	964	2
20-21	971	964	-7
21-22	981	988	7
22-23	945	960	15
23-24	967	975	8

FTE student membership data: Assessment, Research, and Data Analysis (ARDA)

UTILIZATION & CAPACITY	ENROLLMENT October Survey 2	Percent Utilization Capacity: 1110
19-20	962	86.7%
20-21	971	87.5%
21-22	981	88.4%
22-23	945	85.1%
23-24	967	87.1%

Enrollment data: Assessment, Research, and Data Analysis (ARDA)  
PK included if applicable. Capacity data: FISH reports

### GRADE LEVEL COUNTS

GRADE LEVEL	2023-2024 (OCT - Survey 2)	2024-2025* (8/26/24)
K	147	162
1	138	155
2	170	145
3	153	177
4	156	144
5	146	143
<b>TOTAL</b>	<b>910</b>	<b>926</b>
Exiting Cohort	146	16
Incoming Cohort	162	

\*2024-25 grade level enrollment data from FASCO

Enrollment	Capacity	% Utilization
1001	1110	90.18%

Enrollment data includes PrK (8/26/24 - FASCO)

2023-24 WITHDRAWALS	Private School	Out-of-State	Out-of-County
54	36	14	3

Withdrawal data source: FASCO June 2024

FEEDER PATTERN UTILIZATION	
Miami Palmetto	
Membership	12460
% Utilization	81%

Feeder data: ARDA October 2023 - Survey 2

2023-24 Accountability		
Grade	% pts	24-25 Tier
A	76%	T1

Data source: FLDOE & ETO

### 2023-24 Student Counts for School Grade Accountability Components

	ELA	ELA LG	ELA L25	MATH	MA LG	MA L25
@ school	440	284	71	440	284	75
*home-zoned	0	0	0	0	0	0
	SCI	SS	MSA	GRAD	CCA	ELA 3rd
@ school	140	N/A	N/A	N/A	N/A	148
*home-zoned	0	N/A	N/A	N/A	N/A	0

\*Represents the number of students at alternative centers tied back to the school



# ENROLLMENT AT-A-GLANCE



Location Number: 7431 School Name: MIAMI PALMETTO SENIOR HIGH

Region: SOUTH Board District: 9 Feeder: Miami Palmetto

2023-24 School Boundary Student Counts EOY (June 2024)				
Capacity	TOTAL Boundary Student Count	Attending From In-Boundary	Attend Other Public Schools	Attend Charter
<b>2713</b>				
Seats Available	<b>2370</b>	1533	744	93
<b>104</b>		64.7%	31.4%	3.9%

School Enrollment (June 2024)		
Total Enrollment	From within Boundary	From Outside of Boundary
<b>2609</b>	1533	1076
	58.8%	41.2%

School Boundary Choice Data - Top 5 traditional & charter schools that boundary students are attending (data: Location Analytics 6/24)					
Other Traditional Public			Charter		
Other Traditional Public	Count	% Boundary	Charter	Count	% Boundary
Coral Reef Senior High (7101)	336	14.35%	True North Classical Academy (1000)	43	1.84%
Robert Morgan Educational Ctr (7371)	70	2.99%	Mater Academy Bay High School (7120)	19	0.81%
Terra Environmental Research (7029)	46	1.96%	Somerset Academy Charter High (7042)	8	0.34%
Cutler Bay Senior High (6081)	39	1.67%	Doral Academy High School (7020)	8	0.34%
New World School Of The Arts (7901)	25	1.07%	Archimedean Upper Conserv Char (7265)	5	0.21%

### STUDENT MEMBERSHIP (FTE Survey Window)

Year	October Survey 2	February Survey 3	Difference
14-15	2612	2604	-8
15-16	2588	2573	-15
16-17	2645	2642	-3
17-18	2777	2729	-48
18-19	2776	2728	-48
19-20	2739	2721	-18
20-21	2772	2746	-26
21-22	2716	2697	-19
22-23	2669	2625	-44
23-24	2672	2658	-14

FTE student membership data: Assessment, Research, and Data Analysis (ARDA)

UTILIZATION & CAPACITY	ENROLLMENT October Survey 2	Percent Utilization Capacity: 2713
19-20	2739	101.0%
20-21	2772	102.2%
21-22	2716	100.1%
22-23	2669	98.4%
23-24	2672	98.5%

Enrollment data: Assessment, Research, and Data Analysis (ARDA)  
PK included if applicable. Capacity data: FISH reports

### GRADE LEVEL COUNTS

GRADE LEVEL	2023-2024 (OCT - Survey 2)	2024-2025* (8/26/24)
9	641	556
10	660	679
11	694	688
12	677	684
<b>TOTAL</b>	<b>2672</b>	<b>2607</b>
Exiting Cohort	677	<b>-121</b>
Incoming Cohort	556	

\*2024-25 grade level enrollment data from FASCO

Enrollment	Capacity	% Utilization
2607	2713	96.09%

Enrollment data includes PrK (8/26/24 - FASCO)

2023-24 WITHDRAWALS	Private School	Out-of-State	Out-of-County
<b>96</b>	42	37	13

Withdrawal data source: FASCO June 2024

FEEDER PATTERN UTILIZATION	
Miami Palmetto	
Membership	12460
% Utilization	81%

Feeder data: ARDA October 2023 - Survey 2

2023-24 Accountability		
Grade	% pts	24-25 Tier
A	64%	T1

Data source: FLDOE & ETO

### 2023-24 Student Counts for School Grade Accountability Components

	ELA	ELA LG	ELA L25	MATH	MA LG	MA L25
@ school	1191	1164	300	890	867	226
*home-zoned	6	3	0	3	1	1
	SCI	SS	MSA	GRAD	CCA	ELA 3rd
@ school	502	629	N/A	614	345	N/A
*home-zoned	1	5	N/A	0	0	N/A

\*Represents the number of students at alternative centers tied back to the school

**PUBLIC HEARING NOTICE**



**PINECREST**

Building and Planning Department

**PUBLIC HEARING NOTICE**

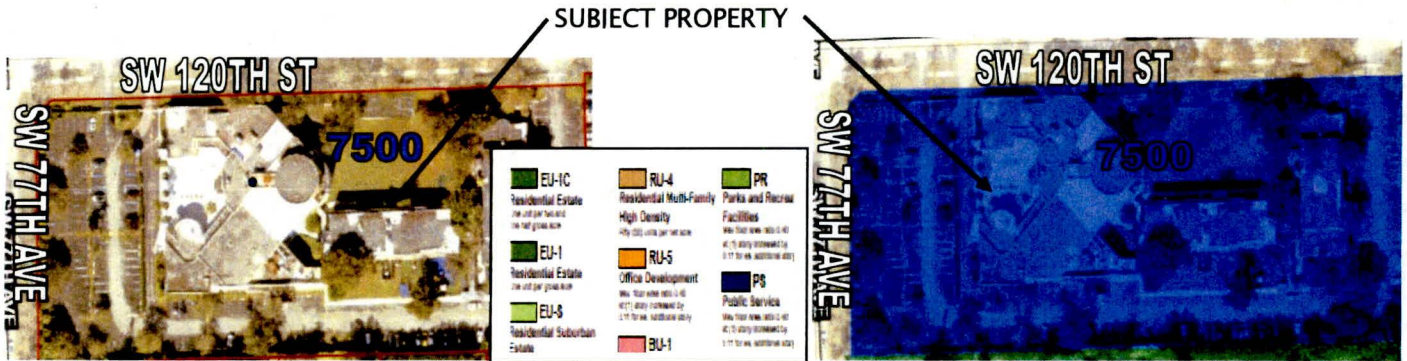
On Tuesday, February 11, 2025, to begin being heard not later than 7:00 p.m., the Village Council will conduct the following Public Hearing to be held at the Pinecrest Municipal Center, Council Chamber, 12645 Pinecrest Parkway, Pinecrest, Florida:

**OWNERS/APPLICANT(S):** Bet Shira Congregation, Inc. (Owner) and True North Classical Academy (Applicant)

**ITEM:** Conditional use permit and modification of Miami-Dade County Resolution 4-ZAB-68-79 as later modified by Resolution Z-114-86.

**LOCATION:** The subject property, True North Academy and Temple Bet Shira, is located at 7500 SW 120 Street, Pinecrest, FL 33156, within the PS, Public Service Zoning District.

**REQUEST:** Bet Shira Congregation, Inc. (Owner) and True North Classical Academy (Applicant) are requesting approval of a conditional use permit and modification of Miami-Dade County Resolution 4-ZAB-68-79 as later modified by Resolution Z-114-86 to permit an increase in student enrollment and occupancy from 325 students to 600 students; to permit an increase in hours of operation from 8:00 a.m. to 4:30 p.m., to 7:15 a.m. to 6:00 p.m.; to delete the condition requiring driveway access from SW 77 Avenue to be closed; to delete the limitation on the number of classrooms; to delete the limitation on the number of teachers and administrative personnel; to add a new condition allowing the requested increase in enrollment to be phased in over the course of 4 years; and to add a new condition establishing preference for admissions for students who live within the Village of Pinecrest.



All interested parties are urged to participate. Live streaming video of the meeting will be available at [www.pinecrest-fl.gov/live](http://www.pinecrest-fl.gov/live). If you do not wish to attend the in-person meeting, public comments may be submitted in writing prior to the hearing via e-mail to [planning@pinecrest-fl.gov](mailto:planning@pinecrest-fl.gov) or on ZOOM during the meeting by sending a request to [clerk@pinecrest-fl.gov](mailto:clerk@pinecrest-fl.gov) prior to 12:00 Noon on Tuesday, February 11, 2025. For additional information, you may contact the Building and Planning Department by calling (305) 234-2121, via e-mail at [planning@pinecrest-fl.gov](mailto:planning@pinecrest-fl.gov), or writing to the department at 12645 Pinecrest Parkway, Pinecrest, Florida 33156, where a copy of the application is on file.

In accordance with the Americans with Disabilities Act of 1990, all persons with disabilities and who need special accommodations to participate in this meeting because of that disability should contact the Village Clerk at (305) 234-2121 no later than four (4) business days prior to such proceeding.

Should any person decide to appeal any decision of the Village Council with respect to any matter considered at such meeting or hearing, that person will need a record of the proceedings, and for such purpose, may need to ensure that a verbatim record of the proceedings is made, which record includes the testimony and evidence upon which the appeal is to be based (F.S. 286.0105).

# ZONING HEARING SIGNS NOTIFICATION INSTALLATION

TRUE NORTH CLASSICAL ACADEMY – VILLAGE OF PINECREST CAMPUS  
(7500 S.W. 120<sup>th</sup> STREET)

- NUMBER OF NOTIFICATION SIGNS ISSUED ..... (6) SIX SIGNS
- DATE COLLECTED ..... JANUARY 10<sup>th</sup>, 2025
- DATE OF INSTALLATION ..... JANUARY 10<sup>th</sup>, 2025
- INSTALL LOCATION..... (2) TWO SIGNS INSTALLED FRONTING S.W. 77 AVENUE.  
(4) FOUR SIGNS INSTALLED FRONTING S.W. 120 STREET.



SIGNATURE OF INSTALLER:


IVAN FAJARDO

CIVICA, LLC. PROJECT MANAGER

01.10.2025

# ZONING HEARING SIGNS NOTIFICATION INSTALLATION

## TRUE NORTH CLASSICAL ACADEMY – VILLAGE OF PINECREST CAMPUS (7500 S.W. 120<sup>th</sup> STREET)



**PUBLIC HEARING NOTICE**      **PUBLIC HEARING NOTICE**

**PINECREST**  
Building and Planning Department

On Tuesday, February 11, 2025, to begin being heard not later than 7:00 p.m., the Village Council will conduct the following Public Hearing to be held at the Pinecrest Municipal Center, Council Chamber, 12645 Pinecrest Parkway, Pinecrest, Florida:

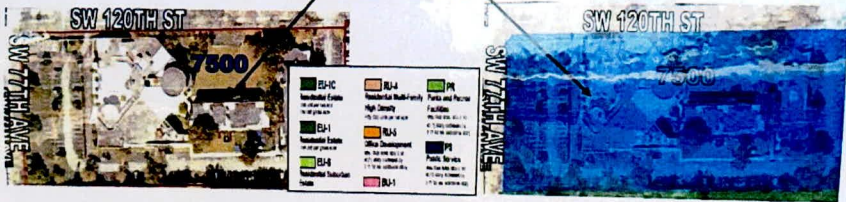
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**ITEM:** Conditional use permit and modification of Miami-Dade County Resolution 4-ZAB-68-79 as later modified by Resolution Z-114-86

**LOCATION:** The subject property, True North Academy and Temple Bet Shira, is located at 7500 SW 120 Street, Pinecrest, FL 33156, within the PS, Public Service Zoning District.

**REQUEST:** Bet Shira Congregation, Inc. (Owner) and True North Classical Academy (Applicant) are requesting approval of a conditional use permit and modification of Miami-Dade County Resolution 4-ZAB-68-79 as later modified by Resolution Z-114-86 to permit an increase in student enrollment and occupancy from 325 students to 600 students; to permit an increase in hours of operation from 8:00 a.m. to 4:30 p.m., to 7:15 a.m. to 6:00 p.m.; to delete the condition requiring driveway access from SW 77 Avenue to be closed; to delete the limitation on the number of classrooms; to delete the limitation on the number of teachers and administrative personnel; to add a new condition allowing the requested increase in enrollment to be phased in over the course of 4 years; and to add a new condition establishing preference for admissions for students who live within the Village of Pinecrest.

**SUBJECT PROPERTY**



<b>ES-1C</b> Executive Center 100,000 sq. ft. or more	<b>RS-1</b> Residential Single-Family 100,000 sq. ft. or more	<b>PS</b> Public Service 100,000 sq. ft. or more
<b>ES-1</b> Executive Center 10,000 sq. ft. or more	<b>RS-2</b> Residential Single-Family 10,000 sq. ft. or more	<b>PS-1</b> Public Service 10,000 sq. ft. or more
<b>ES-2</b> Executive Center 10,000 sq. ft. or more	<b>RS-3</b> Residential Single-Family 10,000 sq. ft. or more	<b>PS-2</b> Public Service 10,000 sq. ft. or more
<b>ES-3</b> Executive Center 10,000 sq. ft. or more	<b>RS-4</b> Residential Single-Family 10,000 sq. ft. or more	<b>PS-3</b> Public Service 10,000 sq. ft. or more
<b>ES-4</b> Executive Center 10,000 sq. ft. or more	<b>RS-5</b> Residential Single-Family 10,000 sq. ft. or more	<b>PS-4</b> Public Service 10,000 sq. ft. or more
<b>ES-5</b> Executive Center 10,000 sq. ft. or more	<b>RS-6</b> Residential Single-Family 10,000 sq. ft. or more	<b>PS-5</b> Public Service 10,000 sq. ft. or more
<b>ES-6</b> Executive Center 10,000 sq. ft. or more	<b>RS-7</b> Residential Single-Family 10,000 sq. ft. or more	<b>PS-6</b> Public Service 10,000 sq. ft. or more
<b>ES-7</b> Executive Center 10,000 sq. ft. or more	<b>RS-8</b> Residential Single-Family 10,000 sq. ft. or more	<b>PS-7</b> Public Service 10,000 sq. ft. or more
<b>ES-8</b> Executive Center 10,000 sq. ft. or more	<b>RS-9</b> Residential Single-Family 10,000 sq. ft. or more	<b>PS-8</b> Public Service 10,000 sq. ft. or more
<b>ES-9</b> Executive Center 10,000 sq. ft. or more	<b>RS-10</b> Residential Single-Family 10,000 sq. ft. or more	<b>PS-9</b> Public Service 10,000 sq. ft. or more
<b>ES-10</b> Executive Center 10,000 sq. ft. or more	<b>RS-11</b> Residential Single-Family 10,000 sq. ft. or more	<b>PS-10</b> Public Service 10,000 sq. ft. or more

All interested parties are urged to participate. Live streaming video of the meeting will be available at [www.pinecrest-fl.gov/live](http://www.pinecrest-fl.gov/live). If you do not wish to attend the in-person meeting, public comments may be submitted in writing prior to the hearing via e-mail to [planning@pinecrest-fl.gov](mailto:planning@pinecrest-fl.gov) or on ZOOM during the meeting by sending a request to [clerk@pinecrest-fl.gov](mailto:clerk@pinecrest-fl.gov) prior to 12:00 Noon on Tuesday, February 11, 2025. For additional information, you may contact the Building and Planning Department by calling (305) 234-2121, via e-mail at [planning@pinecrest-fl.gov](mailto:planning@pinecrest-fl.gov), or writing to the department at 12645 Pinecrest Parkway, Pinecrest, Florida 33156, where a copy of the application is on file.

In accordance with the Americans with Disabilities Act of 1990, all persons with disabilities and who need special accommodations to participate in this meeting because of that disability should contact the Village Clerk at (305) 234-2121 no later than four (4) business days prior to such proceeding.

Should any person decide to appeal any decision of the Village Council with respect to any matter considered at such meeting or hearing, that person will need a record of the proceedings, and for such purpose, may need to ensure that a verbatim record of the proceedings is made, which record includes the testimony and evidence upon which the appeal is to be based (F.S. 286.0105).

12645 Pinecrest Parkway, Pinecrest, Florida 33156 | T: 305 234 2121 | F: 305 234 2133 | [www.pinecrest-fl.gov](http://www.pinecrest-fl.gov)

# ZONING HEARING SIGNS NOTIFICATION INSTALLATION

TRUE NORTH CLASSICAL ACADEMY – VILLAGE OF PINECREST CAMPUS  
(7500 S.W. 120<sup>th</sup> STREET)

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## SIGN LOCATION No. 01: (SW 77<sup>th</sup> Avenue)



# ZONING HEARING SIGNS NOTIFICATION INSTALLATION

TRUE NORTH CLASSICAL ACADEMY – VILLAGE OF PINECREST CAMPUS  
(7500 S.W. 120<sup>th</sup> STREET)

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## SIGN LOCATION No. 02: (SW 77<sup>th</sup> Avenue – Intersection with SW 120<sup>th</sup> Street)

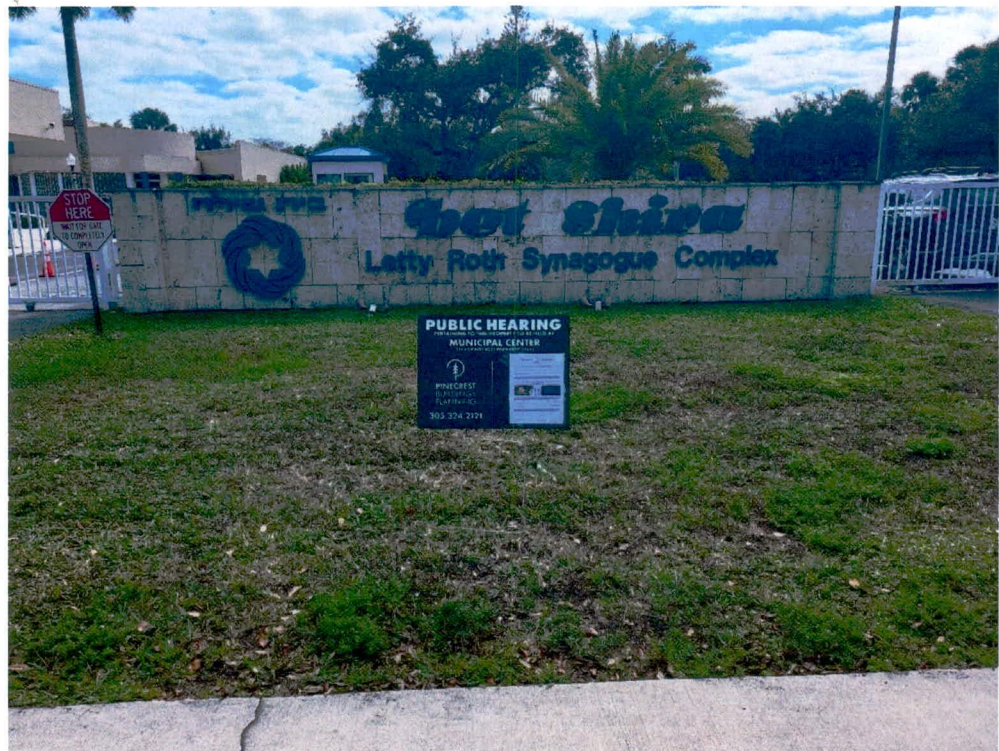


# ZONING HEARING SIGNS NOTIFICATION INSTALLATION

TRUE NORTH CLASSICAL ACADEMY – VILLAGE OF PINECREST CAMPUS  
(7500 S.W. 120<sup>th</sup> STREET)

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## SIGN LOCATION No. 03: (SW 120<sup>th</sup> Street)

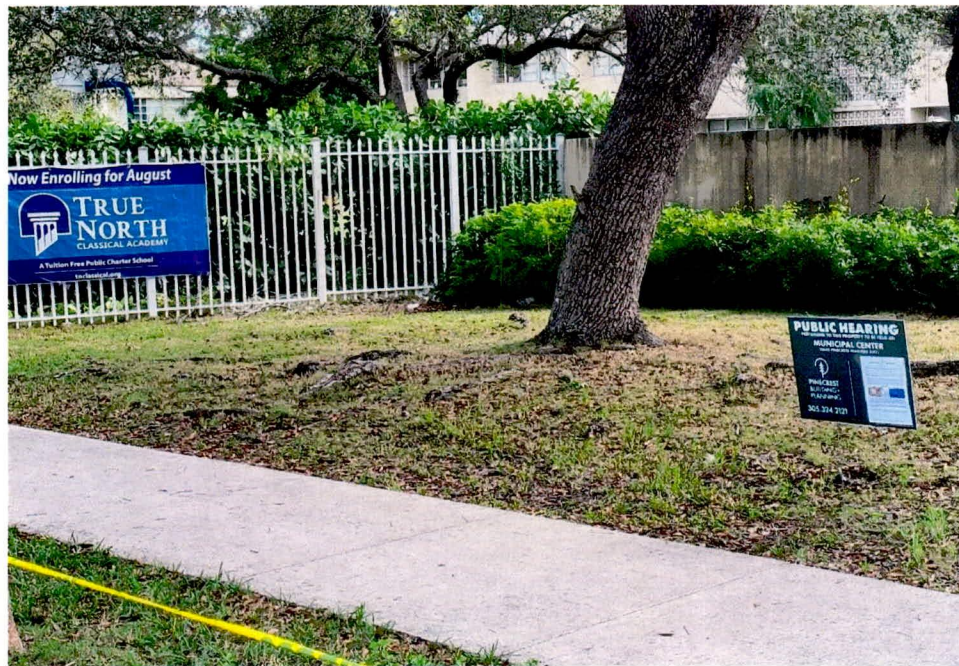


# ZONING HEARING SIGNS NOTIFICATION INSTALLATION

TRUE NORTH CLASSICAL ACADEMY – VILLAGE OF PINECREST CAMPUS  
(7500 S.W. 120<sup>th</sup> STREET)

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## SIGN LOCATION No. 04: (SW 120<sup>th</sup> Street)



# ZONING HEARING SIGNS NOTIFICATION INSTALLATION

TRUE NORTH CLASSICAL ACADEMY – VILLAGE OF PINECREST CAMPUS  
(7500 S.W. 120<sup>th</sup> STREET)

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## SIGN LOCATION No. 05: (SW 120<sup>th</sup> Street)



# ZONING HEARING SIGNS NOTIFICATION INSTALLATION

TRUE NORTH CLASSICAL ACADEMY – VILLAGE OF PINECREST CAMPUS  
(7500 S.W. 120<sup>th</sup> STREET)

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## SIGN LOCATION No. 06: (SW 120<sup>th</sup> Street)



*End of Document*

**Ana Caceres**

---

**From:** Esther Cabrera (BPD)  
**Sent:** Monday, January 27, 2025 9:52 AM  
**To:** Stephen Olmsted (BPD)  
**Cc:** Melissa Oklan; Ana Caceres  
**Subject:** RE: Hearing 2025-0211-1

Steve,

Please see response from Ms. Oklan.



**Esther Cabrera**

Code Compliance Supervisor  
Village of Pinecrest  
12645 Pinecrest Parkway  
Pinecrest, Florida 33156  
305.234.2121  
[www.pinecrest-fl.gov](http://www.pinecrest-fl.gov)



---

**From:** Melissa Oklan <[melissaoklan@gmail.com](mailto:melissaoklan@gmail.com)>  
**Sent:** Sunday, January 26, 2025 9:27 AM  
**To:** Esther Cabrera (BPD) <[ecabrera@pinecrest-fl.gov](mailto:ecabrera@pinecrest-fl.gov)>  
**Subject:** Hearing 2025-0211-1

**CAUTION: Don't be quick to click! We're counting on you!** This email is from an external sender! Don't click on links or open attachments from unknown sources. If you know this is spam delete the message. If you need further assistance with an unknown message please contact [gwilson@pinecrest-fl.gov](mailto:gwilson@pinecrest-fl.gov)

I am writing in reference to Hearing 2025-0211-1. I am a resident of Pinecrest. Please be advised I OPPOSE the CONDITIONAL USE PERMIT AND MODIFICATION OF MIAMI-DADE COUNTY RESOLUTION 4ZAB-68-79 AS LATER MODIFIED BY RESOLUTION Z-114-86 TO PERMIT AN INCREASE IN STUDENT ENROLLMENT AND OCCUPANCY FROM 325 STUDENTS TO 600 STUDENTS; TO PERMIT AN INCREASE IN HOURS OF OPERATION FROM 8:00 A.M. TO 4:30 P.M. TO 7:15 A.M. TO 6:00 P.M.; TO DELETE THE CONDITION REQUIRING DRIVEWAY ACCESS FROM SW 77 AVENUE TO BE CLOSED; TO DELETE THE LIMITATION ON THE NUMBER OF CLASSROOMS; AND TO DELETE THE LIMITATION ON THE NUMBER OF TEACHERS AND ADMINISTRATIVE PERSONNEL AT TRUE NORTH ACADEMY AND TEMPLE BET SHIRA, LOCATED AT 7500 SW 120 STREET.

As Pinecrest residents, we are lucky to have excellent public schools. This does not benefit us the residents and is only a burden on traffic. Please do not permit this change.

Sincerely submitted,

April 21, 2023

**VIA HAND DELIVERY**

Mr. Stephen Olmstead  
Planning Director  
Village of Pinecrest  
12645 Pinecrest Parkway  
Pinecrest, Florida 33156

**Re: Letter of Intent / Bet Shira Congregation, Inc.**

Dear Mr. Olmstead:

This shall constitute our Letter of Intent on behalf of Bet Shira Congregation, Inc. (the "Applicant") in connection with the enclosed application for a modification of a previously approved zoning resolution (the "Application"). The purpose of the Application is to request the modification of Resolution 4-ZAB-68-79 as later modified by Resolution Z-114-86, in order to permit the expansion of the True North public charter school currently operating on the subject property. The subject property is located on that certain parcel of land, consisting of approximately ±1.32 acres, located at 7500 SW 120th Street, Pinecrest, Florida. This parcel is further identified by Miami-Dade County Property Appraiser Folio No. 20-5014-004-0050 (the "Property"). The boundaries of the Property are generally depicted on the aerial below.



The Property is currently developed with the Congregation Bet Shira religious facility, a portion of which is being operated as an existing public charter school by True North Classical Academy, Inc.. The temple educational uses were originally established through a series of approvals granted by Miami-Dade County ("County") prior to the incorporation of the Village of Pinecrest. In 1979, the County approved the addition of a private school to the then-existing religious facility through the adoption of Resolution 4-ZAB-68-79 (the "1979 Resolution"), together with a number of non-use variances as shown on the approved hearing plan and subject to a number of conditions. This approval was later modified through Resolution Z-114-86 (the "1986 Resolution"), which approved an expansion of the temple facilities along with additional non-use variances (collectively, the "Resolutions"). Copies of the Resolutions are included with the application materials for your convenience. The temple facilities and the private school have continued to operate under the Resolutions through the incorporation of the Village.

As noted above, the Resolutions included a number of non-use variances and conditions relating to the temple structures and the private school use. In addition to conditions tying development of the site to the hearing plan, Conditions 6 through 11 of the 1979 Resolution governed the operation of the then-private school:

6. That the use shall be restricted to a maximum of 325 children.
7. That the hours of operation shall be from 8:00 a.m. to 4:30 p.m.
8. That a play area, if any, shall be enclosed with a fence of a type and at a location to be approved by the Zoning Director.
9. That the number of grades will be from Kindergarten to sixth grade.
10. That the maximum number of classrooms will be 19.
11. That the maximum number of teachers will be 35. That the maximum number of administrative and clerical personnel (other than teachers) will be 5.

For the last several decades, the educational facility on the Property has operated under these conditions, variously as a private religious day school operated by the congregation or as a private school.

The Applicant has now entered into an agreement with True North Classical Academy ("True North") to operate the educational uses on the Property. In order to accommodate the strong demand for enrollment at the school, True North would like to increase the number of student stations provided on the Property from 325 children to 600 children. True North has determined that the existing facilities have more than enough room to accommodate a larger student body, so no modifications to the existing buildings or other aspect of the site are necessary. The only changes would be internal, converting office and other existing spaces to classroom area. True North has also commissioned a traffic study prepared by Langan Engineering, to analyze whether the existing circulation patterns and area roads are sufficient to handle an increase in students, and that study is also included in the Application materials.

As we have discussed, True North is a public charter school operating under a duly-granted charter from the Miami-Dade County Public School district. By operation of state law, such charter schools are expressly deemed to be public schools, and are part of the state's program of public education. Because the educational use is changing from a private school to a public charter school, under the provisions of Chapter 1002, Florida Statute and the Village of Pinecrest Code the various non-use variances relating to private schools that were granted by the Resolutions are

no longer relevant. Public charter schools do not fall within the provisions governing private schools, and are instead regulated as would any other public school.

Accordingly, the Applicant is requesting modifications to the various conditions of the 1979 Resolution to accommodate their proposed public charter school operations, as follows:

6. That the use shall be restricted to a maximum of ~~325~~ 600 children
7. That the hours of operation shall be from ~~8:00~~ 7:15 a.m. to ~~4:30~~ 5:30 p.m.
8. That a play area, if any, shall be enclosed with a fence of a type and at a location to be approved by the Zoning Director.
9. That the number of grades will be from Kindergarten to ~~sixth~~ eighth grade.
10. ~~That the maximum number of classrooms will be 19.~~
11. ~~That the maximum number of teachers will be 35. That the maximum number of administrative and clerical personnel (other than teachers) will be 5.~~

As noted above, the change in the number of student stations will allow True North to meet the needs of students and families that wish to enroll, within the capacity of the site to handle those students as documented in the traffic study. The expansion of the hours of operation is being requested in order to accommodate before-school and after-school activities, which will be used by only a portion of the school enrollment and will help disperse student pickups and dropoffs through a longer period. They are also requesting that Conditions 10 and 11 be deleted, to provide the same flexibility afforded to other public schools to direct the number of classrooms and teachers as may be appropriate to meet the educational needs of the children, subject to the requirements of the State Requirements for Educational Facilities ("SREF") standards that apply to all such schools. The Applicant is further requesting that the reference to the approved site plan in Condition #2 be modified to reflect the updated site plan submitted with this Application.

Based on the forgoing, we respectfully your favorable consideration of this application. Should you have any questions or concerns, please do not hesitate to call me at my direct line at (305) 789-7758.

Respectfully submitted,

HOLLAND & KNIGHT LLP



Alan S. Krischer

PINECREST

RESOLUTION NO. 4-ZAB-68-79

The following resolution was offered by Mr. Murray Sisselman, seconded by Mrs. Lillian Dickmon, and upon poll of members present, the vote was as follows:

Thelma Damewood	aye	Peter Goldring	aye
Jose A. Losa	aye	Margaret C. Nelson	aye
Lillian Dickmon	aye	Betty S. Page	absent
Jollivette Frazier	aye	Murray Sisselman	aye
Edward G. Coll, Jr.	aye		

WHEREAS, BETH DAVID HOLDING CORPORATION has applied for the following:

(1) Modification of condition #2 of Resolution 4-ZAB-90-76 passed and adopted by the Zoning Appeals Board on the 5th day of February, 1976 and reading as follows:

From: That in the approval of the plan, the same be substantially in accordance with that submitted for the hearing entitled "Addition to Beth David, South Florida" as prepared by Bert Saul P.E., Consulting Engineer, Inc., and dated 10-18-74.

To: That in the approval of the plan, the same be substantially in accordance with that submitted for the hearing entitled "Temple Beth David (South Campus)" as prepared by Leff & Alexander, Architects and Planners and dated November 22, 1978.

Purpose of the request is to substitute plans including additional property and day care facility.

- (2) SPECIAL EXCEPTION to permit the expansion of an existing school.
- (3) UNUSUAL USE to permit a day care center.
- (4) NON-USE VARIANCES as indicated on plans as applied to setbacks, off-street parking requirements, number of children permitted and use of trailer.

Plans of the proposed school and day care facility are on file and may be examined in the Zoning Department entitled "Temple Beth David (South Campus)" as prepared by Leff & Alexander, Architects and dated November 22, 1978.

SUBJECT PROPERTY: Lots 9, 10, 11, 12 & 13, Block 2, 2nd ADDITION TO HOLIDAY ESTATES, Plat book 62, Page 23.

LOCATION: 7500 S.W. 120 Street, Dade County, Florida.

WHEREAS, public hearing of the Metropolitan Dade County Zoning Appeals Board was advertised and held, as required by law, and all interested parties concerned in the matter were heard, and

WHEREAS, upon due and proper consideration having been given to the matter, it is the opinion of this Board that the requested Modification, the requested Special Exception, and the requested Unusual Use would be compatible with the neighborhood and area concerned and would not be in conflict with the

PINECREST

principles and intent of the plan for the development of Dade County, and that the requested Non-Use Variance would be in harmony with the general purpose and intent of the regulations and would conform with the requirements and intent of the Zoning Procedure Ordinance; however, the request for a trailer would not be compatible with the neighborhood and area concerned and would be in conflict with the principles and intent of the plan for the development of Dade County;

NOW THEREFORE BE IT RESOLVED by the Metropolitan Dade County Zoning Appeals that the requested Non-Use Variance for use of a trailer be and the same is hereby denied without prejudice;

BE IT FURTHER RESOLVED that the requested Modification of Condition #2 of Resolution No. 4-ZAB-90-76, and the requested Special Exception to permit the expansion of the existing school, and the requested Unusual Use to permit a day care center, and the requested Non-Use Variances (other than the use of the trailer) be and the same are hereby approved, subject to the following conditions:

1. That a detailed plot use plan be submitted to and meet with the approval of the Zoning Director; said plan shall be drawn to scale and shall include dimensions to indicate lot size, distances from center line of streets to property lines, size and location of building or buildings; said plan shall also include among other things, but not limited thereto, off-street parking areas and driveways, walls, hedges and fences, landscaping, etc. The plot plan shall contain a title block identifying the project, the name and title of the person preparing the plan, the date of preparation of the plan, and the scale of the drawing.
2. That the proposed structure, or the addition to the existing structure, be of a residential type and character and meet with the approval of the Zoning Director.
3. That the driveway to the existing residence for the day nursery on S.W. 77 Avenue be closed and that area be appropriately landscaped.
4. That the use be made to conform to the requirements and/or recommendations of the Dade County Fire Chief and Dade County Department of Public Health and State Welfare Department.
5. That the use shall be established and maintained in accordance with the approved plan.
6. That the use shall be restricted to a maximum of 325 children.
7. That the hours of operation shall be from 8:00 a.m. to 4:30 p.m.

MINUTES

8. That a play area, if any, shall be enclosed with a fence of a type and at a location to be approved by the Zoning Director.
9. That the number of grades will be from Kindergarten to sixth grade.
10. That the maximum number of classrooms will be 19.
11. That the maximum number of teachers will be 35. That the maximum number of administrative and clerical personnel (other than teachers) will be 5.
12. That no new or additional signs shall be permitted.
13. That no transportation vehicles shall be stored on the premises at any time.
14. That the use shall be approved for pre-school, day nursery, elementary school and after school care.
15. That the Certificate of Use and Occupancy be automatically renewable annually by the Dade County Building and Zoning Department upon compliance with all terms and conditions, and be subject to cancellation upon violation of any of the conditions or when, in the opinion of the Metropolitan Dade County Zoning Appeals Board, after public hearing, it is determined that the use is detrimental and/or incompatible to the surrounding neighborhood.

The Zoning Director is hereby directed to make the necessary notations upon the maps and records of the Dade County Building and Zoning Department, and to issue all permits in accordance with the terms and conditions of this Resolution.

PASSED AND ADOPTED this 16th day of February, 1979.

Heard 2/16/79  
Hearing No. 79-2-28  
2/22/79  
ns

PINECAST

bcc: Hearing File  
Tax Assessor

04.21.79

February 20, 1979

Mr. Sam Badanca, President  
Beth David Holding Corp.,  
a non-profit Florida Corporation  
2625 S. W. 3rd Street Apt. 2  
Miami, Florida 33131 33129

*Application address wrong  
called & re-mailed*

Re: Hearing No. 79-2-28

Request: MODIFICATION, SPECIAL EXCEPTION, UNUSUAL USE and  
NON-USE VARIANCE.

Dear Mr. Badanca:

Enclosed herewith is a copy of Resolution No. 4-ZAB-68-79 adopted by  
the Metropolitan Dade County Zoning Appeals Board, concerning your  
application in reference to the above subject matter.

You will note that a portion of said application is denied and that a  
portion is approved. If said approval was with conditions, please  
note same carefully, inasmuch as strict compliance therewith will be  
required.

If you are not satisfied with the decision of the Zoning Appeals  
Board, you may appeal the same within 14 days; and you are hereby  
advised that the decision of the Zoning Appeals Board may be appealed  
by an aggrieved party (within 14 days) or by the Directors of the Dade  
County Building and Zoning Department and Planning Department (within  
14 days), as is provided in Chapter 33-313 of the Code of Metropolitan  
Dade County, Florida; and that no permits or Certificate of Use and  
Occupancy can be issued until the appeal periods have expired, and  
only if no appeal has been filed. Application for necessary permits  
and/or Certificate of Use and Occupancy should be made with this  
Department. The deadline for an appeal by the applicant and/or an  
aggrieved party is MARCH 8, 1979.

Very truly yours,

Chester C. Czebrinski  
Assistant Director

CCC:ms

Enclosure: Resolution No. 4-ZAB-68-79.

cc: Enforcement Division  
Dade County Fire Chief  
Dade County Department of Public Health  
State Welfare Department  
Enforcement Division

PRECEDENT

85-702/14-55-40

RESOLUTION NO. Z-114-86

PRECEDENT

The following resolution was offered by Commissioner Stephen P. Clark, seconded by Commissioner Barry D. Schreiber, and upon poll of members present the vote was as follows:

Barbara M. Carey	absent	Barry D. Schreiber	aye
Clara Oesterle	aye	Sherman S. Winn	aye
Beverly B. Phillips	aye	Jorge (George) Valdes	absent
James F. Redford, Jr.	aye	Stephen P. Clark	aye
Harvey Ruvin	aye		

WHEREAS, BET SHIRA CONGREGATION, INC., had applied for the following:

- (1) MODIFICATION of Condition #2 of Resolution 4-ZAB-90-76, passed and adopted by the Zoning Appeals Board on the 5<sup>th</sup> day of February, 1976, as further modified by Resolution 4-ZAB-68-79 on the 16<sup>th</sup> day of February, 1979, as follows:

FROM: "2. That in the approval of the plan, the same be substantially in accordance with that submitted for the hearing entitled 'Temple Beth David (South Campus)' as prepared by Leiff & Alexander, Architects and Planners and dated November 22, 1978."

TO: "2. That in the approval of the plan, the same be substantially in accordance with that submitted for the hearing entitled 'Bet Shira Congregation', as prepared by Zyscovich & Grafton Architects, and dated 10-3-85."

The purpose of the request is to permit the applicants to submit revised site plans for the existing synagogue and school, to show a new multi-purpose building to function as their sanctuary, fellowship hall and to include administrative offices. The new plans show an additional 2 lots to the rear to be used as the school and day care's outdoor recreational area, as well as to serve as a grassed area for parking when necessary.

- (2) SPECIAL EXCEPTION to permit the expansion of an existing synagogue, day care center, and elementary school onto additional property to the south.
- (3) NON-USE VARIANCE OF PARKING REGULATIONS to permit 411 parking spaces (1,558 parking spaces required).
- (4) NON-USE VARIANCE OF PARKING REGULATIONS to permit 241 of the aforementioned spaces to be on natural terrain (not permitted).
- (5) NON-USE VARIANCE OF SETBACK REQUIREMENTS for a building of public assemblage to permit the synagogue to be setback 25' (50' required) from the front (north) property line and 61.82' (75' required) from the rear (south) property line.
- (6) MODIFICATION of Conditions 6, 7, and 10 of Resolution 4-ZAB-68-79, passed and adopted by the Zoning Appeals Board on the 16<sup>th</sup> day of February, 1979 as follows:

FROM: "6. That the use shall be restricted to a maximum of 325 children."

TO: "6. That the use shall be restricted to a maximum of 400 children."

FROM: "7. That the hours of operation shall be from 8:00 a.m. to 4:30 p.m."

TO: "7. That the hours of operation shall be from 8:00 a.m. to 5:00 p.m."

FROM: "10. That the maximum number of classrooms will be 19."

TO: "10. That the maximum number of classrooms will be 30."

P. A. C. O. N. G. R. E. G. A. T. I. O. N.

BET SHIRA

Page Two

14-55-40

W. 120 ST.

The purpose of the above request is to modify previously imposed conditions for the day care center and school to allow an increase in the overall number of students and classrooms as well as a half-hour extension on the hours of operation.

The aforementioned plans are on file and may be examined in the Zoning Department.

SUBJECT PROPERTY: Lots 9 through 13 inclusive, Block 2 of SECOND ADDITION TO HOLIDAY ESTATES, Plat book 62, Page 23.

AND:

Lots 5 and 6, Block 2 of FIRST ADDITION TO HOLIDAY ESTATES, Plat book 55, Page 101.

LOCATION: The Southeast corner of S.W. 120 Street and S.W. 77 Avenue, Dade County, Florida, and

WHEREAS, a public hearing of the Metropolitan Dade County Zoning Appeals Board was advertised and held, as required by law, and all interested parties concerned in the matter were heard, at which time the attorney for the applicant requested withdrawal of Item #4 (pertaining to parking on natural terrain) and upon due and proper consideration having been given to the matter, it was the opinion of the Zoning Appeals Board that the requested modifications, special exception and non-use variances would not be in harmony with the general purpose and intent of the regulations, would not be compatible with the area and its development and would not conform with the requirements and intent of the Zoning Procedure Ordinance and Items #1, #2, #3, #5 and #6 were denied without prejudice, and the withdrawal of Item #4 was permitted;

WHEREAS, BET SHIRA CONGREGATION, the applicant, appealed the decision of the Zoning Appeals Board to this Board, and after a 15-day notice of the time and place of the meeting of this Board was published as required by the Zoning Procedure Ordinance a hearing was held by this Board, at which time the applicant requested a deferment of the hearing to permit a modification and re-advertisement of the application and after reviewing the record and decision of the Zoning Appeals Board and after having given an opportunity for interested parties to be heard, it is the opinion of this Board that the grounds and reasons specified for the reversal of the ruling made by the Zoning Appeals Board were sufficient to merit a reversal of the decision in consideration of the modification and readvertisement of of the application as follows:

- (1) MODIFICATION of Condition #2 of Resolution 4-ZAB-90-76, passed and adopted by the Zoning Appeals Board on the 5<sup>th</sup> day of February, 1976, as further modified by Resolution 4-ZAB-68-79 on the 16<sup>th</sup> day of February, 1979, as follows:

REC'D

1986

FROM: "2. That in the approval of the plan, the same be substantially in accordance with that submitted for the hearing entitled 'Temple Beth David (South Campus)' as prepared by Leiff & Alexander, Architects and Planners and dated November 22, 1978."

TO: "2. That in the approval of the plan, the same be substantially in accordance with that submitted for the hearing entitled 'Bet Shira Congregation', as prepared by Zyscovich & Grafton Architects, and dated 3-27-86."

The purpose of the request is to permit the applicants to submit revised site plans for the existing synagogue and school, to show a new multi-purpose building to function as their sanctuary, fellowship hall and to include administrative offices.

- (2) SPECIAL EXCEPTION to permit the expansion of an existing synagogue.
- (3) NON-USE VARIANCE OF PARKING REGULATIONS to permit 158 parking spaces (1,551 parking spaces required).
- (4) NON-USE VARIANCE OF SETBACK REQUIREMENTS for a building of public assemblage to permit the synagogue to be setback 25' (50' required) from the front (north) property line.
- (5) NON-USE VARIANCE OF ZONING REGULATIONS as applied to private schools to permit 49,972 square feet of outdoor playground area (82,450 square feet required).

The aforementioned plans are on file and may be examined in the Zoning Department.

SUBJECT PROPERTY: Lots 9 through 13 inclusive, Block 2 of SECOND ADDITION TO HOLIDAY ESTATES, Plat book 62, Page 23.

LOCATION: The Southeast corner S.W. 120 Street and S.W. 77 Avenue, and

NOW THEREFORE BE IT RESOLVED by the Board of County Commissioners, Dade County, Florida, that the decision of the Zoning Appeals Board be and the same is hereby overruled and the requested application as modified and readvertised be and the same is hereby approved, subject to the following condition:

That the applicant submit to the Planning Department for its review and approval a landscaping plan which indicates the type of plant material and size prior to the issuance of a building permit and to be installed prior to the issuance of a certificate of use and occupancy.

The Zoning Director is hereby directed to make the necessary changes and notations upon the maps and records of the Dade County Building and Zoning Department and to issue all permits in accordance with the terms and condition of this resolution.

PASSED AND ADOPTED this 17th day of April, 1986.

April, 1986  
No. 86-1-16  
mr  
5/29/86

DADE COUNTY, FLORIDA, BY ITS  
BOARD OF COUNTY COMMISSIONERS  
Richard P. Brinker, Clerk

By \_\_\_\_\_  
Deputy Clerk

This resolution transmitted to the Clerk of the Board of County Commissioners on the 6th day of June 1986.

PINECREST

04.21.86

June 6, 1986

Bet Shira Congregation  
c/o Stanley Price, Esq.  
2401 Douglas Road  
Miami, FL 33145

Re: Hearing No. 86-1-36; The Southeast corner S.W. 120 St. & S.W. 77 Ave.

Gentlemen:

Enclosed, herewith, is a copy of resolution No. Z-114-86, adopted by the Board of County Commission which overruled the decision of the Zoning Appeals Board and approved, as modified and re-advertised, your requested application on the above-described property. Please note the condition under which said approval was granted, inasmuch as strict compliance, therewith, will be required.

You are, hereby, advised that the decision of the Dade County Commission may be appealed by an aggrieved party within 30 days of the date of the submittal of the resolution to the Clerk of the County Commission. You are, further, advised that in the event that an appropriate appeal is timely filed in the Circuit Court, any building permit sought or obtained shall be solely at the risk of the party obtaining said permit.

Very truly yours,

Chester C. Czebrinski  
Assistant Director

CCC/mr

Enclosure

# Holland & Knight

701 Brickell Avenue, Suite 3300 | Miami, FL 33131 | T 305.789.7413 | F 305.789.7799  
Holland & Knight LLP | www.hklaw.com

Pincrest

11503  
Alan S. Krischer  
(305) 789-7758  
alan.krischer@hklaw.com

November 15, 2023

## VIA HAND DELIVERY

Mr. Stephen Olmstead  
Planning Director  
Village of Pinecrest  
12645 Pinecrest Parkway  
Pinecrest, Florida 33156

**Re: First Amended Letter of Intent / Bet Shira Congregation, Inc.**

Dear Mr. Olmstead:

This shall constitute the First Letter of Intent on behalf of Bet Shira Congregation, Inc. (the "Applicant") in connection with their pending application for a modification of a previously approved zoning resolution (the "Application"). As indicated in our original submittal, the purpose of the Application is to request the modification of Resolution 4-ZAB-68-79 as later modified by Resolution Z-114-86, in order to permit the expansion of the True North public charter school currently operating on the subject property. The subject property is located on that certain parcel of land, consisting of approximately ±1.32 acres, located at 7500 SW 120th Street, Pinecrest, Florida. This parcel is further identified by Miami-Dade County Property Appraiser Folio No. 20-5014-004-0050 (the "Property"). The boundaries of the Property are generally depicted on the aerial below.



# PINECREST

In the course of processing the submitted materials, the Village provided the Applicant with a comment letter requesting revisions, additional information, and further explanation of various aspects of the requests. A copy of that letter is attached for your convenience. Many of the requests for comments and additional information related specifically to the site plans and the traffic study that accompanied the application. With this First Amended Letter of Intent, the Applicant is submitting a set of revised site plans and a revised traffic study responsive to those comments, as well as comment response letters prepared by our architect and traffic engineers outlining the specific responses to each comment as appropriate.

Those response letters detail the modifications that have been made to the site plan, but the most significant changes are the proposed redesign of the traffic circulation. Based upon suggestions from both the Village's traffic consultant and the Miami-Dade County Traffic and Engineering Department, the Applicant is proposing to modify the site layout to eliminate one of the driveway access points on S.W. 120<sup>th</sup> Street, and to proposed an additional entrance on S.W. 77<sup>th</sup> Avenue. These measures, along with right-of-way improvements in both those roadways, will enhance the accessibility and circulation for motor vehicles into and within the site.

As noted in our initial submittal, the Property is currently developed with the Congregation Bet Shira religious facility, a portion of which is being operated as an existing public charter school by True North Classical Academy, Inc. ("True North") under an agreement with the Applicant. The pending Application requests an expansion of the number of student stations from the 325 students presently permitted under the prior Resolutions. Under this proposal, the total number of students would be 100 students in the pre-school/daycare operated by the Congregation, and 600 students in the K-8 school operated by True North, for a total of 700 students altogether.

Accordingly, the Applicant is requesting modifications to the various conditions of the 1979 Resolution, as later modified by Resolution 4-ZAB-131-80 to accommodate their proposed public charter school operations, as follows:

6. That the use shall be restricted to a maximum of ~~325~~ 700 children
7. That the hours of operation shall be from ~~8:00~~ 7:15 a.m. to ~~4:30~~ 5:30 p.m.
8. That a play area, if any, shall be enclosed with a fence of a type and at a location to be approved by the Zoning Director.
9. That the number of grades will be from Kindergarten to eighth grade.
10. ~~That the maximum number of classrooms will be 19.~~
11. ~~That the maximum number of teachers will be 35. That the maximum number of administrative and clerical personnel (other than teachers) will be 5.~~

As noted in our prior Letter of Intent, the change in the number of student stations will allow True North to meet the needs of students and families that wish to enroll, within the capacity of the site to handle those students as documented in the traffic study. The expansion of the hours of operation is being requested in order to accommodate before-school and after-school activities, which will be used by only a portion of the school enrollment and will help disperse student pickups and dropoffs through a longer period. They are also requesting that Conditions 10 and 11 be deleted, to provide the same flexibility afforded to other public schools to direct the number of classrooms and teachers as may be appropriate to meet the educational needs of the children, subject to the requirements of the State Requirements for Educational Facilities ("SREF") standards that apply to all such schools. The Applicant is further requesting that the reference to

the approved site plan in Condition #2 be modified to reflect the updated site plan submitted with this Application.

The foregoing requests are consistent with the Village's Comprehensive Development Master Plan, and specifically Goal 1-1 that provides:

GOAL 1-1: LAND USE. The Village of Pinecrest Shall Maintain and Enhance the Extraordinary Character and Quality of Land Uses within the Village by:

- Balancing the Immediate and Long Term Community Interests and Resources;
- Advancing the Aesthetic, Physical, Social, Cultural, and Economic conditions of its Residents;
- Ensuring that the Character and Location of Land Uses Incorporate Best Management Practices and Principles of Resource Conservation;
- Promoting Orderly Land Use Transition; and
- Protecting the Public Health, Safety, Welfare and rural aesthetic and Preventing Threats to Health, Safety, Welfare and rural aesthetic which May Be Caused by Incompatible Land Uses, Environmental Degradation, Hazards, and Nuisances.

The Property is already approved for and improved with a religious and educational institution, and indeed is immediately adjacent to the much more intense educational facility of Miami Palmetto Senior High, so the proposed modification still maintains the same orderly land use transitions that exist today. One of the fundamental needs of any community, and especially one that is largely residential, is to have educational opportunities for the children in the jurisdiction. The proposed modification will more efficiently utilize a property that is already dedicated to institutional uses in order to provide greater educational capacity for the immediate area, without the need to create any additional new sites for institutional uses within the Village.

Policy 1-2.4.1 of the Village's Comprehensive Plan also specifically addresses lands zoned for Public and Semi-Public Services, providing that "Lands designated PS shall contain sufficient acreage and open space and shall be properly screened and buffered in order to minimize potential adverse impacts on adjacent land uses." Policy 1.2.4.2 directs that the intensity of public and semi-public land uses is to be measured and regulated pursuant to maximum floor area ratios (0.40 plus 0.11 per additional story) provided therein. The proposed development is well below that threshold, indicating that the size of the Property is more than adequate to accommodate the proposed intensity of use. The amount of open green space provided is also consistent with the requirements of the Village's Land Development Regulations, indicating that it is sufficient.

Finally, no changes are proposed to the layout of the existing structures or the overall configuration of the site. The classroom buildings continue to be set back well beyond the zoning district requirements from the adjoining residential uses to the south and west, and the new athletic field for the expanded K-8 uses is completely screened from those adjoining residential uses by the "U"-shaped layout of the surrounding buildings – open only to Palmetto Senior High to the north. Finally, as noted in the accompanying architect's response letter there is considerable dense landscape buffering, both existing and proposed to be filled and enhanced by this request, as well as an existing six foot high CBS well between the existing buildings and the adjoining homes.

The proposed Application also fulfills the purpose of the underlying PS zoning district, as described in Div. 4.4 of the Village's Land Development Regulations, in the following manner:

*1. Accommodating the land needs of existing public and semi-public land uses within the village.*

The proposed Application does not diminish or reduce the amount of lands available for existing public and semi-public land uses within the Village. The proposed land uses are core semi-public services, specifically educational institutions and places of worship and accessory facilities.

*2. Provision, maintenance, and conservation of quality public services required to meet the needs of existing and projected future population magnitude, distribution, and characteristics.*

The proposed Application will provide, maintain, and conserve quality public services to meet the needs of the residents of the Village. Temple Bet Shira has been a valuable and integral part of the Pinecrest community for decades, tending to the spiritual and educational needs of its congregants. True North is a longstanding and respected educational institution with established facilities throughout the Miami-Dade area, and will provide a quality educational service for their students.

*3. Protecting established residential areas from encroachment by non-residential development.*

Since the proposed True North facilities are being proposed for lands that are already zoned and used for non-residential uses, the Application does not create any encroachment of non-residential development into the residential areas. The Property has been used for non-residential uses for many decades.

*4. Ensuring compatibility with adjacent land uses to the greatest practical extent by providing landscaping and buffer areas to protect adjacent residential development from the off-site impacts of public and semi-public land uses.*

As noted above, no changes are being proposed to the existing structures, and the existing semi-public uses will continue to be separated by as large a buffer area and landscaped area, as well as the six foot high CBS wall, as they are today.

*5. Providing and maintaining of transportation and infrastructure system improvements.*

The proposed site plan and accompanying traffic analysis outline the existing and proposed transportation improvements that are intended to improve vehicular access and circulation within the site. The addition of an additional driveway access and deceleration lane on S.W. 77<sup>th</sup> Avenue, and the simplification and consolidation of the driveways on S.W. 120<sup>th</sup> Street, will greatly facilitate traffic movements on the surrounding roadways.

6. *The public and semi-public service (PS) zoning district is intended to accommodate existing public and semi-public services including: governmental administration buildings; public schools and not-for-profit educational institutions; arts and cultural or civic facilities; essential public services, facilities, and utilities; fire and emergency operation facilities; places of worship; and accessory facilities; and other similar activities as identified in the land development code.*

The existing and proposed uses are fully consistent with the intended purpose of the PS zoning district, consisting of a longstanding place of worship and accessory facilities and a not-for-profit educational institution.

Please find accompanying this First Amended Letter of Intent the following documents:

- Architectural Response Letter prepared by Civica Architecture
- Landscape Response Letter prepared by Gardner + Semler Landscape Architecture
- Revised Site Plan (Rev-1) prepared by Civica Architecture
- Comment Response Letter and revised Traffic Impact Study prepared by Langan Engineering

Based on the foregoing, the Applicant continues to believe that the proposed requests will enable the provision of additional excellent education resources to the citizens of Pinecrest within the longstanding Bet Shira institution. They believe that the proposal represents an opportunity to take an underutilized asset of the Village of Pinecrest community and strengthen the Bet Shira Congregation while providing greater educational opportunities to the Village's residents. We continue respectfully your favorable consideration of this application. Should you have any questions or concerns, or require any additional information in response to your comments, please do not hesitate to call me at my direct line at (305) 789-7758.

Respectfully submitted,

HOLLAND & KNIGHT LLP



Alan S. Krischer

# Holland & Knight

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Alan S. Krischer  
(305) 789-7758  
alan.krischer@hklaw.com

September 4, 2024

## **VIA HAND DELIVERY**

Mr. Stephen Olmstead  
Planning Director  
Village of Pinecrest  
12645 Pinecrest Parkway  
Pinecrest, Florida 33156

**Re: First Amended Letter of Intent / Bet Shira Congregation, Inc.**

Dear Mr. Olmstead:

This shall constitute the First Amended Letter of Intent on behalf of Bet Shira Congregation, Inc. (the "Applicant") in connection with their application for a modification of a previously approved zoning resolution (the "Application"). As indicated in our initial letter of intent the purpose of the Application is to request the modification of Resolution 4-ZAB-68-79 as later modified by Resolution Z-114-86, in order to permit the expansion of the True North public charter school currently operating on the subject property (the "Property").

After the initial submittal of the Application, receipt of comments and submittal of a revised set of plans, we received a further round of comments from the Village and its traffic consultants reviewing the materials. Accompanying this letter are a further set of revised plans and a revised traffic analysis, both responsive to the comments raised by the Village and reflecting changes to the proposed development – particularly ingress and egress and the adjoining transportation systems – to address the concerns raised therein. Also accompanying this letter is a comment response document prepared by the Applicant's architect, explaining the changes that have been made in response to each of the individual comments provided by the Village. This Amended Letter of Intent provides additional information to assist in the review of the Application.

## **Total Enrollment and Allocation**

As noted in our original submittal, a portion of the Property is currently being used by True North to operate a public charter school. Temple Bet Shira also operates a smaller day care/infant care/after school program. Among the requests in the Application are amendments to the prior resolution to increase the number of students permitted on the site, and the Village's comments asked for clarity on the requested student count. The request is to increase the number of students

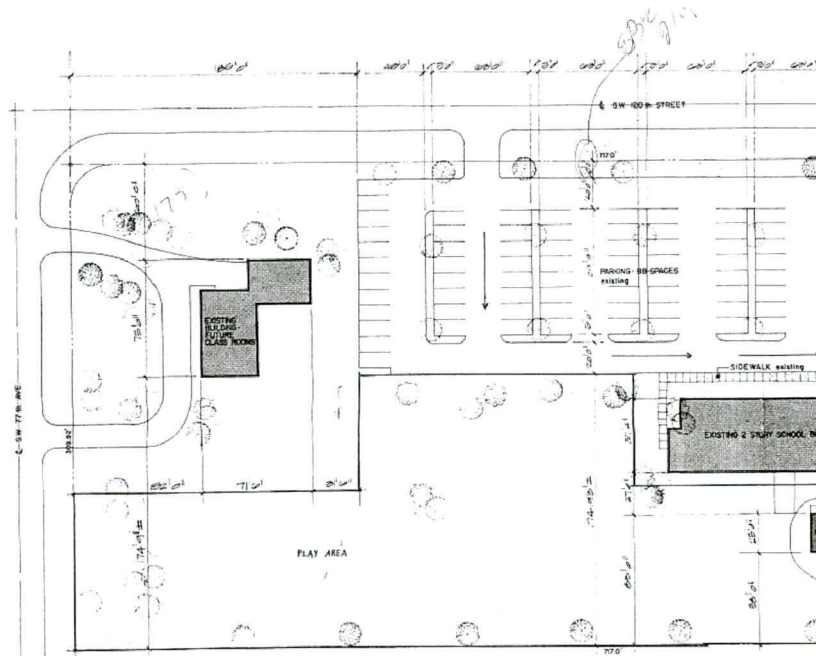
from the 325 currently permitted to a total of 600 students, which total of 600 students reflects **both** the enrollment in the True North public charter and the Temple's child care programs. While the allocation of that total number of students may vary from time to time based on the enrollment of both facilities, at no time will the number of children in the Temple's program exceed 100 of the total 600. The revised traffic study and site plans analyze the requirements and impacts of this total combined enrollment of up to 600 children.

### 1979 Resolution Driveway Condition

In the course of responding to the Village's prior comments, the Applicant has revised the ingress and egress design to improve the traffic flow within and around the Property. As part of that revised traffic circulation, the Applicant is proposing improvements to SW 77<sup>th</sup> Avenue and an additional driveway in the southwestern corner of the property. In the last round of comments, the Village asked us to address condition number 3 of the 1979 Resolution, which provided that:

“The driveway to the existing residence for the day nursery on SW 77<sup>th</sup> be closed and that the area be appropriately landscaped.”

After reviewing the zoning file for the 1979 Resolution and the accompanying approved site plan, the Applicant believes that the above condition has already been fully satisfied. At the time, the portion of the Property addressed by this condition was an existing single family home, which apparently had been acquired by the temple. The proposal was to keep the existing single family home building in place, and simply use that existing structure for the day nursery, as shown in the below detail from the approved plan:



The then-existing driveway was designed for a single-family home, not for the proposed new use of the structure as a day nursery – particularly given the proximity of the northern access to the intersection of SW 77<sup>th</sup> Avenue and SW 120<sup>th</sup> Street. Since the single-family driveway was

not designed for an institutional use, it was appropriate to remove that existing driveway. Of course, not only was the then-existing driveway removed, the single-family structure has also long since been replaced by the Temple facility.

With the removal of the then-existing driveway, the condition was long ago satisfied. The condition does not prohibit future connections to 77<sup>th</sup> Avenue, and there is nothing in the zoning file to indicate that the purpose was to prohibit such future connections: merely to remove the then-existing driveway that was not designed for the proposed new use. The condition was not intended to prohibit the temple and school use from having a suitably-designed driveway connection to SW 77th Avenue, merely the elimination of the vestigial single-family driveway. The Applicant does not believe that the condition has any bearing on the new proposal; but in order to avoid confusion, requests that the condition be deleted as part of the public hearing.

**Updated Requested Conditions**

The remaining comments in the Village's last correspondence have been primarily addressed in the revised plans and traffic analysis, which changes are further described in the comment responses accompanying both of those submittals. In addition to the information discussed above, as noted in the architectural response the Applicant is requesting that the aftercare programs be permitted to continue until 6:00 p.m. In addition to tying the development of the Property to the revised site plan, we have compiled the request changes to the conditions of the 1979 Resolution (as later amended) below:

**FROM:**

3. The driveway to the existing residence for the day nursery on SW 77th be closed and that the area be appropriately landscaped.

\* \* \*

6. That the use shall be restricted to a maximum of 325 children.
7. That the hours of operation shall be from 8:00 a.m. to 4:30 p.m.
8. That a play area, if any, shall be enclosed with a fence of a type and at a location to be approved by the Zoning Director.

\* \* \*

10. That the maximum number of classrooms will be 19.
11. That the maximum number of teachers will be 35. That the maximum number of administrative and clerical personnel (other than teachers) will be 5.

**TO:**

3. ~~The driveway to the existing residence for the day nursery on SW 77th be closed and that the area be appropriately landscaped.~~

\* \* \*

- 6. That the use shall be restricted to a maximum of ~~325~~ 600 children
- 7. That the hours of operation shall be from ~~8:00~~ 7:15 a.m. to ~~4:30~~ 6:00 p.m.
- 8. That a play area, if any, shall be enclosed with a fence of a type and at a location to be approved by the Zoning Director.

\* \* \*

- ~~10. That the maximum number of classrooms will be 19.~~
- ~~11. That the maximum number of teachers will be 35. That the maximum number of administrative and clerical personnel (other than teachers) will be 5.~~

In addition to the foregoing changes, the Applicant suggests adopting two additional conditions. The first would provide for the increase in student capacity to be phased in over time. As we have indicated in our discussions with staff, the Applicant does not anticipate that enrollment will expand immediately to the full 600 children if the request is approved. Rather, the enrollment will rise over time over successive school years. To memorialize that, the Applicant suggests that there be a condition phasing in the enrollment over time, in accordance with the chart below. Moreover, the Applicant suggests that once the enrollment has reached 500 students (expected to take place in the 2027-2028 school year), approval of the final increase of 100 students will be subject to the Village’s professional staff administrative review of an updated traffic study reflecting then-current conditions.

School Year 25-26	400	
School Year 26-27	460	
School Year 27-28	500	
School Year 28-29	600	Subject to administrative review of updated traffic study

The second proposed condition would establish a preference for admissions for students who live within the geographic boundaries of the Village of Pinecrest. This condition would help ensure that the educational resources of the charter school are a benefit to the Village and continues to be an important part of the community fabric.

As noted in our original submittal, the change in the number of student stations will allow True North to meet the needs of students and families that wish to enroll, within the capacity of the site to handle those students as documented in the traffic study. The expansion of the hours of operation is being requested in order to accommodate before-school and after-school activities, which will be used by only a portion of the school enrollment and will help disperse student pickups and dropoffs through a longer period. Deletion of Conditions 10 and 11 will provide the same flexibility afforded to other public schools to direct the number of classrooms and teachers as may be appropriate to meet the educational needs of the children. The Applicant continues to request that the reference to the approved site plan in Condition #2 be modified to reflect the updated site plan submitted with this Application.

Based on the forgoing, we respectfully your continued favorable consideration of this application. Should you have any questions or concerns, please do not hesitate to call me at my direct line at (305) 789-7758.

Respectfully submitted,

A handwritten signature in blue ink, appearing to read 'AK', with a long horizontal flourish extending to the right.

HOLLAND & KNIGHT LLP

Alan S. Krischer

# Holland & Knight

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Alan S. Krischer  
(305) 789-7758  
alan.krischer@hklaw.com

January 10, 2025

## VIA HAND DELIVERY

Mr. Stephen Olmstead  
Planning Director  
Village of Pinecrest  
12645 Pinecrest Parkway  
Pinecrest, Florida 33156

**Re: Submittal of Revised Plans and Comment Response / Bet Shira Congregation, Inc.**

Dear Mr. Olmstead:

We have received your comments regarding the last submittal of Bet Shira Congregation, Inc. (the "Applicant") in connection with their application for a modification of a previously approved zoning resolution (the "Application"). As indicated in our initial letter of intent the purpose of the Application is to request the modification of Resolution 4-ZAB-68-79 as later modified by Resolution Z-114-86, in order to permit the expansion of the True North public charter school currently operating on the subject property (the "Property").

As we discussed, in our meeting the other day, accompanying this letter are a further set of revised plans and a revised traffic analysis. These revised plans are both responsive to the comments raised by the Village in the written comments and the suggestions that were raised by the various representatives of the Village and their traffic consultants in our meeting. Also accompanying this letter is a comment response document prepared by the Applicant's architect, explaining the changes that have been made to the plans in response to each of the individual comments provided by the Village. In addition to those responses, this letter provides additional information in response to selected comments.

With respect to Comment #5, the Applicant is requesting an increase in enrollment to no more than six hundred (600) total students in the combined enrollment of both the charter school and the existing Temple school program. Within that 600 students, the total enrollment of the Temple's school program will never exceed 100 students. The allocation of those 600 students between the K-8 charter school and the Temple's school program may vary; if, for example, the Temple only enrolled 50 students, then the K-8 charter school might enroll up to 550 students.

Based on the forgoing, we respectfully your continued favorable consideration of this application. Should you have any questions or concerns, please do not hesitate to call me at my direct line at (305) 789-7758.

Respectfully submitted,

A handwritten signature in blue ink, appearing to read 'Alan S. Krischer', with a long horizontal flourish extending to the right.

HOLLAND & KNIGHT LLP

Alan S. Krischer



PINECREST

04.21.23

VILLAGE OF PINECREST
Building & Planning Department

PUBLIC HEARING APPLICATION
Administrative Review Application

- Please check one:
[X] VILLAGE COUNCIL
[ ] PLANNING BOARD
[ ] ADMINISTRATIVE REVIEW

OFFICIAL USE ONLY
Application No. \_\_\_\_\_
Date Received: \_\_\_\_\_

INSTRUCTIONS

This application, with all supplemental data and information, must be completed in accordance with the specific instructions in this application. Applications and all supplemental information must be filed no later than 60 days prior to the regular public hearing date.

APPLICATION

Please indicate which type of application you are submitting by checking one category below:

- [ ] Change in Zoning District
[ ] Variance
[ ] Appeal of Decision
[ ] Conditional Use
[ ] Plat
[ ] Entry Feature
[ ] Site Plan
[X] Other Modification of Prior Resolution

IMPORTANT: THE APPLICANT, OR REPRESENTATIVE, MUST BE PRESENT AT THE HEARING TO PRESENT THE PROPOSAL.

Please print or type

Name of Applicant, agent or tenant (with owner's affidavit)
BET SHIRA CONGREGATION INC
Mailing Address: 7500 SW 120 Street
City, State, Zip: Pinecrest, Florida 33156
Telephone: Email
Name of Owner: Same as above
Mailing Address:
City, State, Zip:
Telephone: Email

PROPERTY INFORMATION

A. LEGAL DESCRIPTION. (If subdivided - lot, block, complete name of subdivision, plat book and page numbers).
If metes and bounds description - Complete description, including section, township and range).

Folio Number 20-5014-004-0050

Address 7500 SW 120 Street, Pinecrest, Florida, 33156

Lot(s) 9-13 (less ROW) Block 2 Section Plat Book No. 62 Page No. 23

FINISHED FLOOR ELEVATION (If applicable): FLOOD ZONE

Revised 8/2016

B. ADDRESS (If number has been assigned) 7500 SW 120 Street, Pinecrest, Florida 33156

C. SIZE OF PROPERTY \_\_\_\_\_ ft. X \_\_\_\_\_ ft. = 44,000 sq. ft.; 5.04 acre(s)  
Width Depth

D. Provide legal description or address of any property held by the owner which is contiguous to that which is the subject matter of this application.

None.  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

E. DATE SUBJECT PROPERTY WAS ACQUIRED \_\_\_\_\_

## APPLICANT'S PROPOSAL

Specify in full the request. (Use a separate sheet of paper if necessary.)

Modification of prior zoning resolutions to accommodate proposed charter school - please see accompanying letter of intent for additional details.  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

## INTENT

Explain purpose of application, benefit(s) in the change and reasons why this application should be approved. Specify the exact nature of the use or operation applied for, together with any pertinent technical data, which will clarify the proposal. (Use a separate sheet of paper if necessary.)

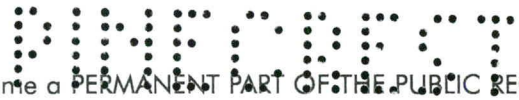
Please see accompanying letter of intent.  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Is this application the result of a Notice of Violation or deviation from approved plans?  Yes  No

Are there any existing structures on the property?  Yes  No

If so, what type? (CBS, Frame, Frame-Stucco, Wood, Other) Existing two-story religious, office and educational buildings

Any applications (except plat applications) which involve new building construction or exterior improvements to an existing building must submit the plans with this application. Plans that are not filed with this application will not be considered by the Village of Pinecrest.



All data and exhibits submitted in connection with this application become a PERMANENT PART OF THE PUBLIC RECORDS OF THE VILLAGE OF PINECREST.

The following enclosures where applicable MUST BE ATTACHED to complete the application:

- A.  **SURVEY OF PROPERTY:** For vacant or improved property. Must be no more than five years old and sealed by a registered land surveyor. The Building and Planning Department may require a more recent survey if a site visit indicates any discrepancies. Survey must include, where applicable, lot lines, all structures, walls, fences, landscaping, and all physical improvements.
- B.  **SITE DEVELOPMENT PLAN:** Where applicable, plans shall show location and elevations of existing and proposed buildings, proposed additions, alterations and use of each; all dimensions of buildings and space between buildings; setbacks from property lines; proposed and existing off-street parking showing lined spaces, driveways, handicap spaces, compact spaces; a landscape plan that complies with Village of Pinecrest Landscape Ordinance showing location of existing and proposed vegetation, landscaping (i.e. trees and hedges), number, height and species type. The plan shall also show wall and fence height, location and material.
- C.  **LETTER OF INTENT:** A letter of intent must be filed explaining in detail the extent of the development. Signature and address must be shown.
- D.  **OTHER GOVERNMENTS/AGENCIES ENDORSEMENTS:** All applicable DERM, Miami-Dade Fire Department, or the Miami-Dade Water and Sewer Department's endorsement must be submitted.
- E.  **OWNER'S AFFIDAVIT:** Owner's affidavit allowing the filing of an application is required on all applications where the applicant is not the owner of the property under consideration; same form allows posting of property.
- F.  **TRAFFIC STUDY:** A detailed traffic analysis considering the impacts of the proposed development on current level-of-service (LOS) standards in abutting (or nearby) roads and intersections.

**NOTE:** SURVEYS, SITE DEVELOPMENT PLANS, LANDSCAPE PLANS MUST BE SUBMITTED AT STANDARD PLAN SIZE AND DRAWN TO AN ENGINEERING OR ARCHITECTURAL SCALE (STRUCTURES ONLY). AN APPLICATION WILL NOT BE CONSIDERED COMPLETE UNLESS 10 COPIES OF THE APPLICATION AND SUPPORTING DOCUMENTATION (FRONT AND BACK), APPROPRIATE PLAN DRAWINGS AND SURVEYS ARE SUBMITTED. **UNLESS DETERMINED OTHERWISE BY THE PLANNING DIRECTOR, ALL PRESENTATION GRAPHICS MUST BE IN POWERPOINT OR OTHER DIGITAL FORMAT.**

In support of this request, I submit the following additional items, which are attached hereto and made a part of this application:

- 35 MM Photo(s) (Mounted 8 1/2 x 11)
- Letters from Area Residents
- Other \_\_\_\_\_

Please check only one of the following options:

**FOR VILLAGE COUNCIL PURPOSES**  
 I/We understand that additional public hearings may be required and any interested person may discuss the application with Village staff to the same extent as the applicant. The application may change during the hearing process and additional public notices may affect the schedule of the hearings. If my/our appeal is denied, I/we must file an appeal to the Circuit Court within 30 days of the meeting.

**FOR ZONING BOARD PURPOSES**  
 I/We understand that additional public hearings may be required and any interested person may discuss the application with Village staff to the same extent as applicant. The application may change during the hearing process and additional public notices may affect the schedule of the hearings. If my/our appeal is denied, I/we must file an appeal with the Village Clerk within 14 days of the decision.

4/9/23 / Steven Goldstein / Steven Goldstein, President  
 Date Applicant's Signature Print Name

\_\_\_\_\_/\_\_\_\_\_  
 Date Applicant's Signature (if more than one) Print Name

# PUBLIC HEARING APPLICATION SUPPLEMENT

**OPTIONAL**  
 However, applicants are encouraged to contact neighbors regarding application.

\_\_\_\_\_ has applied to the Village of Pinetrest for a variance,  
 Name of Applicant  
 which will affect the property located at \_\_\_\_\_ as follows:  
 Property Address

To permit \_\_\_\_\_

The  Village Council  Zoning Board will hold a public hearing on this request. I have read the above requested variance, the applicable plans and understand that I am waiving any objection to the proposed variance and related construction as described above. By subscribing my name below, I hereby certify that I have done so freely and without any duress or misrepresentation on the part of the applicant.

Parcel 1 _____ Owner Name _____ Address _____ Signature _____ Date _____	Parcel 2 _____ Owner Name _____ Address _____ Signature _____ Date _____	Parcel 3 _____ Owner Name _____ Address _____ Signature _____ Date _____
Parcel 4 _____ Owner Name _____ Address _____ Signature _____ Date _____	SUBJECT PROPERTY Please indicate the NORTH direction. (Circle one)  ←    ↑    ↓    →	Parcel 5 _____ Owner Name _____ Address _____ Signature _____ Date _____
STREET / AVENUE / TERRACE / ROAD / COURT		
Parcel 6 _____ Owner Name _____ Address _____ Signature _____ Date _____	Parcel 7 _____ Owner Name _____ Address _____ Signature _____ Date _____	Parcel 8 _____ Owner Name _____ Address _____ Signature _____ Date _____

# pinecrest

I/We BET SHIRA CONGREGATION INC as Owner(s) of Lot(s) 9-13 (less ROW)

Block 2 Section PB/PG 62/23

of property which is located at 7500 SW 120 Street, Pinecrest, Florida 33156 desire to file an application for a public hearing before the  Village Council  Planning Board  Administrative Review, and I/We do understand and agree as follows:


1. That the application for a variance will not be heard unless the applicant is present at the hearing.
2. The property will be posted with a sign, which must not be removed until after the public hearing, at which time the Village staff will remove the sign.
3. That the requirements of the zoning code, Miami-Dade County Ordinances, the Florida Building Code, and other government agencies may affect the scheduling and ability to obtain/issue a permit for the proposal.
4. That the only exceptions to the zoning code are those that have been specified in the written application and any other code or plan issues will be corrected by modifying the plans to comply with the respective codes and ordinances of the Village of Pinecrest or Miami-Dade County ordinances.
5. That the applicant will be responsible for complying with all the conditions and restrictions imposed by the Village Council or Zoning Board in connection with the request and will take the necessary steps to make the request effective if approved by the Village Council or Zoning Board.
6. That it is the responsibility of the applicant to submit a complete application with all of the documents necessary for the Village Council or Zoning Board to hear the applicant's request.
7. That the applicant is responsible for timely submission and accuracy of all items requested on the application.
8. That the applicant is responsible for an additional fee for mailing notice to surrounding property owners and recording of the Development Order as specified by the LDRs.

I/We as the owners of the subject property (check one):

- do hereby authorize Alan S. Krischer to act on my/our behalf as the applicant.
- will on my/our own behalf act as applicant(s), and make application in connection with this request for a public hearing before the Village Council or Zoning Board.

Owner's Name Steven Goldstein Signature Steven Goldstein Date 4/10/23

Owner's Name \_\_\_\_\_ Signature \_\_\_\_\_ Date \_\_\_\_\_

Notary to Owner:  Maria Q Collins  
 Comm. #GG002101  
 Expires: Feb. 25, 2024  
 Bonded thru Anson Notary

Applicant's Name \_\_\_\_\_ Signature [Signature] Date \_\_\_\_\_

Notary to Applicant:

---

# TRAFFIC IMPACT STUDY

For

**True North Classical Academy at Pinecrest  
Pinecrest, Florida**

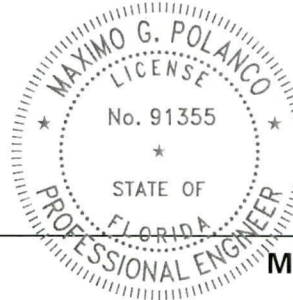
*Prepared For:*  
**True North Classical Academy**  
9393 SW 72nd Street  
Miami, FL 33173

*Prepared By:*  
**Langan Engineering & Environmental Services, Inc.**  
1221 Brickell Avenue, Suite 1800  
Miami, FL 33131  
FL Certificate of Authorization No: 6601

Maximo G Polanco



Digitally signed by Maximo G Polanco  
DN: CN=Maximo G Polanco,  
o=LANGAN ENGINEERING AND ENVIRONMENTAL  
SERVICES INC, CA=US  
Reason: I am approving this document  
Date: 2025.01.09 16:43:04-05'00'



This item has been digitally signed and sealed by Maximo Polanco, PE on the date adjacent to the seal.

Printed copies of this document are not considered signed and sealed and the signature must be verified on any electronic copies.

---

**Maximo G. Polanco, P.E.**  
**P.E. License No. 91355**

A handwritten signature in black ink, appearing to read "E. Schwarz".

---

**Eric Schwarz, P.E., LEED AP**  
**Principal/Vice President**

March 2023  
November 2023  
August 2024

**Revised: December 2024**

---

# LANGAN

**330118701**

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## EXECUTIVE SUMMARY

Langan Engineering and Environmental Services, LLC was retained by True North Classical Academy to prepare a traffic-impact analysis report for the proposed True North Classical Academy at Pinecrest 600-student K-8 Charter School in Pinecrest, Florida. The 5.04-acre site is at 7500 SW 120<sup>th</sup> Street and is occupied by the Bet Shira Congregation, which currently operates a preschool on site. The study shows that the proposed school will have the capacity to accommodate vehicles within its property during student drop-off and pick-up times and will not cause school traffic to queue onto adjacent streets. The intersection capacity analyses show that the study intersections are expected to operate within their adopted Level of Service (LOS). The roadway capacity analysis shows that all roadway segments are expected to operate within their adopted LOS during the morning and afternoon peak hour periods. This report includes morning peak-hour intersection capacity analysis, roadway-capacity analyses, afternoon vehicle-accumulation analysis and a traffic-operations plan.

The site is approved for a PreK-8 private school with a 325-student enrollment and was approved through the zoning application Z85-702 and opened in the summer of 2023 and has a current 280-student enrollment. The proposed school is requesting an increase of 275 students that will increase the maximum enrollment to 600 students and will provide classes from kindergarten through eighth grades. The school is currently open and operating with an enrollment of 280-students operating Monday through Friday from 7:30 AM to 6:00 PM. The proposed school is expected to generate 489 morning peak-hour trips based on the *Institute of Transportation Engineers (ITE) 11<sup>th</sup> Edition Trip Generation Manual*. To provide a conservative analysis, we did not account for the approved school nor the existing preschool for the intersection capacity analysis. Vehicle-accumulation data was collected at Academir Charter School Preparatory (ACSP) K-8 Charter School (surrogate school) to determine the onsite vehicle accumulation of the proposed school. We conducted capacity analyses for the existing, no-build (future without project) and build (future with project) conditions at the following intersections:

- SW 112<sup>th</sup> Street & Palmetto Road (signalized)
- SW 120<sup>th</sup> Street and US-1 (signalized)
- SW 120<sup>th</sup> Street and Palmetto Road (signalized)
- SW 120<sup>th</sup> Street and SW 74<sup>th</sup> Court (unsignalized)
- SW 120<sup>th</sup> Street and SW 74<sup>th</sup> Avenue (unsignalized)
- SW 120<sup>th</sup> Street and SW 72<sup>nd</sup> Avenue (signalized)
- SW 124<sup>th</sup> Street & Palmetto Road (signalized)

The proposed school can accommodate 115 percent of the afternoon onsite vehicle-accumulation with three instructional shifts and will not cause traffic to spillback onto public streets.

### Proposed improvements

Based on the traffic study findings, the proposed school has agreed to construct the following improvements to mitigate the project's impacts:

1. Construct an exclusive eastbound right-turn lane at the existing easternmost driveway connection to SW 120<sup>th</sup> Street.
2. Remove the existing westernmost driveway connection on SW 120<sup>th</sup> Street.
3. Construct an exclusive northbound right-turn lane at the intersection of Palmetto Road and SW 120<sup>th</sup> Street.
4. Reconstruct a portion of the site's parking lot to provide a mini roundabout to allow vehicles to turnaround within the site to facilitate the school's pick-up/drop-off operations.
5. Extend the southbound left-turn lane at the intersection of SW 120<sup>th</sup> Street and Palmetto Road by approximately 100-feet to accommodate existing and expected queues.
6. Update school speed-zone signs and pavement markings along SW 120<sup>th</sup> Street, Palmetto Road, and SW 74<sup>th</sup> Avenue and update the existing school schedule zone flashers along Palmetto Road in accordance with FDOT design standards.
7. Construct curb and gutter along the south side of SW 120<sup>th</sup> Street to restrict the on-street parking.

## 1.0 INTRODUCTION

Langan was retained by True North Classical Academy to prepare a traffic-impact analysis report for the proposed True North Classical Academy at Pinecrest 600-student K-8 Charter School in Pinecrest, Florida. The 5.04-acre site comprises one land parcel (Folio Nos.: 20-5014-004-0050) located at 7500 SW 120<sup>th</sup> Street in Pinecrest, Florida. The proposed school is requesting an increase of 275 students, raising the maximum enrollment to 600 students, and will offer classes from kindergarten through eighth grade. The study shows that the proposed school will have the capacity to accommodate vehicles within its property during student drop-off and pick-up times and will not cause traffic to queue onto abutting public streets. The intersection capacity analyses show that the study intersections are expected to operate within their adopted LOS. This report includes morning peak-hour roadway-capacity analyses, afternoon peak-hour vehicle-accumulation analysis and a traffic-operations plan in accordance with county methodologies for school traffic-impact analyses.

**Appendix A** contains the figures of this report. **Figure 1** illustrates the site location. **Appendix B** contains a copy of the site plan that shows the proposed development program and location of the development's driveways. The site will have access through two driveway connections to SW 120<sup>th</sup> Street. The west driveway (Driveway 1) on SW 120<sup>th</sup> Street will operate as an egress right-turn only driveway for parents during drop-off/pick-up times. The east driveway (Driveway 2) will operate as a right-turn only ingress/egress driveway that serves as the main driveway to the school. However, during arrival and dismissal periods, the driveway will operate as an ingress only driveway. The site was approved for a PreK-8 private school with a 325-student enrollment and was approved through the zoning application Z85-702. Appendix B includes zoning approval for the private school. The school opened in the summer of 2023 and currently has a 280-student enrollment. In addition, currently the Bet Shira Early Childhood Center operates on site with an enrollment of 44-students. This report presents the traffic-data and traffic-impact analysis for this proposed development.

### 1.1 Study Objective

The study's objectives were to analyze the weekday morning peak-hour LOS of the intersections listed in the traffic methodology and determine the number of instructional shifts that will keep the afternoon vehicle accumulation within the school's property.

## 1.2 Study Methodology and Study Area

Langan undertook the following steps to prepare this study in accordance with the methodology reviewed by the Miami Dade County Traffic Engineering Division under application DR2023002086.

- Collected traffic volumes from 7:00 to 9:00 AM at the following intersections (study intersections):
  - SW 112<sup>th</sup> Street & Palmetto Road (signalized)
  - SW 120<sup>th</sup> Street and US-1 (signalized)
  - SW 120<sup>th</sup> Street and Palmetto Road (signalized)
  - SW 120<sup>th</sup> Street and SW 74<sup>th</sup> Court (unsignalized)
  - SW 120<sup>th</sup> Street and SW 74<sup>th</sup> Avenue (unsignalized)
  - SW 120<sup>th</sup> Street and SW 72<sup>nd</sup> Avenue (signalized)
  - SW 124<sup>th</sup> Street & Palmetto Road (signalized)
- Collected automatic traffic recorder (ATR) roadway volumes for a 24-hour period at the following roadway segments (study segments):
  - US-1 from SW 112<sup>th</sup> to SW 124<sup>th</sup> avenues
  - Palmetto Road from SW 112<sup>th</sup> to SW 124<sup>th</sup> avenues
  - SW 120<sup>th</sup> Street between Palmetto Road and SW 74<sup>th</sup> Court
- Identified the existing weekday morning peak-hour traffic volumes based on the collected data.
- Collected afternoon vehicle-accumulation data collected at Academir Charter School Preparatory (ACSP) K-8 Charter School (surrogate school) at 19185 SW 127<sup>th</sup> Ave, Miami, FL 33177.
- Used the 2022 FDOT historical peak-season conversion factor to convert the traffic data into peak-season volumes.
- Calculated a compounded annual growth rate for background traffic by using the morning and afternoon peak hour total volume at the intersection of SW 120<sup>th</sup> Street and Palmetto Road for three (3) consecutive years (2022, 2023, and 2024) to develop 2025 no-build traffic volumes. The growth rate of 3.03% was applied to the existing traffic volumes and committed development trips were added to determine the 2025 no-build traffic volumes.
- Prepared trip-generation estimates for the proposed development, based on accepted trip-generation rates developed by the Institute of Transportation Engineers (ITE).
- Developed trip-distribution estimates for the project, based on the cardinal distribution for the corresponding Traffic Analysis Zone of the Miami-Dade County 2045 Long Range Transportation Plan (LRTP). A computer program used to develop the *2045 LRTP Directional Distribution Report* generates directional distributions for each TAZ for the

eight secondary-intercardinal directions (NNE; ENE; ESE; SSE; SSW; WSW; WNW; NNW).

- Developed 2025 build traffic volumes by adding site-generated trips to the no-build traffic volumes.
- Assigned site-generated trips to the driveways based on the estimated distribution, existing roadway network and likely travel routes motorists will use to and from the site.
- Analyzed the intersection capacity for the weekday morning peak-hour using Synchro software.
- Used the vehicle-accumulation data collected at the school to develop afternoon vehicle-accumulation estimates for the proposed expansion.
- Prepared a traffic operations plan.
- Prepared a 2025 peak-hour directional roadway capacity analysis for the specified segments as requested by the Village of Pinecrest.

**Appendix C** contains a copy of the methodology letter comments provided by Miami-Dade County staff. This report presents the database collected by Langan and the traffic analysis for this development.

### **1.3 School Traffic Operations**

The school is expected to operate with three instructional shifts staggered by 30 minutes. The school will provide access through two driveway connections to SW 120<sup>th</sup> Street. The west driveway (Driveway 1) on SW 120<sup>th</sup> Street will operate as an egress right-turn only driveway for parents during drop-off/pick-up times. The east driveway (Driveway 2) will operate as an all-access driveway that serves as the main driveway to the school. However, during arrival and dismissal periods, the driveway will operate as an ingress only driveway. The passenger vehicle drop-off and pick-up operations will be located on the west side of the building with a dedicated stacking area for safe arrival and departure. School parents will be required to enter the site through the east driveway on SW 120<sup>th</sup> Street to access the dedicated pick-up / drop-off lanes and they will exit using the west driveway connection on SW 120<sup>th</sup> Street. School staff will assist at the stacking areas and will guide traffic during pick-up and drop-off times. It should be noted that the school is proposing to reconstruct a portion of the site's parking lot to provide a mini roundabout to allow vehicles to turnaround within the site to facilitate the school's pick-up/drop-off operations. Service vehicles will not be allowed to access the site during arrival and dismissal timeframes. **Appendix D** contains traffic-operation plans for the proposed school.

The existing roadway infrastructure provides safe crossing locations for pedestrians, as the existing Miami Palmetto Senior High School is directly north of the proposed school. The closest signalized intersection is at the northwest corner of the site and provides marked crosswalks to cross Palmetto Road and SW 120<sup>th</sup> Street. Additionally, there are three existing midblock crosswalks along SW 120<sup>th</sup> Street between the east driveway and SW 74<sup>th</sup> Court. The transit route serving the area is route 73, which is less than half a mile from the school, however because the school will only serve K-8 grades, no students are expected to use public transportation to access the school. In addition, SW 120<sup>th</sup> Street and Palmetto Road have speed zones serving the existing preschool at Bet Shira and the Palmetto High School.

## 2.0 EXISTING CONDITIONS

### 2.1 Data Collection

Langan collected traffic-volume and field data to analyze the current traffic conditions of the study intersections.

#### 2.1.1 Roadway Characteristics

Langan visited the intersections to collect lane-configurations and traffic-control data. **Figure 2** shows the lane configurations for each of the study intersections. A summary of the roadway characteristics is provided below. The maximum acceptable LOS for roadways and intersections is LOS D for county roads and LOS E for State Urban Minor Arterials (SUMA) between Infill Area and Urban Development Boundary.

##### SW 112<sup>th</sup> Street / Killian Drive

SW 112<sup>th</sup> Street is a two-lane, undivided, east-west, county-maintained major collector roadway with a 35 MPH posted speed limit.

##### SW 120<sup>th</sup> Street

SW 120<sup>th</sup> Street is a two-lane, undivided, east-west, county-maintained major collector roadway with a 30 MPH posted speed limit.

##### SW 124<sup>th</sup> Street

SW 124<sup>th</sup> Street is a two-lane, undivided, east-west, county-maintained major collector roadway with a 30 MPH posted speed limit.

##### US-1 / S. Dixie Highway

US-1 is a six-lane road, northeast-southwest, divided, state-maintained principal arterial roadway with a 45 MPH posted speed limit.

##### Palmetto Road / SW 77<sup>th</sup> Avenue

Palmetto Road is a two-lane road, north-south, undivided, county-maintained major collector roadway with a 35 MPH posted speed limit.

##### SW 74<sup>th</sup> Court

SW 74<sup>th</sup> Court is a two-lane road, north-south, undivided, county-maintained local collector roadway with a 30 MPH posted speed limit.

### SW 74<sup>th</sup> Avenue

SW 74<sup>th</sup> Avenue is a two-lane road, north-south, undivided, county-maintained local collector roadway with a 30 MPH posted speed limit.

### SW 72<sup>nd</sup> Avenue

SW 72<sup>nd</sup> Avenue is a two-lane road, north-south, undivided, county-maintained local collector roadway with a 35 MPH posted speed limit.

## 2.1.2 Traffic Counts

Traffic-volume data was collected on Tuesday, December 6, 2022, and Tuesday August 29, 2023, from 7:00 to 9:00 AM. Additionally, 24-hour bidirectional automatic traffic recorder data were collected on Tuesday December 3, 2024, at the study segments. We collected data at seven intersections while schools were in regular session based on the county-accepted methodology. We applied the FDOT peak-season conversion factor (1.05 & 1.03) to the traffic data that was collected in December 2022 and August 2023. We compared the data of each intersection and determined that the morning peak hour occurs between 7:45 AM and 8:45 AM for the study area. **Figure 3** illustrates existing weekday morning peak-hour traffic volumes. **Appendix E** contains the traffic data and seasonal adjustment factors.

It should be noted that based on their current 325-student approval the school opened in the summer of 2023 and currently has a 280-student enrollment and the data collected in 2023 and 2024 accounts for the traffic currently being generated by the school.

## 2.2 Intersection Analysis

We conducted intersection-capacity analyses and found that all study intersections are operating within their adopted LOS during the morning peak. Capacity analysis provides an indication of the adequacy of road facilities to serve traffic demand. The evaluation criteria used to analyze the study-area intersections are based methodologies in the Highway Capacity Manual 7<sup>th</sup> Edition published by the Transportation Research Board, as applied by the latest version of Synchro software. **Table 1** summarizes the existing-conditions analysis and shows the overall delay for the signalized intersections. **Appendix F** contains the intersection volume tables; **Appendix G** contains the capacity-analyses.

**Table 1 - 2022 Existing Intersection Capacity Analysis Summary**

Location	Traffic Control	Approach	AM Peak Hour	
			LOS	Delay (sec/veh)
(1) Palmetto Road & SW 112 Street	Signalized	Overall	B	19.3
(2) US-1 & SW 120 Street	Signalized	Overall	C	23.0
(3) Palmetto Road & SW 120 Street	Signalized	Overall	C	20.1
(4) SW 120 Street & SW 74 Court	Stop-Controlled	NB	B	11.4
(5) SW 120 Street & SW 74 Avenue	Stop-Controlled	SB	B	10.6
(6) SW 72 Avenue & SW 120 Street	Signalized	Overall	C	20.5
(7) Palmetto Road & SW 124 Street	Signalized	Overall	C	21.6

### 3.0 PLANNED AND PROGRAMMED ROADWAY IMPROVEMENTS

We reviewed the Transportation Planning Organization's 2022 Transportation Improvement Program (2023 through 2027), the county Long Range Transportation Plan (2045) and the FDOT Five Year Work Program (2023 through 2027) and we found that there are no roadway improvement projects nearby the project location.

## 4.0 NO-BUILD CONDITIONS

This section of the report covers background traffic growth and future traffic volumes used to evaluate the no-build conditions. The no-build conditions evaluate future traffic volumes without the impacts of the proposed school.

### 4.1 Background Traffic

Background traffic volumes account for annual increases in traffic from approved and unbuilt land-development projects and historical increases in traffic volumes. The proposed school should be completed by the end of 2025. We developed 2025 no-build traffic volumes by applying a compounded growth rate to the 2022 volumes and by adding traffic from the Skilled Nursing Facility as committed development. We used a 3.03 percent annual growth-rate factor to develop future background volumes based on the TMC data collected for three consecutive years at the intersection of SW 120<sup>th</sup> Street and Palmetto Road. **Figure 4** illustrates the 2025 no-build traffic volumes. Appendix E contains the TMC data collected and growth-rate calculation. Appendix F contains the intersection volume tables. **Appendix H** contains excerpts from the committed development.

### 4.2 Intersection Analysis

We conducted intersection-capacity analyses for the no-build conditions and found that all study intersections are expected to operate within their adopted LOS during the morning peak hours **Table 2** summarizes the results of the 2025 no-build conditions analysis. Appendix G contains the capacity-analyses worksheets.

**Table 2 - 2025 No Build Intersection Capacity Analysis Summary**

Location	Traffic Control	Approach	AM Peak Hour	
			LOS	Delay (sec/veh)
(1) Palmetto Road & SW 112 Street	Signalized	Overall	C	20.5
(2) US-1 & SW 120 Street	Signalized	Overall	C	29.1
(3) Palmetto Road & SW 120 Street	Signalized	Overall	C	21.2
(4) SW 120 Street & SW 74 Court	Stop-Controlled	NB	B	11.8
(5) SW 120 Street & SW 74 Avenue	Stop-Controlled	SB	B	10.9
(6) SW 72 Avenue & SW 120 Street	Signalized	Overall	C	21.3
(7) Palmetto Road & SW 124 Street	Signalized	Overall	C	22.6

## 5.0 BUILD CONDITIONS

This section of the report covers site-generated trips, trip distribution, and future traffic volumes used to evaluate the build conditions. The analysis of the build conditions evaluates the future traffic volumes for the anticipated build-out year of the school by adding its traffic to the 2025 no-build volumes.

### 5.1 School Traffic

#### 5.1.1 Site Access

The site will have access through two driveway connections to SW 120<sup>th</sup> Street. The west driveway on SW 120<sup>th</sup> Street (Driveway 1) will operate as an egress right-turn only driveway for parents during drop-off/pick-up times. The east driveway (Driveway 2) will operate as an all-access driveway that serves as the main driveway to the school. However, during arrival and dismissal periods, the driveway will operate as right-turn only ingress driveway. Service vehicles are expected to use the east driveway on SW 120<sup>th</sup> Street to enter and exit the site and will not arrive/depart during drop-off and pick-up timeframes.

#### 5.1.2 Trip Generation

The site is approved for a PreK-8 private school with a 325-student enrollment and was approved through the zoning application Z85-702. The school opened in the summer of 2023 and currently has a 280-student enrollment. The proposed school is requesting an increase of 275 students that will increase the maximum approved enrollment to 600 students and will provide classes from kindergarten through eighth grades. To provide a conservative analysis, we did not account for the approved nor the existing school. The proposed school is expected to generate 489 morning peak-hour trips based on the full 600 student enrollment, determined by equations from the 11<sup>th</sup> Edition of the ITE Trip Generation Manual. **Table 3** summarizes the results of the trip-generation estimates. Appendix I includes the trip-generation data. The proposed school will operate will three instructional shifts each staggered by 30 minutes. We applied a reduction factor of 22% to the trip-generation's proposed uses calculations to account for a three-shift operation that spreads the arrival and departure of vehicles beyond one hour. We used a 22% reduction instead of a 33% reduction per the Village of Pinecrest request, as this more accurately represents the conditions that are expected to occur during arrival and dismissal operations.

**Table 3 - Trip Generation Estimates**

Use	Size	Daily	Weekday Morning Peak Hour		
			In	Out	Total
<b>Existing Uses</b>					
Charter Elementary School	280 Students	518	152	140	292
<b>Peak-hour vehicle reduction due to total students per instructional shifts (22%)</b>		-	34	31	65
<b>Net-New Trips</b>		518	118	109	227
<b>Proposed Uses</b>					
Charter Elementary School	600 Students	1,110	327	301	628
<b>Peak-hour vehicle reduction due to total students per instructional shifts (22%)</b>		-	72	67	139
<b>Net-New Trips</b>		<b>1,110</b>	<b>255</b>	<b>234</b>	<b>489</b>
<b>Difference</b>		<b>592</b>	<b>137</b>	<b>125</b>	<b>262</b>

As previously mentioned, the site has an approval to operate a 325-student private school which opened in the summer of 2023 and currently has a 280-student enrollment. Nevertheless, we did not account for the existing school on the trip generation analysis to provide a conservative analysis.

### 5.1.3 Trip Distribution

We estimated project trip distribution based on the cardinal distribution for Traffic Analysis Zone 688 of the Miami-Dade County 2045 Transportation Model. **Table 4** below shows the interpolated cardinal distribution based on 2025 build out year. **Figure 5** shows the project-traffic distribution at each of the study intersections. **Figure 6** shows project-trip assignments.

**Table 4 - Cardinal Distribution**

Year	NNE	ENE	ESE	SSE	SSW	WSW	WNW	NNW
2015	27.70%	3.50%	0.50%	0.30%	11.20%	14.90%	11.40%	30.60%
2045	30.80%	1.70%	0.40%	0.20%	10.70%	14.60%	11.20%	30.50%
2025	<b>28.73%</b>	<b>2.90%</b>	<b>0.47%</b>	<b>0.27%</b>	<b>11.03%</b>	<b>14.80%</b>	<b>11.33%</b>	<b>30.57%</b>

### 5.2 Intersection Analysis

We developed the 2025 build traffic volumes by adding the total site-generated trips to the 2025 no-build traffic volumes. **Figure 7** illustrates the 2025 build morning peak-hour traffic volumes. We found that the study intersections are expected to operate within their adopted LOS.

Although the overall intersection of US-1 and SW 120<sup>th</sup> Street is expected to operate within its adopted LOS, the southbound left-turn movement is expected to operate beyond capacity due

to the expected increase in traffic from background growth. We optimized the signal timing, without changing the cycle length, to improve the failing approach of the intersection to a condition that is better than the existing condition.

We optimized the signal timing, without changing the cycle length, of the intersection of SW 120<sup>th</sup> Street and Palmetto Road to improve the southbound left-turn lane delay time and queue length. Through signal timing optimization, the expected delay of the southbound left-turn lane improved to a better LOS, but the queues still exceed the storage lane. As such, the school has agreed to extend the southbound left turn lane to accommodate the expected traffic at this approach.

**Table 5** summarizes the 2025 build LOS for the morning peak hour. Appendix G contains the capacity analysis reports.

**Table 5 - 2025 Build Intersection Capacity Analysis Summary**

Location	Traffic Control	Approach	AM Peak Hour	
			LOS	Delay (sec/veh)
(1) Palmetto Road & SW 112 Street	Signalized	Overall	C	22.8
(2) US-1 & SW 120 Street	Signalized	Overall	D	45.6
		Overall <sup>[1]</sup>	D	37.5
(3) Palmetto Road & SW 120 Street	Signalized	Overall	D	44.1
		Overall <sup>[1]</sup>	C	26.0
(4) SW 120 Street & SW 74 Court	Stop-Controlled	NB	B	14.8
(5) SW 120 Street & SW 74 Avenue	Stop-Controlled	SB	B	11.8
(6) SW 72 Avenue & SW 120 Street	Signalized	Overall	C	24.8
(7) Palmetto Road & SW 124 Street	Signalized	Overall	C	26.6
(8) SW 120 Street & Driveway 2	Stop-Controlled	NB	B	13.8
(12) SW 120 Street & Driveway 1	Stop-Controlled	NB	C	16.5

[1] Optimized signal timing without changing cycle length

### 5.3 Driveway and Volumes and Turn-Lane Analysis

We developed site driveway volumes for the morning peak hour using the project-trip generation and distribution percentages. **Figure 8** shows morning peak-hour volumes for the school's driveway. We determined that all driveways are expected to operate at LOS C or better. The west driveway (Driveway 1) on SW 120<sup>th</sup> Street will operate as an egress right-turn only driveway for parents during drop-off/pick-up times. The east driveway (Driveway 2) will operate as an all-access driveway that serves as the main driveway to the school. However, during arrival and dismissal periods, the driveway will operate as a right-turn ingress only driveway. Appendix G contains the capacity-analysis reports. We analyzed the need for exclusive right turn lanes at the driveway connections on SW 120<sup>th</sup> Street and determined that the expected traffic volumes warrant the need for an exclusive eastbound right-turn lane into Driveway 2 based on the FDOT access management guidelines. The school has agreed to construct an exclusive eastbound right turn lane to mitigate the project's impacts and to alleviate the traffic flow on SW 120<sup>th</sup> Street.

### 5.4 Vehicle Accumulation Assessment

Traffic studies for schools must include a vehicle-accumulation assessment to determine the adequacy of the parking and student pick-up and drop-off vehicle-stacking area. Multiple school shifts are required for sites that are expected to cause vehicles to spill back into public roadways.

#### 5.4.1 Methodology

The analysis is consistent with the Miami-Dade County’s methodology for schools. Data was collected at Academir Charter School Preparatory (ACSP) K-8 Charter School (surrogate school) at 19185 SW 127<sup>th</sup> Ave, Miami, FL 33177 to develop accumulation values.

#### 5.4.2 Data Collection

Vehicle-accumulation data was collected at the surrogate school on Tuesday, February 28, 2023, between 2:00 and 4:00 PM. Data was collected every five minutes and accounted for all the vehicles generated by the surrogate school. **Appendix J** contains the vehicle-accumulation assessment data.

#### 5.4.3 Assessment Results

The elementary school program is expected to accommodate 115% of its passenger vehicle accumulation within its property with three instructional shifts. The surrogate school’s highest vehicles accumulation was 85 vehicles over three shifts. The proposed school will provide 195 parking spaces and 69 queuing/stacking spaces. A multiplier of 0.71 was applied to the surrogate accumulation data to account for enrollment difference and determine accommodation percentage of the proposed school. The school will also utilize double stacking for certain segments of the on-site accumulation to accommodate more vehicles internally. **Table 6** shows the summary of the vehicle assessment. Appendix I contains the county’s accumulation assessment form.

**Table 6 - Vehicle Accumulation Assessment Summary**

Number of Shifts	Vehicle Type	Vehicles Generated*	Multiplier	PINECREST AT BET SHIRA K-8 CHARTER SCHOOL			
				Spaces Required for School	Spaces Provided by School	Percent Accommodation	Requires Additional Shift
3	Cars	85	0.71	60	69	115%	NO

\* Based on Surrogate

## 6.0 ROADWAY CAPACITY ANALYSIS

We prepared a 2025 peak-hour directional roadway capacity analysis for the following segments:

- US-1 from SW 112<sup>th</sup> to SW 124<sup>th</sup> avenues
- Palmetto Road from SW 112<sup>th</sup> to SW 124<sup>th</sup> avenues
- SW 120<sup>th</sup> Street between Palmetto Road and SW 74<sup>th</sup> Court

We found that all roadway segments are expected to operate within their adopted LOS for the morning and afternoon peak hour 2025 build conditions. We used the collected 24-hour automatic traffic recorder (ATR) data for the study segments and adjusted the data with the peak-season conversion factor to convert the data to 2024 peak season volumes. We developed 2025 future background volumes by applying a 3.03 percent annual growth-rate factor and by adding traffic from the Skilled Nursing Facility as a committed development. We applied our project development trips to the 2025 traffic volume to develop our 2025 build conditions. Since the ATR data was collected in December 2024 while the school had an existing enrollment of 280 students, the project development trips was based on the net-new traffic generated by the additional 320 students that would be enrolled at the full capacity of 600 students. The adopted LOS for the roadway segments analyzed are based on the Maximum Service Volumes are based on the peak-hour directional table from FDOT 2023 Multimodal Quality / Level of Service Handbook. **Table 7** and **Table 8** summarize the results of the morning and afternoon peak-hour roadway bidirectional capacity analysis. Appendix D includes the collected traffic data at the study segments.

To mitigate the project's traffic along Palmetto Road, the school is proposing to construct a northbound right-turn lane at the intersection of Palmetto Road and SW 120<sup>th</sup> Street. In doing so, the roadway will increase its capacity to better accommodate future traffic.

**Table 7 - 2025 Morning Peak Hour Roadway Capacity Analysis Summary**

Roadway	From	To	Peak Hour	Facility Type	Adopted LOS	Maximum Service Volume LOS <sup>[1,2]</sup>	Direction	2024 Volume <sup>[3]</sup>	Existing LOS	2025 Volume	2025 Committed Development Volume	Project Trips	2025 Total Traffic	LOS with Project	Meets Capacity
US-1	SW 112 Street	SW 120 Street	AM	6L	E	3,140	NB	1,477	C	1,522	10	39	1,571	C	Yes
							SB	1,650	C	1,700	10	42	1,752	C	Yes
US-1	SW 120 Street	SW 124 Street	AM	6L	E	3,140	NB	1,644	C	1,694	10	15	1,719	C	Yes
							SB	1,569	C	1,617	10	14	1,641	C	Yes
Palmetto Road	SW 112 Street	SW 120 Street	AM	2L	D	999	NB	818	C	843	0	0	843	C	Yes
							SB	364	C	375	0	9	384	C	Yes
Palmetto Road	SW 120 Street	SW 124 Street	AM	2L	D	999	NB	829	C	854	0	36	890	D	Yes
							SB	350	C	361	0	0	361	C	Yes
SW 120 Street	Palmetto Road	SW 74 Court	AM	2L	D	873	EB	373	C	384	0	103	487	C	Yes
							WB	306	C	315	0	94	409	C	Yes

[1] Adopted LOS based on FDOT 2023 Quality / LOS Tables

[2] Non-State Signalized Roadway multiplied by 0.9

[3] Volumes converted to peak season volumes using FDOT Seasonal Adjustment factor

**Table 8 - 2025 Afternoon Peak Hour Roadway Capacity Analysis Summary**

Roadway	From	To	Peak Hour	Facility Type	Adopted LOS	Maximum Service Volume LOS <sup>[1,2]</sup>	Direction	2024 Volume <sup>[3]</sup>	Existing LOS	2025 Volume	2025 Committed Development Volume	Project Trips	2025 Total Traffic	LOS with Project	Meets Capacity
US-1	SW 112 Street	SW 120 Street	AM	6L	E	3,140	NB	1,625	C	1,674	0	8	1,682	C	Yes
							SB	1,873	C	1,930	0	4	1,934	C	Yes
US-1	SW 120 Street	SW 124 Street	AM	6L	E	3,140	NB	1,644	C	1,694	0	2	1,696	C	Yes
							SB	1,569	C	1,617	0	3	1,620	C	Yes
Palmetto Road	SW 112 Street	SW 120 Street	AM	2L	D	999	NB	493	C	508	0	0	508	C	Yes
							SB	886	D	913	0	1	914	D	Yes
Palmetto Road	SW 120 Street	SW 124 Street	AM	2L	D	999	NB	447	C	461	0	4	465	C	Yes
							SB	951	D	980	0	0	980	D	Yes
SW 120 Street	Palmetto Road	SW 74 Court	AM	2L	D	873	EB	227	C	234	0	10	244	C	Yes
							WB	406	C	418	0	19	437	C	Yes

[1] Adopted LOS based on FDOT 2023 Quality / LOS Tables

[2] Non-State Signalized Roadway multiplied by 0.9

[3] Volumes converted to peak season volumes using FDOT Seasonal Adjustment factor

## 7.0 ROADWAY SEGMENT QUEUING ANALYSIS

We prepared a queuing analysis along SW 120<sup>th</sup> Street between Palmetto Road and SW 74<sup>th</sup> Avenue and determined that the proposed development is not expected to significantly increase the existing queues along the roadway segment. At the intersection of SW 120<sup>th</sup> Street and Palmetto Road, SW 120<sup>th</sup> Street has one exclusive left-turn lane with 100-feet of storage. The remaining westbound and eastbound through movements along roadway segment operate with one lane in each direction. The roadway segment has a length approximately of 1295 feet (approximately 59 vehicles in each direction). Additionally, legal parking spaces are provided along the north side of SW 120<sup>th</sup> Street; however, cars are illegally parking along most of the south side of SW 120<sup>th</sup> Street. We collected 5-minute queueing interval data on Tuesday, August 29, 2023, between 6:00 AM – 9:00 AM and from 1:00 PM – 4:00 PM along SW 120<sup>th</sup> Street to understand the existing queueing conditions that occur during the school peak hours. The queue study was divided into four different zones. Zone 1 represents the area between the easternmost midblock crossing and SW 74<sup>th</sup> Avenue, Zone 2 represents the area between the middle midblock crossing and the easternmost midblock crossing, Zone 3 represents the area between the westernmost midblock crossing and the middle midblock crossing while Zone 4 represents the area between Palmetto Road and the west midblock crossing.

Based on our findings, during the observed 5-minute increments, the eastbound direction for the entire road segment experienced a maximum queue of 36 vehicles from 7:20 AM – 7:25 AM, and a maximum queue of 38 vehicles from 2:25 PM – 2:30 PM. We note that during the morning peak hour, eastbound queueing begins around 6:45 AM, and abruptly drops to no queueing after 7:25 AM, suggesting that any queueing observed is focused within this short time frame and is mainly due to school traffic. Similarly, during the afternoon peak hour, an eastbound queue of 5 vehicles forms at 2:20 PM, and returns to no vehicles queued by 2:35 PM.

In the westbound direction, during the observed 5-minute increments, the entire road segment experienced a maximum queue of 31 vehicles from 7:05 AM – 7:10 AM, and a maximum queue of 24 vehicles from 2:30 PM – 2:35 PM. We note that during the morning peak hour, westbound queueing begins around 6:55 AM, and abruptly drops to no queueing after 7:25 AM, suggesting that any queueing observed is focused within this short time frame mainly due to school traffic. During the afternoon peak hour, we first see queueing of approximately 15 vehicles occur at 2:20 PM, with fluctuations over the preceding 5-minute increments until 3:00 PM, where the queues are decreased to 5 vehicles.

We also collected parking data on Tuesday, August 29, 2023, to prepare a parking study along SW 120<sup>th</sup> Street and determined that the north side of the road has a maximum parking demand of nine vehicles in the morning peak hour and is fully occupied with legal on-street parking during the afternoon peak hour. The south side of SW 120<sup>th</sup> Street currently does not allow parking and is controlled by regulatory signage. Nevertheless, it is currently being used and was observed with a maximum parking demand of three vehicles in the morning peak hour, and 23 vehicles in the afternoon peak hour. The school will construct curb and gutter along the north façade of the school on the south side of SW 120<sup>th</sup> Street to restrict the on-street parking.

We anticipate a total of 197 new project-generated trips during the morning peak hour will utilize the road segment of SW 120<sup>th</sup> Street between Palmetto Road and SW 74<sup>th</sup> Court to either enter or exiting the project site. As previously mentioned, the school is currently operating with a 280-student enrollment and the queueing data captured the traffic currently generated by the school. As shown on the roadway capacity analysis the roadway segment is expected to operate within its roadway capacity for the future conditions. Additionally, based on the intersection capacity analysis, we demonstrated that the expected traffic on SW 120<sup>th</sup> Street and Palmetto Road, as well as SW 120<sup>th</sup> Street and SW 74<sup>th</sup> Avenue are not expected to significantly impact the intersections. Although the queueing analysis, roadway capacity analysis, and intersection capacity analysis has demonstrated that this segment of SW 120<sup>th</sup> Street is operating under acceptable conditions, to further mitigate the project's impacts on SW 120<sup>th</sup> Avenue, the school has agreed to construct an exclusive eastbound right-turn lane at the existing easternmost driveway and is proposing a new driveway connection to Palmetto Road to distribute traffic. Appendix E contains the queueing analysis data.

In addition, we reviewed the possibility of widening SW 120<sup>th</sup> Street into a four-lane road and found that the right-of-way (ROW) width along the road varies and to be able to widen the road from 2 to 4 lanes will likely require that all on-street parking spaces to be eliminated.

## 8.0 SCHOOL SPEED ZONE

The site currently has pedestrian access through SW 120<sup>th</sup> Street, where there are sidewalks on the north and south side of the road. In addition, there are crosswalks at the intersection of SW 120<sup>th</sup> Street and Palmetto Road and there are mid-block crossings on SW 120<sup>th</sup> Street just east of the site, which provide safe access and crossing for students. However, we do not anticipate any students or parents will walk to the school, which will be enforced through the school staff.

**Figure 9** shows an aerial with the existing pedestrian infrastructure.

The proposed development warrants the need for a school speed zone based on the latest FDOT requirements. Currently there is an existing school speed zone along SW 120<sup>th</sup> Street and Palmetto Road serving the Miami Palmetto Senior High School and based on field review there are some signage that will need to be replaced and upgraded to the latest standards. Per Florida Statute Section 316.1895(6), any school zone in which the signing has been replaced, a sign stating "SPEEDING FINES DOUBLED" shall be installed within the school zone. The school has committed to upgrading the existing school speed zone to comply with the latest FDOT standards. In addition, the school will also install flashing beacons at the existing and proposed school zone speed limit assemblies to meet the current FDOT standards and ensure that drivers know about the presence of the school so they can properly adjust their speed. **Figure 10** shows an aerial detailing the proposed school speed zone upgrade of the existing pavement markings and signage.

## 9.0 TRANSPORTATION DEMAND MANAGEMENT STRATEGIES

The site abuts Palmetto Road (SW 77<sup>th</sup> Avenue) and SW 120<sup>th</sup> Street which currently provide sidewalks to access the project site and the surrounding developments in the area. The proposed development is approximately a 7-minute walk to the nearest bus station (Route 73) south of the intersection of Palmetto Road and SW 124<sup>th</sup> Street. Appendix E contains a copy of the transit route maps.

The proposed development is an elementary charter school and is not expected to have students walking or riding a bicycle to access the site. Therefore, the proposed TDM strategies will focus on providing options mainly on school staff to motivate the use of the available multimodal transportation systems provided by the city and the existing roadway network.

The development will work to create Transportation Demand Management (TDM) strategies to support the overall TDM goals of Miami Dade County and maximize the use of the available transportation systems. The most important action will be doing a regular employee’s outreach to provide them with the multiple commute options and establish preferences to target TDM efforts. **Table 9** summarizes the proposed TDM strategies.

**Table 9 - Proposed TDM Strategies**

Action	Details
Employee Survey	Survey employees to determine current commute characteristics establish preferences, and target TDM efforts.
Education, Marketing, and Outreach	Offer new employees a packet of materials and/or provide personal consultation detailing sustainable (non-SOV) travel options.
Staggered Shifts	The school will operate with three instructional shifts staggered by 30-minutes to spread school traffic.
Siblings Pick-Up	The school will develop a pick-up strategy for siblings to reduce the vehicle trips generated by the school. The strategy will determine the number of families who have siblings attending the same school and require pick up at the same time.
Travel Mapping	Transit route maps and schedules will be made available on site to employees and visitors.
Loading Area	The proposed on-site loading area will have specific timeframes to avoid traffic conflicts in the regular peak-hours.

## 10.0 CONCLUSIONS

Langan prepared a traffic-impact analysis for the proposed development of the True North Classical Academy at Pinecrest K-8 Charter School expected to be built by 2025. The charter school will have a maximum enrollment of 600-student serving kindergarten through eighth grades. Our analysis shows that all study intersections and are expected to operate within their adopted LOS and determined that the existing roadway network can accommodate the proposed development generated trips. The developer is proposing to construct a northbound right turn lane along Palmetto Road which will increase the capacity of the roadway and alleviate traffic flow.

We optimized the signal timing, without changing the cycle length, of the signalized intersection of US-1 and SW 120<sup>th</sup> Street to improve the delay of the intersection's failing approach to a condition that is better than the existing condition. The school's driveway connections are expected to operate within their adopted LOS during the morning peak-hour. The vehicle accumulation analysis indicates that the proposed school can accommodate all vehicles on-site during the drop-off and pick-up peak times with three instructional shifts to mitigate the school's impacts to adjacent public roads.

### Proposed improvements

Based on the traffic study findings, the proposed school has agreed to construct the following improvements to mitigate the project's impacts:

1. Construct an exclusive eastbound right-turn lane at the existing easternmost driveway connection to SW 120<sup>th</sup> Street.
2. Remove the existing westernmost driveway connection on SW 120<sup>th</sup> Street.
3. Construct an exclusive northbound right-turn lane at the intersection of Palmetto Road and SW 120<sup>th</sup> Street.
4. Reconstruct a portion of the site's parking lot to provide a mini roundabout to allow vehicles to turnaround within the site to facilitate the school's pick-up/drop-off operations.
5. Extend the southbound left-turn lane at the intersection of SW 120<sup>th</sup> Street and Palmetto Road by approximately 100-feet to accommodate existing and expected queues.
6. Update school speed-zone signs and pavement markings along SW 120<sup>th</sup> Street, Palmetto Road, and SW 74<sup>th</sup> Avenue and update the existing school schedule zone flashers along Palmetto Road in accordance with FDOT design standards.
7. Construct curb and gutter along the south side of SW 120<sup>th</sup> Street to restrict the on-street parking.

RESPONSES TO VILLAGE OF PINECREST  
PLANNING & ZONING DEPARTMENT - CRC COMMENTS

PROJECT NAME: True North Classical Academy at Pinecrest Campus

PROJECT REVIEWER: Mr. Stephen R. Olmsted, AICP, LEED-GA, (Planning Director)

PROJECT ADDRESS: 7500 SW 120th St, Pinecrest, FL 33156

PROPERTY FOLIO: 20-5014-004-0050

APPLICABLE RESOLUTIONS: 4-ZAB-37-86 & Z-114-86 (Latest of Various)

DATE OF THIS DOCUMENT: 11.15.2023

**COMMENT-1:** THE PROPERTY HAS AN EXTENSIVE ZONING HISTORY WHICH INCLUDES NUMEROUS RESOLUTIONS THAT GOVERN THE USE OF THE PROPERTY INCLUDING RESOLUTIONS 2-ZAB-304-62, 2-ZAB-341-64, 4-ZAB-90-76, 4-ZAB-131-80, AND 4-ZAB-37-86/ZZ-114-86. PLEASE NOTE THAT ANY RESOLUTIONS NOT MODIFIED BY THE VILLAGE COUNCIL WILL CONTINUE TO APPLY.

**RESPONSE:**  
*THE APPLICANT WILL COMPLY. THE APPLICANT SHALL COMPLY WITH ALL RESOLUTIONS NOT MODIFIED BY THE VILLAGE AS WELL AS ANY NEW AND MODIFIED RESOLUTION(S) AND / OR CONDITIONS.*

**COMMENT-2:** THE SUBMITTED TRAFFIC STUDY HAS BEEN PROVIDED TO THE VILLAGE'S TRANSPORTATION ENGINEERING CONSULTANT. REVIEW COMMENTS WILL BE PROVIDED WHEN COMPLETE AND INVOICES WILL BE SUBMITTED WHEN RECEIVED FOR PAYMENT PURSUANT TO REQUIREMENTS OF THE VILLAGE'S COST RECOVERY REGULATIONS.

**RESPONSE:**  
*THE APPLICANT WILL COMPLY. PLEASE SEE THE REVISED TRAFFIC STUDY UPDATED SO AS TO ADDRESS THE LATEST COMMENTS AS PROVIDED BY REVIEWING AGENCIES. THE SITE PLAN DRAWINGS SUBMITTED UNDER THIS REVISION ALSO REFLECT ADJUSTMENTS REQUIRED BY THE TRAFFIC ENGINEERING COMMENTS. SEE ALSO INCLUDED RESPONSES TO COMMENTS.*

*PAYMENT FOR ANY REQUIRED INVOICE SHALL BE EXPEDITED AS REQUIRED, PLEASE NOTIFY THE APPLICANT AS SOON AS THESE ARE AVAILABLE.*



**COMMENT-7:**

**RESPONSE:**

**THE APPLICANT WILL COMPLY. PLEASE SEE REVISED DOCUMENTS AND REVISED DRAWINGS WITH CALCULATIONS ON A-1.0 AND A-6.1, INDICATING PROPOSED TOTAL OF K-8<sup>TH</sup> GRADE SCHOOL WITH 600 STUDENTS, AND SEPARATE PRESCHOOL / DAY-CARE FOR 100 STUDENTS.**

**COMMENT-8:** PLEASE INDICATE A SHADE TREE (MINIMUM 14 FEET IN HEIGHT, 3 INCHES IN DIAMETER) FOR THOSE LANDSCAPE ISLANDS IN THE PARKING LOT CURRENTLY MISSING THE REQUIRED TREE.

**RESPONSE:**

**THE APPLICANT WILL COMPLY. SEE REVISED LANDSCAPING DRAWINGS.**

**EVERY PARKING LOT ISLAND THAT DID NOT ALREADY CONTAIN AN EXISTING TREE / PALM HAS A NEW TREE PROPOSED THAT MEETS THE MINIMUM SIZE REQUIREMENT. NOTE THAT EXISTING TREES ARE INDICATED WITH A DASHED SYMBOL. PLEASE SEE NEW LEGEND ADDED TO SHEET LA2-01 TO CLARIFY EXISTING TREES/PALM/SHRUBS.**

**COMMENT-9:** PLEASE PROVIDE A DETAILED DESCRIPTION OF THE EXISTING PARKING LOT LIGHTING SHOWING THAT ALL LIGHTS ARE AIMED DOWNWARD AND SHIELDED TO PREVENT GLARE AS REQUIRED BY DIVISION 5.19 OF THE VILLAGE'S LAND DEVELOPMENT REGULATIONS.

**RESPONSE:**

**THE APPLICANT WILL COMPLY. PLEASE NOTE THAT EXISTING LIGHTING IS OUTDATED AND DOES NOT MEET THE VILLAGE'S CODE REQUIREMENTS. THE PARKING LOT SHALL BE UPDATED WITH MODERN LIGHTING FIXTURES THAT COMPLY WITH THE PHOTOMETRIC REQUIREMENTS OF THE VILLAGE.**

**PLEASE SEE NEW SHEET A-6.7 INDICATING THE NEW FIXTURE TYPE AND THE ILLUMINATION LEVELS SCIENTIFICALLY CALCULATED.**

**COMMENT-10:** PLEASE PROVIDE A PERIMETER LINE DRAWING DELINEATING THE AREAS OF THE OUTDOOR SPORTS AND RECREATIONAL AREA, THE CLASSROOMS AND OFFICES, AND THE PERVIOUS AND IMPERVIOUS AREAS.

**RESPONSE:**

**THE APPLICANT WILL COMPLY. PLEASE SEE SHEET A-1.0, THE OUTDOOR SPORTS AND RECREATION AREAS ARE MARKED WITH A RED DASHED PERIMETER LINE AND A GREEN DASHED PERIMETER LINE WITH A GREEN SHADING.**

**FOR THE PERVIOUS AREAS, PLEASE SEE LANDSCAPED AREAS INDICATED BY THE GRASS PATTERN SHOWN ON REVISED SHEET A-1.0. THE PERVIOUS AREAS AMOUNT TO 77,295 S.F. SHOWN IN THE OPEN SPACE INFORMATION CHART.**

**FOR THE IMPERVIOUS AREAS, PLEASE REFER TO THE GENERAL NOTES ON A-1.0. THE IMPERVIOUS AREAS INCLUDE THE BUILDING FOOTPRINTS, SIDEWALKS, & PAVED ROADWAYS WHICH TOTAL TO 139,676 SF.**

**FOR CLASSROOM AREAS AND OFFICE LOCATIONS, PLEASE REFER TO DRAWINGS A-2.0, A-4.0, A-5.0, A-6.1, & A-6.2 EACH ROOM IS NAMED PER USE AND NEW CLASSROOM AREAS ARE SHADED IN ORANGE.**

**COMMENT-11:** DIVISION 1.4 OF THE VILLAGE'S LAND DEVELOPMENT REGULATIONS REQUIRES ALL DEVELOPMENT TO BE CONSISTENT WITH THE REQUIREMENTS OF THE VILLAGE'S COMPREHENSIVE DEVELOPMENT MASTER PLAN AND, AS DESCRIBED IN DIVISION 4.4 OF THE VILLAGE'S LAND DEVELOPMENT REGULATIONS, THE PURPOSE OF THE PS, PUBLIC AND SEMI-PUBLIC SERVICES ZONING DISTRICT IS "TO PROVIDE A MANAGEMENT FRAMEWORK FOR IMPLEMENTING COMPREHENSIVE DEVELOPMENT MASTER PLAN DEVELOPMENT OBJECTIVES AND POLICIES FOR EXISTING AND FUTURE PUBLIC AND SEMI-PUBLIC SERVICES." PLEASE INDICATE HOW THE PROPOSED INCREASE IN STUDENT ENROLLMENT AND INCREASE HOURS OF OPERATION ARE CONSISTENT WITH THE VILLAGE'S COMPREHENSIVE DEVELOPMENT MASTER PLAN BUT NOT LIMITED TO THE FOLLOWING GOAL AND POLICY OF THE PLAN:

GOAL 1-1: LAND USE. THE VILLAGE OF PINECREST SHALL MAINTAIN AND ENHANCE THE EXTRAORDINARY CHARACTER AND QUALITY OF LAND USES WITHIN THE VILLAGE BY:

- BALANCING THE IMMEDIATE AND LONG-TERM COMMUNITY INTERESTS AND RESOURCES;
- ADVANCING THE EASTHETIC, PHYSICAL, SOCIAL, CULTURAL, AND ECONOMIC CONDITIONS OF ITS RESIDENTS;
- ENSURING THAT THE CHARACTER OF THE AND LOCATION AND LAND USES INCORPORATE BEST MANAGEMENT PRACTICES AND PRINCIPLES OF RESOURCE CONSERVATION;
- PROMOTING ODERLY LAND USE TRANSITION; AND
- PROTECTING THE PUBLIC HEALTH, SAFETY, WELFARE, AND RURAL AESTHETIC AND PREVENTING THREATS TO HEALTH, SAFETY, WELFARE, AND RURAL AESTHETICS WHICH MAY BE CAUSED BY INCOMPATIBLE LAND USES, ENVIRONMENTAL DEGRADATION, HAZARDS, AND NUISANCES.

POLICY 1-2.4.1 OF THE VILLAGE'S COMPREHENSIVE PLAN REQUIRES LANDS ZONES PS, PUBLIC AND SEMI-PUBLIC SERVICES TO "CONTAIN SUFFICIENT OPEN SPACE" - "LANDS DESIGNATED PS SHALL CONTAIN SUFFICIENT ACREAGE AND OPEN SPACE AND SHALL BE PROPERLY SCREENED AND BUFFERED IN ORDER TO MINIMIZE POTENTIAL ADVERSE IMPACTS ON ADJACENT LAND USES.

**RESPONSE:**

**THE APPLICANT WILL COMPLY. PLEASE NOTE THAT THIS SITE IS NEARLY A 5-ACRE SITE, AND ITS CURRENT USE IS EXTREMELY UNDERUTILIZED. THE PROPOSED INCREASE IN STUDENTS TO 600 SHALL REPRESENT A VERY LOW DISTRIBUTION OF 120 STUDENT PER ACRE OF LAND. THIS NUMBER IS WELL BELOW THE THRESHOLD LIMIT FOR RESPONSIBLE AND SUSTAINABLE PLANNING. THE PROPOSED NUMBER OF STUDENTS IS THE CORRECT AMOUNT SINCE IT DOES NOT REQUIRE THE ALTERING OF THIS SITE, THE IMPLEMENTATION OF ANY BUILDING ADDITIONS; IN TURN, THIS ALSO MAINTAINS THE EXISTING LEVELS OF "RURALITY" DESIRED.**

**PLEASE ALSO NOTE THAT THIS APPLICATION SHALL KEEP THE OTHER SITE RELATED IMPACTS TO LEVELS WELL BELOW THE NORM. FOR INSTANCE, NOTE THAT 195 PARKING SPACES ARE AVAILABLE FOR A PROPOSED INCREASE IN USE THAT SHALL NOT REQUIRE EVEN 50% OF THE PROVIDED PARKING; THE CLASSROOM SPACES AVAILABLE WITHIN THE EXISTING BUILDING (AS PRESENTED) SHALL HAVE CAPACITY TO ACCOMMODATE 720 STUDENTS, HOWEVER, ONLY 600 ARE BEING PROPOSED; LASTLY, THIS SITE HAS A RECREATIONAL OPEN AREA OF APPROXIMATELY 37,500 SQ.FT., THIS YIELDS A RATIO OF 62 SQUARE FEET OF GREEN SPACE (OPEN TO THE SKY) FOR STUDENT USE. NOTE THAT ALTHOUGH RECREATION AREAS ARE NOT REQUIRED FOR PUBLIC CHARTER SCHOOL FACILITIES, A 62 SQUARE FEET OF RECREATION AREA PER STUDENT EXCEEDS THE DCF REQUIREMENT.**

- **THE PROPOSED ADJUSTMENT TO THE NUMBER OF STUDENTS BALANCES THE IMMEDIATE AND LONG-TERM INTERESTS AND RESOURCES OF THE VILLAGE DUE TO THE LOW NUMBER PROPOSED. IT BENEFITS THE VILLAGES' REQUIREMENTS SINCE IT MAXIMIZES THE SUSTAINABILITY OF THIS SITE IN A LOCATION THAT HAS ALREADY THE CORRECT ESTABLISHED USE, (BUT ONCE AGAIN, IS CURRENTLY EXTREMELY UNDERUTILIZED).**
- **EDUCATIONAL FACILITIES ARE PILLARS OF OUR COMMUNITIES, AND RESPONSIBLE PLANNING ALWAYS REQUIRES THESE FACILITIES TO BE PAIRED WITH SIMILAR CIVIC USES, SUCH AS LIBRARIES, PARKS, AND ASSEMBLY BUILDINGS. IN THIS CASE, THE SITE HAS ALREADY THE COMBINATION OF THE CIVIC USES ESTABLISHED, ONLY THE UNDERUTILIZATION OF THE SITE IS REQUIRED TO BE CORRECTED. THIS APPLICATION SHALL ELEVATE THE PURPOSE OF THE SITE WITH A SCHOOL THAT WILL NOT REQUIRE ANY BUILDING ADDITIONS OR ANY PRONOUNCED CHANGES TO THE SITE PLAN. IN ADDITION, HIGH-PERFORMING SCHOOLS HAVE BEEN PROVEN TO GENERATE THE HIGHEST REAL-ESTATE VALUES IN DUE TO THE DESIRABILITY OF THE RESIDENCES WITHIN THEIR PROXIMITY.**

THE CHARACTER OF THE SITE SHALL BE MAINTAINED AND IMPROVED BY THE LANDSCAPING REFINEMENTS THAT ARE BEING PROPOSED AND THE INCREASE OF LANDSCAPE BUFFERS ALONG THE PROPERTY LINES. THE EXISTING USES FOR THE SITE ARE ALREADY CORRECT, BUT THE ADJUSTMENT IN THE NUMBER OF STUDENTS SHALL BE PROPERLY MANAGED BY SIMPLY SUBDIVIDING THE REQUIRED FUNCTIONS OF RECREATION TIME, ARRIVAL & DISMISSAL TIMES INTO SEPARATE SHIFTS. THUS, THE USE OF THE SITE PER SHIFT WILL VIRTUALLY REMAIN AS IS. FOR INSTANCE, THE SCHOOL WILL OPERATE WITH THREE SEPARATE ARRIVAL AND DISMISSAL SHIFTS, THIS YIELDS THAT AT NO POINT ANY SHIFT SHALL EXCEED MORE THAN 200 STUDENTS.

- THIS SITE ALREADY HAS ESTABLISHED THE ORDERLY TRANSITION REQUIRED BY THE ASSEMBLY AND SCHOOL USES – THIS IS DUE TO ITS LOCATION AND ORIGINAL PLANING. FOR ORDERLY DISTRIBUTION OF THE USES, PLEASE NOTE THAT EDUCATIONAL FACILITIES ARE ACTIVE FOR ONLY HALF OF THE DAY, AND ONLY DURING WEEKDAYS; IN CONTRAST, THE RELIGIOUS TEMPLE WILL CONTINUE TO FUNCTION AFTER HOURS AND DURING WEEKENDS. THE PROPOSED ADJUSTMENTS UNDER THIS APPLICATION WILL MAINTAIN THE DIVISION OF FUNCTIONS IN AN ORDERLY FASHION. MOREOVER, THERE WILL BE AT LEAST ONE YEAR REQUIRED TO PERFORM THE MINOR INTERNAL ADJUSTMENTS REQUIRED TO THE EXISTING BUILDINGS, THIS WILL YIELD A GRADUAL INCREASE IN THE NUMBER OF STUDENTS IN LIEU OF AN ABRUPT GROWTH TO THE PROPOSED MAXIMUM.
- NOTE THAT UNDER THIS APPLICATION, THE HEALTH, SAFETY, AND WELFARE OF THE RESIDENTS AND THE USERS OF THIS FACILITY ARE IMPROVED DUE TO THE INCREASE IN LANDSCAPING MATERIAL, THE LACK OF EXTERIOR CONSTRUCTION (NOT REQUIRED NOR BEING PROPOSED), AND THE ADJUSTMENTS TO THE PUBLIC ROW INFRASTRUCTURE WHICH WILL IMPROVE THE CURRENT FUNCTIONING OF AT LEAST ONE INTERSECTION. IN ADDITION, THE EXISTING SITE LIGHTING WHICH DOES NOT COMPLY WITH THE VILLAGE'S PROPERTY LINE ILLUMINATION LEVELS SHALL BE REPLACED WITH MODERN, ENERGY EFFICIENT LIGHTING, THAT COMPLIES WITH THE REQUIRED LIGHTING LEVELS WHILE PREVENTING SPILLING LIGHT ONTO THE ADJACENT PROPERTIES.

*AND FINALLY, THIS APPLICATION COMPLETELY SATISFIES THE REQUIREMENTS OF POLICY 1-2.4.1 BY MAINTAINING THE 8" CONCRETE FENCE WALL BETWEEN THE DISSIMILAR LAND USE AND PROVIDING ADDITIONAL SHRUBS ALONG THE PROPERTY LINE. THE ROW BUFFERING WALLS WILL NOT ONLY REMAIN IN PLACE, BUT SHALL ALSO BE COMPLEMENTED WITH ADDITIONAL LANDSCAPING IMPROVEMENTS. SINCE THERE IS NO NEED TO ADD BUILDINGS UNDER THIS APPLICATION, AND ALL OF THE SITE'S GREEN SPACES, RECREATION SPACES, AND ENTRANCE COURTS TO ASSEMBLY AREAS WILL REMAIN IN PLACE, COMPLIANCE WITH OPEN SPACE REQUIREMENT UNDER THE VILLAGE'S POLICY SHALL BE MAINTAINED AND PROTECTED.*

**COMMENT-12:** AS INDICATED IN DIVISION 4.4 OF THE VILLAGE'S LAND DEVELOPMENT REGULATIONS, THE LOCATIONS AND DISTRIBUTION OF SPECIFIC TYPES OF PUBLIC AND SEMI-PUBLIC SERVICES ARE REQUIRED TO BE DETERMINED BASED ON SPECIFIED CONSIDERATIONS 1 THROUGH 6 BELOW. PLEASE INDICATE HOW THE PROPOSED INCREASE IN STUDENT ENROLLMENT IS CONSISTENT WITH EACH OF THE FOLLOWING CONSIDERATIONS.

**RESPONSE**

*THE APPLICANT SHALL PROVIDE, PLEASE SEE DETAILED DESCRIPTION OF COMPLIANCE ON AMENDED LETTER OF INTENT DOCUMENT.*

*THE PROPOSED APPLICATION ALSO FULFILLS THE PURPOSE OF THE UNDERLYING PS ZONING DISTRICT, AS DESCRIBED IN DIV. 4.4 OF THE VILLAGE'S LAND DEVELOPMENT REGULATIONS, IN THE FOLLOWING MANNER:*

- 1.** ACCOMMODATING THE LAND NEEDS OF EXISTING PUBLIC AND SEMI-PUBLIC LAND USES WITHIN THE VILLAGE.

*RESPONSE - THE PROPOSED APPLICATION DOES NOT DIMINISH OR REDUCE THE AMOUNT OF LANDS AVAILABLE FOR EXISTING PUBLIC AND SEMI-PUBLIC LAND USES WITHIN THE VILLAGE. THE PROPOSED LAND USES ARE CORE SEMI-PUBLIC SERVICES, SPECIFICALLY EDUCATIONAL INSTITUTIONS AND PLACES OF WORSHIP AND ACCESSORY FACILITIES.*

- 2. PROVISION, MAINTENANCE, AND CONSERVATION OF QUALITY PUBLIC SERVICES REQUIRED TO MEET THE NEEDS OF EXISTING AND PROJECTED FUTURE POPULATION MAGNITUDE, DISTRIBUTION, AND CHARACTERISTICS.**

**RESPONSE - THE PROPOSED APPLICATION WILL PROVIDE, MAINTAIN, AND CONSERVE QUALITY PUBLIC SERVICES TO MEET THE NEEDS OF THE RESIDENTS OF THE VILLAGE. TEMPLE BET SHIRA HAS BEEN A VALUABLE AND INTEGRAL PART OF THE PINECREST COMMUNITY FOR DECADES, TENDING TO THE SPIRITUAL AND EDUCATIONAL NEEDS OF ITS CONGREGANTS. TRUE NORTH IS A LONGSTANDING AND RESPECTED EDUCATIONAL INSTITUTION WITH ESTABLISHED FACILITIES THROUGHOUT THE MIAMI-DADE AREA, AND WILL PROVIDE A QUALITY EDUCATIONAL SERVICE FOR THEIR STUDENTS.**

- 3. PROTECTING ESTABLISHED RESIDENTIAL AREAS FROM ENCROACHMENT BY NON-RESIDENTIAL DEVELOPMENT.**

**RESPONSE - SINCE THE PROPOSED TRUE NORTH FACILITIES ARE BEING PROPOSED FOR LANDS THAT ARE ALREADY ZONED AND USED FOR NON-RESIDENTIAL USES, THE APPLICATION DOES NOT CREATE ANY ENCROACHMENT OF NON-RESIDENTIAL DEVELOPMENT INTO THE RESIDENTIAL AREAS. THE PROPERTY HAS BEEN USED FOR NON-RESIDENTIAL USES FOR MANY DECADES.**

- 4. ENSURING COMPATIBILITY WITH ADJACENT LAND USES TO THE GREATEST PRACTICAL EXTENT BY PROVIDING LANDSCAPING AND BUFFER AREAS TO PROTECT ADJACENT RESIDENTIAL DEVELOPMENT FROM THE OFF-SITE IMPACTS OF PUBLIC AND SEMI-PUBLIC LAND USES.**

**RESPONSE - AS NOTED ABOVE, NO CHANGES ARE BEING PROPOSED TO THE EXISTING STRUCTURES, AND THE EXISTING SEMI-PUBLIC USES WILL CONTINUE TO BE SEPARATED BY AS LARGE A BUFFER AREA AND LANDSCAPED AREA, AS WELL AS THE SIX FOOT HIGH CBS WALL, AS THEY ARE TODAY.**

- 5. PROVIDING AND MAINTAINING OF TRANSPORTATION AND INFRASTRUCTURE SYSTEM IMPROVEMENTS.**

**RESPONSE - THE PROPOSED SITE PLAN AND ACCOMPANYING TRAFFIC ANALYSIS OUTLINE THE EXISTING AND PROPOSED TRANSPORTATION IMPROVEMENTS THAT ARE INTENDED TO IMPROVE VEHICULAR ACCESS AND CIRCULATION WITHIN THE SITE. THE ADDITION OF AN ADDITIONAL DRIVEWAY ACCESS AND DECELERATION LANE ON S.W. 77<sup>TH</sup> AVENUE, AND THE SIMPLIFICATION AND CONSOLIDATION OF THE DRIVEWAYS ON S.W. 120<sup>TH</sup> STREET, WILL GREATLY FACILITATE TRAFFIC MOVEMENTS ON THE SURROUNDING ROADWAYS.**

6. THE PUBLIC AND SEMI-PUBLIC SERVICE (PS) ZONING DISTRICT IS INTENDED TO ACCOMMODATE EXISTING PUBLIC AND SEMI-PUBLIC SERVICES INCLUDING: GOVERNMENTAL ADMINISTRATION BUILDINGS; PUBLIC SCHOOLS AND NOT-FOR-PROFIT EDUCATIONAL INSTITUTIONS; ARTS AND CULTURAL OR CIVIC FACILITIES; ESSENTIAL PUBLIC SERVICES, FACILITIES, AND UTILITIES; FIRE AND EMERGENCY OPERATION FACILITIES; PLACES OF WORSHIP; AND ACCESSORY FACILITIES; AND OTHER SIMILAR ACTIVITIES AS IDENTIFIED IN THE LAND DEVELOPMENT CODE.

**RESPONSE - THE EXISTING AND PROPOSED USES ARE FULLY CONSISTENT WITH THE INTENDED PURPOSE OF THE PS ZONING DISTRICT, CONSISTING OF A LONGSTANDING PLACE OF WORSHIP AND ACCESSORY FACILITIES AND A NOT-FOR-PROFIT EDUCATIONAL INSTITUTION.**

**COMMENT-13:** OPEN SPACE THE PS, PUBLIC AND SEMIPUBLIC SERVICES ZONING DISTRICT REQUIRES A MINIMUM OF 35% GREEN SPACE. PLEASE PROVIDE GREEN SPACE CALCULATIONS DEMONSTRATING COMPLIANCE.

**RESPONSE:**  
**THE APPLICANT WILL COMPLY. SEE A-1.0, WITH ZONING TABLE INDICATING A PROPOSED GREEN SPACE AMOUNT OF 35.62%**

**COMMENT-14:** A VARIANCE WAS APPROVED IN 1986 TO ALLOW A REDUCTION IN REQUIRED OUTDOOR RECREATION AREA FROM 82,450 SQUARE FEET TO 49,972 SQUARE FEET. THE PROPOSED INCREASE IN STUDENT ENROLLMENT FURTHER INCREASES THE NEED FOR OUTDOOR RECREATION AREA. PLEASE PROVIDE CALCULATIONS DEMONSTRATING THAT THE EXISTING AND PROPOSED OUTDOOR RECREATION AREA IS SUFFICIENT TO MEET THE REQUIREMENTS OF THE VILLAGE'S LAND DEVELOPMENT REGULATIONS; AND CONSISTENT WITH DIVISION 5.10 OF THE VILLAGE'S LAND DEVELOPMENT REGULATIONS FOR THE PRESCHOOL, DAY NURSERY, AND AFTER SCHOOL CARE USES. NOTE: DIVISION 5.10 REQUIRES OUTDOOR RECREATION AREA TO BE PROVIDED IN THE AMOUNT OF 45 SQUARE FEET FOR EACH CHILD IN THE DAY NURSERY/KINDERGARTEN AND PRESCHOOL AND AFTERSCHOOL CARE PROGRAMS.

**RESPONSE:**  
**THE APPLICANT WILL COMPLY, PLEASE SEE REVISED A-1.0 OUTDOOR RECREATION INFORMATION CHART. THE REQUIRED RECREATION FOR THE PRE-K, KINDERGARTEN, DAYCARE / NURSERY, AND AFTERSCHOOL IS 7,110 SF.**

**THE PROVIDED OUTDOOR RECREATION PROVIDED IS 9,500 SF WHICH TOTALS 60.13 SF PER CHILD.**

**CHARTER SCHOOL GRADE LEVELS K-8<sup>TH</sup> ARE EXEMPT FROM ANY RECREATION AREA REQUIREMENTS, HOWEVER, A TOTAL AREA OF 28,000 S.F. HAS BEEN ASSIGNED FOR THESE GRADE LEVELS. PLEASE SEE RED OUTLINE ON A-1.0.**

5 5 1 1

**COMMENT-15:** CLASSROOM SIZE – THE CURRENT SCHOOL FACILITY IS APPROVED AND UTILIZED TO ACCOMMODATE 325 STUDENTS. PLEASE ADVISE HOW YOU ARE PROPOSING TO ACCOMMODATE 600 STUDENTS IN A FACILITY ORIGINALLY DESIGNED TO ACCOMMODATE 325 STUDENTS.

**RESPONSE:**

**THE APPLICANT WILL COMPLY. PLEASE REFER TO THE NEW SHEET A-5.0 INDICATING THE NUMBER OF CLASSROOMS BREAKDOWN.**

**A TOTAL OF 29 CLASSROOMS WILL BE AVAILABLE FOR THE SCHOOL, THESE HAVE THE CAPACITY TO ACCOMMODATE UP TO 725 STUDENTS, HOWEVER, ONLY 600 STUDENTS ARE BEING PROPOSED UNDER THIS APPLICATION.**

**COMMENT-16:**

**RESPONSE:**

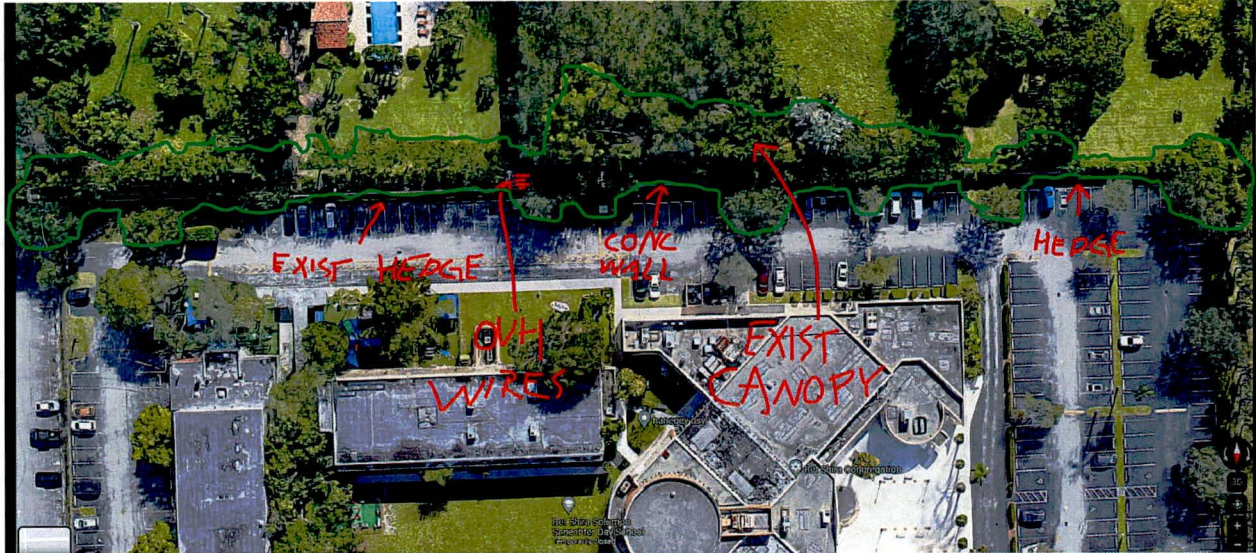
**THE APPLICANT WILL COMPLY. SEE REVISED LANDSCAPING DRAWINGS.**

**PLEASE SEE REVISED SHEET LA2-01. WHILE THERE ARE OVERHEAD POWERLINES ALONG THE ENTIRE SOUTH PROPERTY LINE, THERE IS A CONTINUOUS 6' MIN. HEDGE, LARGE SHADE TREES, AND EXISTING TREE CANOPIES ALONG THE SOUTH BUFFER. WE ARE PROPOSING NEW TREES UNDER THE OVERHEAD WIRES (8' HEIGHT, 2" DBH MIN PER 6.1.3.C "POWER LINES") WHERE THERE IS THE LEAST CONFLICT WITH EXISTING ADJACENT CANOPY AND WHERE ADDITIONAL SCREENING WOULD BE BENEFICIAL PARTICULARLY TOWARDS THE WEST).**

**THE NEW TREES ARE PROPOSED IN FRONT (NORTH) OF THE EXISTING HEDGE SO AS NOT TO CUT INTO IT TO MAKE ROOM FOR THE TREES. PLEASE SEE ILLUSTRATIONS BELOW:**

# CIVICA PINECREST

11.15.23



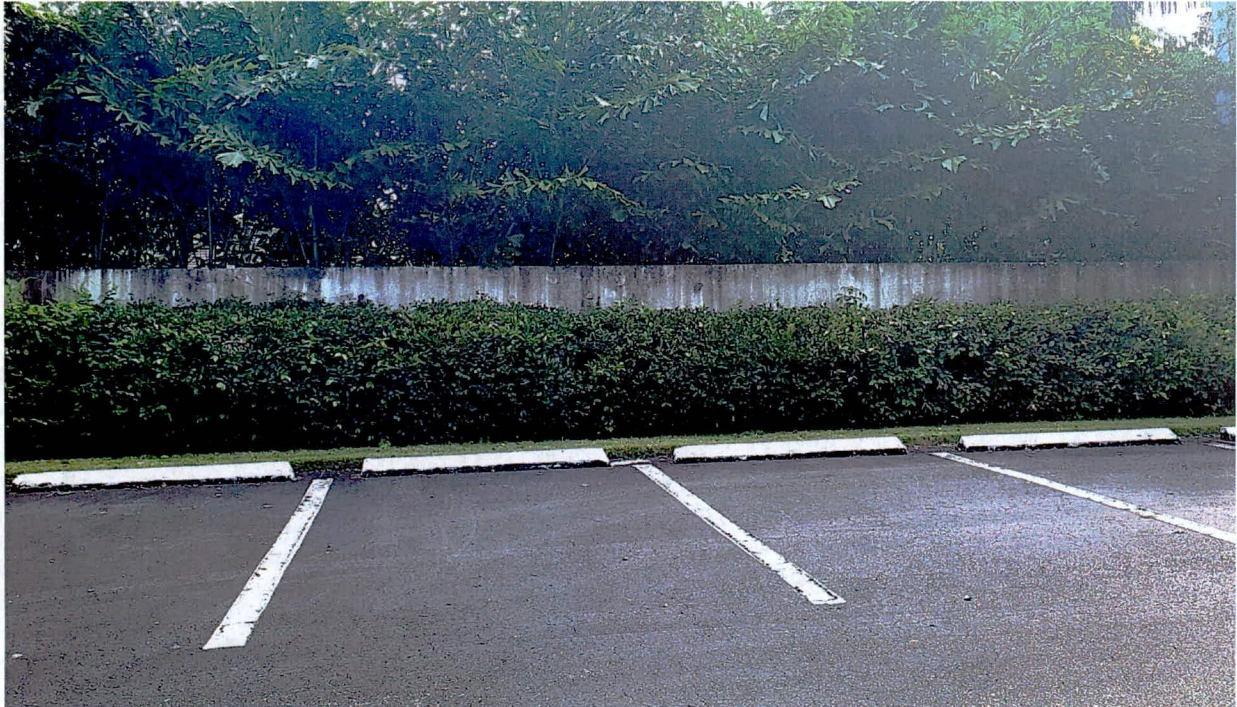
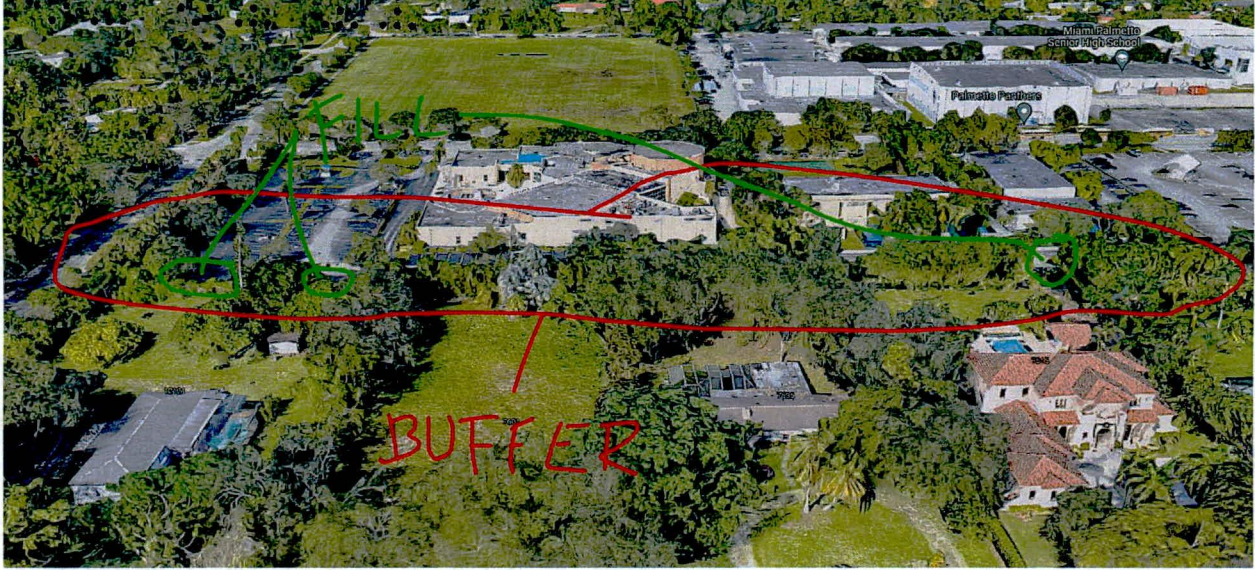
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**COMMENT-17:** THE EXISTING/PROPOSED LANDSCAPE BUFFER ADJACENT TO THE PUBLIC RIGHT-OF-WAY IS INCONSISTENT WITH REQUIREMENTS OF THE VILLAGE'S LAND USE REGULATIONS. PLEASE PROVIDE A BUFFER CONSISTENT WITH THE FOLLOWING REQUIREMENTS:

...ALL VEHICULAR USE AREAS ADJACENT TO A RIGHT-OF-WAY OR PRIVATE STREET SHALL BE SCREENED BY A CONTINUOUS HEDGE PLANTING AT LEAST FOUR FEET HIGH AND SHALL INCLUDE A FIVE FEET LANDSCAPED STRIP INCORPORATING SAID PLANTING OR WALL ON PRIVATE PROPERTY. PLANTING MATERIAL SHALL BE INSTALLED AT A MINIMUM OF 24 INCHES ON CENTER AND SHALL HAVE A HEIGHT OF 30 INCHES AT TIME OF PLANTING.

**RESPONSE:**

***THE APPLICANT WILL COMPLY. SEE REVISED LANDSCAPING DRAWINGS.***

***PLEASE SEE PROVIDED BY AN EXISTING HEDGE MEETING THE MINIMUM REQUIREMENTS ADJACENT TO ALL PUBLIC RIGHT-OF-WAYS AND PRIVATE STREETS. THE ONLY "VUA" AREA WITHOUT AN EXISTING BUFFER IS ALONG THE EAST PROPERTY LINE WHERE IT DOES NOT ABOUT A PRIVATE ROAD OR RIGHT OF WAY.***

***THIS EAST BUFFER IS AGAINST AN EXISTING PARKING LOT SERVING MIAMI PALMETTO SENIOR HIGH SCHOOL AND DOES NOT MEET THE CRITERIA TO REQUIRE A BUFFER; HOWEVER, ONE HAS BEEN ADDED TO BUFFER OUR PROJECT FROM THE ADJACENT LOT.***

**COMMENT-18:** RESOLUTION 4-ZAB-90-76, CONDITION 3, REQUIRES "DENSE AND ADEQUATE LANDSCAPING" TO BE INSTALLED AT THE REAR TO SERVE AS A BUFFER TO THE PROPERTY TO THE SOUTH. IT DOES NOT APPEAR THAT THE BUFFER HAS BEEN MAINTAINED. PLEASE PROVIDE EVIDENCE THAT CONDITION 3 IS BEING MET.

**RESPONSE:**

***THE APPLICANT WILL COMPLY. SEE REVISED LANDSCAPING DRAWINGS.***

***PLEASE SEE DETAILED RESPONSE TO AFOREMENTIONED COMMENT NO.13 ON THIS DOCUMENT.***

**COMMENT-19:** MIAMI-DADE COUNTY RESOLUTION Z-114-86 APPROVED A VARIANCE FOR 158 PARKING SPACES INSTEAD OF 1,556 AS OTHERWISE REQUIRED. THE SUBMITTED PLAN INCLUDES PARKING ALONG THE SOUTHERN PORTION OF THE PROPERTY WITH AN OVERALL TOTAL OF 204 PARKING SPACES. PLEASE INDICATE HOW AND WHEN THE ADDITIONAL PARKING SPACES WERE APPROVED.

**RESPONSE:**

**THE APPLICANT WILL COMPLY.**

**PLEASE NOTE THAT THE MDC RESOLUTION ONLY STIPULATES THE MINIMUM NUMBER OF PARKING SPACES REQUIRED (AS PER AN ESTABLISHED VARIANCE), HOWEVER RESOLUTIONS (IN GENERAL) DO NOT PREVENT THE CONSTRUCTION OF ADDITIONAL PARKING SPACES FROM BEING APPROVED BY THE JURISDICTION.**

**FOR CONVENIENCE, PLEASE SEE RECORD DRAWINGS OF DETAILED ENGINEERING DRAWINGS BY ZYSKOVIUCH ARCHITECTS. THESE PARKING SPACES WERE REVIEWED, APPROVED AND INSPECTED BY THE APPLICABLE JURISDICTION.**

**THESE COMPLETE DRAWINGS INDICATE THE ORIGIN OF THE PARKING SPACES. PLEASE SEE SHEETS A-6.3, A-6.4, A-6.5, A-6.6 SHOWING THE CIVIL DRAWINGS THAT WERE PERMITTED FOR THE PARKING LOT CONSTRUCTION. PLEASE SEE ALSO EVEN WATER & SEWER DRAWINGS INCLUDING THE SAME SITE PLAN IMPROVEMENTS.**

**COMMENT-20:** PLEASE INDICATE THE NUMBER OF SEATS TO REMAIN IN THE TEMPLE AFTER ANY REMODELING AND INDICATE THE FLOOR AREA OF THE PROPOSED SCHOOL AFTER REMODELING.

**RESPONSE:**

**THE APPLICANT WILL COMPLY.**

**PLEASE REFER TO THE GENERAL NOTE ON REVISED A-1.0. INDICATING THE TEMPLE SEATING CAPACITY TO ACCOMMODATE 1,366 OCCUPANTS.**

**COMMENT-21:** PLEASE INDICATE THE NUMBER OF PARKING SPACES TO BE PROVIDED FOR THE ADDITIONAL 275 STUDENTS, CONSISTENT WITH THE PARKING REQUIREMENTS OF APPROVED RESOLUTIONS AND THE VILLAGE'S LAND DEVELOPMENT REGULATIONS.

**RESPONSE:**

**THE APPLICANT WILL COMPLY. PLEASE REFER TO REVISED A-1.0 SHOWING PARKING DATA CHART:**

- **K-8<sup>TH</sup> PROPOSED SCHOOL REQUIRES 44 SPACES, INCLUDING VISITORS AND STAFF.**
- **THE PRE-K, DAYCARE/NURSERY REQUIRE 3 SPACES.**
- **THE TEMPLE SYNAGOGUE IS A NON-CONCURRENT USE AND ITS REQUIREMENT FOR PARKING IS LIMITED TO 158 SPACES AS PER APPROVED RESOLUTION Z-114-86.**

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**END OF COMMENT RESPONSES.**

Dear Mr. Olmsted, thanks to you and to the department for the professional and thorough review and kind comments. We trust that these detailed responses to comments, updated traffic study, and revised drawings will demonstrate compliance with all of the Village's requirements.

Should you have any questions, please do not hesitate to contact me or contact Ms. Melodie Suarez at 305-593-9959.

***Respectfully Submitted***

***Ivan Fajardo***

***CIVICA, LLC.***

72303119

55.31.11

RESPONSES TO ZONING DEPARTMENT COMMENTS :

PROJECT NAME..... True North Classical Academy at Pinecrest Campus

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DISCIPLINE: DEPARTMENT OF BUILDING & PLANNING  
 REVIEWER: STEPHEN R. OLMSTED, AICP  
 PROCESS NUMBER Z-68024  
 DATE OF THIS DOCUMENT 09.03.2024  
 PREPARED BY: IVAN FAJARDO

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**COMMENT-1:** THE REVISED TRAFFIC STUDY HAS BEEN PROVIDED TO THE VILLAGE’S TRANSPORTATION ENGINEERING CONSULTANT. A COPY OF THE TRAFFIC STUDY REVIEW COMMENTS PROVIDED BY CHOICE ENGINEERING ON JANUARY 22, 2024 IS ATTACHED.

**RESPONSE:**

**THE APPLICANT WILL COMPLY; PLEASE SEE THE REVISED TRAFFIC STUDY AS PART OF THIS SUBMITTA.**

**IT ADDRESSES ALL THE COMMENTS ISSUED BY THE VARIOUS DEPARTMENTS AND BY CHOICE ENGINEERING. THE, TRAFFIC ENGINEER OF RECORD, LANGAN ENGINEERING HELD VARIOUS MEETINGS WITH THE REVIEWERS AND HAVE COMPLETED THE REVISED ANALYSIS.**

**PLEASE SEE SEPARATE “COMMENTS REPOSE” DOCUMENT ATTACHED TO EACH TRAFFIC STUDY PACKAGE.**

**COMMENT-2:** PLEASE NOTE THAT THE CONDITION OF APPROVAL #3 OF MIAMI DADE COUNTY RESOLUTION 4-ZAB-68-79 REQUIRES THAT “THE DRIVEWAY TO THE EXISTING RESIDENCE FOR THE DAY NURSERY ON SW 77 AVENUE BE CLOSED AND THAT THE AREA BE APPROPRIATELY LANDSCAPED”. INCLUDING A DRIVEWAY ON SW 77 AVENUE WOULD BE CONTRARY TO THE APPROVED RESOLUTION AND A REQUEST TO ADD A DRIVEWAY WILL REQUIRE AMENDMENT OF YOUR APPLICATION TO INCLUDE THIS REQUEST. ADDITIONALLY, EXCLUSION OF THE DRIVEWAY HAS SERVED TO BUFFER THE EXISTING LOW DENSITY RESIDENTIAL USES TO THE SOUTH, WEST, AND NORTHWEST FROM THE MORE INTENSIVE NON-RESIDENTIAL ACTIVITY AND TRAFFIC GENERATED BY THE TEMPLE AND SCHOOL. PLEASE EXPLAIN A CHANGE IN CONDITIONS THAT WOULD WARRANT ACCESS TO SW 77 AVENUE NOW WHEN CONDITIONS REQUIRED THAT IT BE EXCLUDED FROM CONSIDERATION IN 1979.

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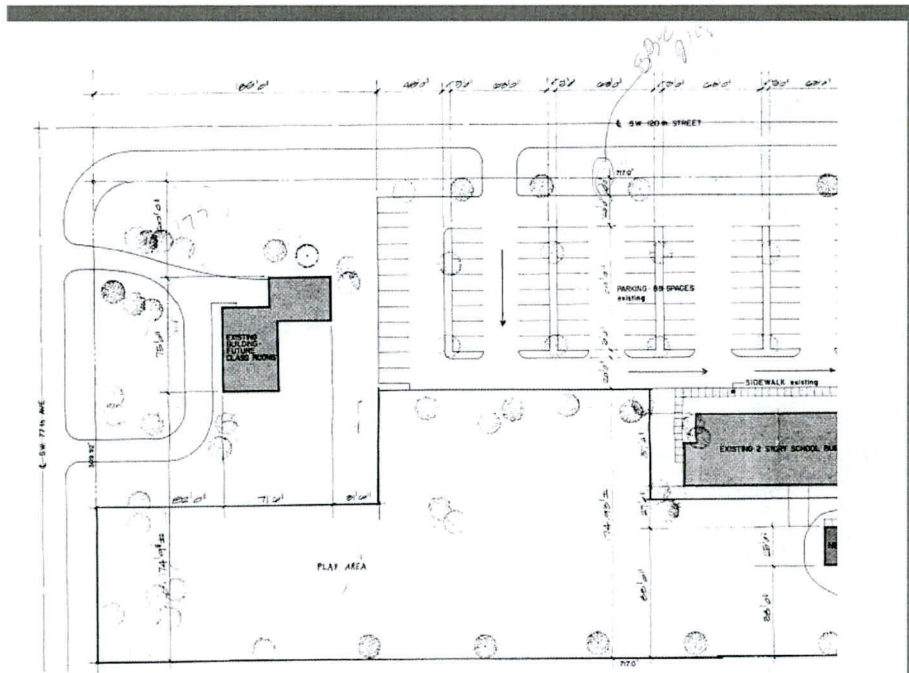
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**RESPONSE:**

**THE APPLICANT WILL COMPLY. AS PER THE REVISED LETTER OF INTENT INCLUDED ON THIS RESUBMITTAL:**

**“THE PORTION OF THE PROPERTY ADDRESSED BY THIS CONDITION WAS AN EXISTING SINGLE-FAMILY HOME, WHICH APPARENTLY HAD BEEN ACQUIRED BY THE TEMPLE.”**

**“THE PROPOSAL WAS TO KEEP THE EXISTING SINGLE FAMILY HOME BUILDING IN PLACE, AND SIMPLY USE THAT EXISTING STRUCTURE FOR THE DAY NURSERY, AS SHOWN IN THE BELOW DETAIL FROM THE APPROVED PLAN.”**



**“THE THEN-EXISTING DRIVEWAY WAS DESIGNED FOR A SINGLE-FAMILY HOME, NOT FOR THE PROPOSED NEW USE OF THE STRUCTURE AS A DAY NURSERY – PARTICULARLY GIVEN THE PROXIMITY OF THE NORTHERN ACCESS TO THE INTERSECTION OF SW 77TH AVENUE AND SW 120TH STREET. SINCE THE SINGLE-FAMILY DRIVEWAY WAS NOT DESIGNED FOR INSTITUTIONAL USE, IT WAS APPROPRIATE TO REMOVE THAT EXISTING DRIVEWAY.”**

**“OF COURSE, NOT ONLY WAS THE THEN-EXISTING DRIVEWAY REMOVED, BUT THE SINGLE-FAMILY STRUCTURE HAS ALSO LONG SINCE BEEN REPLACED BY THE TEMPLE FACILITY.”**

**“WITH THE REMOVAL OF THE THEN-EXISTING DRIVEWAY, THE CONDITION WAS LONG AGO SATISFIED. THE CONDITION DOES NOT PROHIBIT FUTURE CONNECTIONS TO 77TH AVENUE, AND THERE IS NOTHING IN THE ZONING FILE TO INDICATE THAT THE PURPOSE WAS TO PROHIBIT SUCH FUTURE CONNECTIONS: MERELY TO REMOVE THE THEN-EXISTING DRIVEWAY THAT WAS NOT DESIGNED FOR THE PROPOSED NEW USE. THE CONDITION WAS NOT INTENDED TO PROHIBIT THE TEMPLE AND SCHOOL USE FROM HAVING A**

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**SUITABLY DESIGNED DRIVEWAY CONNECTION TO SW 77TH AVENUE, MERELY THE ELIMINATION OF THE VESTIGIAL SINGLE-FAMILY DRIVEWAY. THE APPLICANT DOES NOT BELIEVE THAT THE CONDITION HAS ANY BEARING ON THE NEW PROPOSAL; BUT IN ORDER TO AVOID CONFUSION, REQUESTS THAT THE CONDITION BE DELETED AS PART OF THE PUBLIC HEARING.”**

**COMMENT-3:** THE REVISED TRAFFIC IMPACT STUDY INDICATES THAT THE LEVEL OF SERVICE ON SW 77 AVENUE WILL DETERIORATE FROM EXISTING AND ADOPTED LEVEL OF SERVICE D TO LEVEL OF SERVICE F BETWEEN SW 120 STREET AND SW 124 STREET DURING THE MORNING PEAK HOUR. PLEASE PROVIDE AN EXPLANATION OF THE MEASURES TEMPLE BET SHIRA PROPOSES TO TAKE TO MITIGATE THE PROJECTED DETERIORATION IN LEVEL OF SERVICE AS NECESSARY TO MAINTAIN THE ADOPTED LEVEL OF SERVICE D.

**RESPONSE:**

**THE APPLICANT WILL COMPLY; PLEASE NOTE THAT THE LEVEL OF SERVICE AT THIS PORTION OF ROW SHALL MAINTAIN ACCEPTABLE LEVELS BY THE IMPLEMENTATION OF THE FOLLOWING THREE IMPROVEMENTS:**

- 1. THE CONSTRUCTION OF A RIGHT-TURN LANE ON PALMETTO ROAD, FOR NORTHBOUND VEHICLES TURNING EAST ON S.W 120<sup>th</sup> STREET. THIS SHALL REMOVE SCHOOL TRAFFIC FROM THE NORTHBOUND TRAFFIC TRAVELING THROUGH THE INTERSECTION.**
- 2. THE EXPANSION OF THE LEFT-TURN LANE OF SW 77<sup>TH</sup> AVENUE FOR SOUTHBOUND VEHICLES TURNING ONTO 120<sup>TH</sup> STREET.**
- 3. THE TRAFFIC-LIGHT TIMING SHALL BE OPTIMIZED FOR MORE EFFICIENT PROCESSING OF VEHICLES.**

**IN ADDITION, THE SCHOOL IS PROPOSED TO OPERATE IN SEPARATE ARRIVAL AND DIMISSAL SHIFTS. THIS DIVISION OF IMPACT SHALL HELP TO PROVIDE REQUIRED LEVEL OF SERVICE AT THE AFOREMENTIONED ROW.**

**COMMENT-4:** SHEET A-5.0 OF THE SET OF REVISED PLANS INDICATES THAT 150 STUDENTS OF THE PERMANENT STUDENT COUNT WILL ALSO BE ENROLLED IN “AFTER SCHOOL CARE”. PLEASE CONFIRM HOURS OF AFTER SCHOOL CARE WILL NOT EXTEND PAST REQUESTED HOURS OF OPERATION BETWEEN 7:17AM TO 5:30PM

**RESPONSE:**

**APPLICANT WILL COMPLY; PLEASE NOTE THAT THE PROPOSED HOURS OF OPERATIONS HOURS OF OPERATIONS FOR THE AFTERCARE SHALL BE FROM 7:15AM TO 6:00PM.**

**PLEASE REFER TO THE UPDATED L.O.I., OFFICIALLY INDICATING THESE TIMES AS PART OF THIS APPLICATION.**

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**COMMENT-5:** SHEET A-5.0 OF THE SET OF REVISED PLANS INDICATES 600 STUDENTS IN GRADES K THROUGH 8 AND 100 CHILDREN IN THE PRE-SCHOOL AND DAYCARE PROGRAMS. THIS IS AN INCREASE OF 375 STUDENTS AND CHILDREN BEYOND THE CURRENT APPROVED MAXIMUM OF 325. PLEASE REVIEW THE CALCULATIONS IN CIVICA'S RESPONSE LETTER AND REVISE CALCULATIONS AND ASSUMPTIONS TO INCLUDE CONSIDERATION OF 700 STUDENTS AND CHILDREN INSTEAD OF 600 AS PROPOSED IN YOUR FIRST SUBMITTAL.

**RESPONSE:**

***THE APPLICANT WILL COMPLY; PLEASE REFER TO THE REVISED SHEET A-5.0 INDICATING THAT THE MAXIMUM NUMBER OF STUDENTS SHALL NOT EXCEED A TOTAL OF 600 (ALL GRADE LEVELS COMBINED).***

***IN ADDITION, REVISED DRAWING TABLE INDICATES THAT AT NO POINT SHALL THE PRE-SCHOOL / DAYCARE USES EXCEED 100 STUDENTS.***

***PROPOSED GRADES K-8<sup>TH</sup> USES SHALL NEVER EXCEED 575 STUDENTS.***

***PLEASE REFER TO REVISED L.O.I. LETTER INDICATING 600 STUDENTS MAX.***

**COMMENT-6:** THE ORIGINAL APPLICATION SUBMITTED BY THE APPLICANT INDICATES A REQUESTED INCREASE IN STUDENT ENROLLMENT FROM 325 STUDENTS TO 600 STUDENTS, WHEREAS THE COMMENT RESPONSE LETTER FROM CIVICA DATED NOVEMBER 15, 2023 REFERS TO A TOTAL REQUESTED ENROLLMENT OF 700 STUDENTS. PLEASE CONFIRM THE TOTAL STUDENT ENROLLMENT PROPOSED BY APPLICANT.

**RESPONSE:**

***THE APPLICANT WILL COMPLY.***

***PLEASE REFER TO REVISED L.O.I. AND REVISED DRAWING SHEETS A-1.0 AND A-5.0, INDICATING A PROPOSED MAXIMUM ENROLLMENT OF 600 STUDENTS IN TOTAL. THIS NUMBER IS INCLUSIVE OF PRE-SCHOOL / DAYCARE AND GRADES K-8<sup>TH</sup>.***

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**COMMENT-7:** THE COMMENT RESPONSE LETTER PROVIDED BY CIVICA ON NOVEMBER 15, 2023 INDICATES THAT 9,500 SQUARE FEET OF OUTDOOR RECREATION AREA IS PROVIDED FOR PRE-K, NURSERY, AND DAYCARE CLASSES AT TEMPLE BET SHIRA AND THAT THIS EQUATES TO 60.13 SQUARE FEET PER CHILD. BASED ON THIS STATEMENT, IT IS CALCULATED THAT THERE ARE 158 STUDENTS/CHILDREN IN THE PRE-K/ DAYCARE/ NURSERY PROGRAM AT TEMPLE BET SHIRA. IN RECENT CORRESPONDENCE ADDRESSED TO THE VILLAGE OF PINECREST, TRUE NORTH INDICATES THAT THEY CURRENTLY HAVE AN EXISTING STUDENT ENROLLMENT OF 230 STUDENTS. COMBINED, THE TOTAL OVERALL NUMBER OF STUDENTS AND CHILDREN CURRENTLY AT TEMPLE BET SHIRA (387) EXCEEDS THE MAXIMUM NUMBER OF 325 STUDENTS AND CHILDREN CURRENTLY PERMITTED. PLEASE PROVIDE AN ANALYSIS AND CERTIFICATION OF 1.) THE NUMBER OF STUDENTS PRESENTLY ENROLLED AT TRUE NORTH, AND 2). THE NUMBER OF CHILDREN PRESENTLY ENROLLED IN THE TEMPLE BET SHIRA PRESCHOOL, DAYCARE, AND NURSERY.

**RESPONSE:**

**PLEASE NOTE THAT THE RESPONSE TO COMMENT No.14 ON THE 2023 ORIGINAL CIVICA RESPONSE DOCUMENT ONLY DEMONSTRATES THAT THE AVAILABLE SPACE IS SO LARGE THAT WHEN APPLIED TO THE PROPOSED HIGHEST NUMBER STUDENTS PART OF DAYCARE/PRESCHOOL, NURSERY, OR AFTERCARE USES YIELDS 60.13 SQ.FT. PER STUDENT. (NOTE THAT THE CALCULATION PROCEDURES REQUIRE FOR THE NUMBER TO BE DIVIDED IN HALF)**

**\*WE HAVE RECHECKED THE RESPONSE, AND WE COULD NOT FIND ANY STATEMENT INDICATING THAT THE RESULTING NUMBER REPRESENTED THE CURRENT-EXISTING NUMBER OF STUDENTS AND CHILDREN AT THIS FACILITY.**

**PLEASE NOTE THAT THE RECREATION AREA PROVIDED IS 9,500 SF; AND THIS AREA FAR EXCEEDS THE 6,750 SF. REQUIRED BY THE PROPOSED USES UNDER THIS APPLICATION.**

**WE HAVE REQUESTED CONFIRMATION OF THE EXISTING NUMBER OF STUDENTS AND CHILDREN AT THIS PROPERTY. THIS INFORMATION SHALL BE MADE AVAILABLE AS SOON AS IT IS RECEIVED.**

**COMMENT-8:** PLEASE NOTE THAT RESOLUTION 4-ZAB-68-79 INCLUDES A CONDITION OF APPROVAL STATING, "NO NEW OR ADDITIONAL SIGNS SHALL BE PERMITTED". IT APPEARS THAT ADDITIONAL SIGNAGE IS PROPOSED TO BE ADDED TO THE EXISTING FENCE AND WALL OUTSIDE OF THE ESTABLISHED SIGN AREA. PLEASE INDICATE HOW YOU INTEND TO COMPLY WITH THE SIGNAGE REQUIREMENTS OF RESOLUTION 4-ZAB-68-79.

**RESPONSE:**

**THE APPLICANT WILL COMPLY; PLEASE SEE REVISED ARCH DRAWING A7.0 INDICATING NO NEW SIGNS BEING PROPOSED. THE EXISTING FENCE WALL AND THE EXISTING SIGNAGE SHALL REMAIN UNCHANGED.**

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**COMMENT-9:** PLEASE INDICATE THE AREA IN SQUARE FEET OF EACH CLASSROOM AND THE NUMBER OF STUDENTS PROPOSED TO OCCUPY EACH CLASSROOM.

**RESPONSE:**

**THE APPLICANT WILL COMPLY; PLEASE REFER TO THE REVISED DRAWINGS A2.0 & A4.0 INDICATING THE AREAS FOR EACH CLASSROOM.**

**IN ADDITION, PLEASE SEE ALSO THE OCCUPANCY LOAD PER CLASSROOM. THIS IS BASED ON THE FLORIDA FIRE PREVENTION CODE, AND THE FLORIDA BUILDING CODE OCCUPANCY LOAD FACTORS.**

**PLEASE NOTE THAT AT FULL OCCUPANCY, THE CLASSROOMS CAN ACCOMMODATE UP TO 659 STUDENTS, HOWEVER THE PROPOSED MAXIMUM SHALL NOT EXCEED 575 STUDENTS GRADES K-8TH.**

**COMMENT-10:** CIVICA'S RESPONSE LETTER INDICATES 29 CLASSROOMS WILL BE PROVIDED. HOWEVER, SIX (6) OF THE INDICATED CLASSROOMS ARE SHOWN AS "OPTIONAL FUTURE CLASSROOMS". CLASSROOMS SHOULD NOT BE SHOWN AS "OPTIONAL FUTURE CLASSROOMS." CLASSROOM SIZE – THE CURRENT SCHOOL FACILITY IS APPROVED AND UTILIZED TO ACCOMMODATE 325 STUDENTS. PLEASE ADVISE HOW YOU ARE PROPOSING TO ACCOMMODATE 700 STUDENTS IN A FACILITY ORIGINALLY DESIGNED TO ACCOMMODATE 325 STUDENTS, AND PROVIDE CALCULATIONS SHOWING THE NUMBER OF STUDENTS THAT CAN BE ACCOMMODATED CONSISTENT WITH SREF REQUIREMENTS.

**RESPONSE:**

**THE APPLICANT WILL COMPLY; PLEASE REFER TO THE REVISED DRAWING A4.0, INDICATING THE SIX CLASSROOMS SHADED IN ORANGE AS A "FUTURE CLASSROOMS." THESE SHALL BE CONSTRUCTED IN THE EXISTING OPEN SPACE AND WILL COMPLIMENT THE PROPOSED MAXIMUM 575 STUDENTS (GRADES K-8<sup>th</sup>).**

**PLEASE NOTE THAT AS PER STATE LAW ALL CHARTER SCHOOL FACILITIES ARE EXEMPT FROM SREF REQUIREMENTS.**

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**COMMENT-11:** PLEASE SHOW ALL CLASSROOMS PROPOSED FOR THIS APPLICATION SUBMITTAL AND DELETE REFERENCE TO “OPTIONAL FUTURE CLASSROOMS”. PLEASE ALSO CLARIFY THE MEANING OF BLUE VS. PURPLE SHADING AND PEACH VS YELLOW SHADING.

**RESPONSE:**

**THE APPLICANT WILL COMPLY; PLEASE SEE REVISED DRAWINGS A5.0 & A4.0 REPLACING NOTES STATING “OPTIONAL FUTURE CLASSROOMS” WITH NOTES STATING “FUTURE CLASSROOMS.”**

**IN ADDITION, REVISED SHEET A5.0, PRESENTS CLOUDED NOTES AND GRAPHIC SYMBOL LEGEND WITH COLORS AS PER BELOW:**

- **ORANGE = FUTURE CLASSROOMS TO BE BUILT NOT CURRENTLY EXISTING**
- **BLUE = EXISTING CLASSROOMS**
- **RED = EXISTING PRE-SCHOOL & DAYCARE AREAS**
- **RED STRIPES = INFANT / SLUMBER AREA**
- **YELLOW = CORRIDOR & EGRESS/EXIT PATHWAY IN SANCTUARY BLDG.**

**COMMENT-12:** THERE ARE LANDSCAPE ISLANDS WITHIN THE EXISTING PARKING LOT THAT DO NOT INCLUDE THE REQUIRED CANOPY TREE. PLEASE INDICATE A SHADE TREE (MINIMUM 14 FEET IN HEIGHT, 3 INCHES IN DIAMETER) FOR THOSE LANDSCAPE ISLANDS IN THE PARKING LOT CURRENTLY MISSING THE REQUIRED TREE.

**RESPONSE:**

**THE APPLICANT WILL COMPLY; PLEASE REFER TO THE UPDATED LANDSCAPING PLANS LA1-01, LA2-01, & LA2-02 INDICATING THE REQUIRED SPECIFICATIONS FOR PROPOSED TREES AND LANDSCAPING MATERIAL. PLEASE SEE INFORMATION REGARDING SPECIES, TRUNK DIAMETER, AND HEIGHT AT TIME OF PLANTING.**

**COMMENT-13:** THE EXISTING/PROPOSED LANDSCAPE BUFFER ALONG THE SOUTHERN PROPERTY BOUNDARY IS INCONSISTENT WITH REQUIREMENTS OF THE LAND DEVELOPMENT REGULATIONS FOR BUFFERS BETWEEN DISSIMILAR USES, AND INCONSISTENT WITH THE REQUIREMENTS OF RESOLUTION 4-ZAB-90-76 CONDITION 3, THAT REQUIRES “DENSE AND ADEQUATE LANDSCAPING” TO BE INSTALLED AT THE REAR TO SERVE AS A BUFFER TO THE PROPERTY TO THE SOUTH. PLEASE IDENTIFY THE EXISTING AND PROPOSED SHRUBS AND TREES ALONG THE SOUTHERN BOUNDARY BY SPECIES, HEIGHT AND SPACING, AND CONFIRM HOW YOU INTEND TO COMPLY WITH CONDITION OF APPROVAL 3 OF RESOLUTION 4-ZAB-90-76. NOTE: MUCH OF THE CLOUDED LANDSCAPING ALONG THE SOUTHERN BOUNDARY IS LOCATED ON THE ADJOINING PROPERTY. REQUIRED LANDSCAPING IS REQUIRED TO BE PLANTED ON THE SUBJECT PROPERTY.

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**RESPONSE:**

**THE APPLICANT WILL COMPLY; PLEASE REFER TO UPDATED DRAWINGS A-1.0 & LA2-01 INDICATING THE REQUIRED BUFFER WIDTH AND LANDSCAPING SHRUB MATERIAL ALONG DESCRIBED BOUNDARIES.**

**PLEASE NOTE STATEMENT INDICATING FOR ANY AREAS OF SHRUBS WITH LESS DENSITY THAN 24" O.C., AND / OR LESS HEIGHT THAN 4 FEET, TO BE FILLED WITH SHRUB PLANTING MATERIAL IN COMPLIANCE WITH THE CODE REQUIREMENTS.**

**COMMENT-14:** THE EXISTING/PROPOSED BUFFER ADJACENT TO THE PUBLIC RIGHT-OF-WAY IS INCONSISTENT WITH REQUIREMENTS OF THE VILLAGE'S LAND DEVELOPMENT REGULATIONS. PLEASE PROVIDE A BUFFER CONSISTENT WITH THE FOLLOWING REQUIREMENTS:

BUFFERS REQUIRED FOR VEHICULAR USE AREA...ALL VEHICULAR USE AREAS ADJACENT TO A RIGHT-OF-WAY OR PRIVATE STREET SHALL BE SCREENED BY A CONTINUOUS HEDGE PLANTING AT LEAST FOUR FEET HIGH AND SHALL INCLUDE A FIVE FEET LANDSCAPED STRIP INCORPORATING SAID PLANTING OR WALL ON PRIVATE PROPERTY. PLANTING MATERIAL SHALL BE INSTALLED AT A MINIMUM OF 24 INCHES ON CENTER SHALL HAVE A HEIGHT OF 30 INCHES AT TIME OF PLANTING.

ADDITIONALLY, THE SUBMITTED PLAN INDICATES REMOVAL OF SEVERAL MATURE TREES FROM THE SW 77 AVENUE RIGHT-OF-WAY. PLEASE PROVIDE AN EXPLANATION REGARDING THE NEED TO REMOVE THESE TREES. ALSO, IDENTIFY EXISTING AND PROPOSED TREES BY SPECIES, HEIGHT AND SPACING.

**RESPONSE:**

**THE APPLICANT WILL COMPLY; SAME AS ABOVE, PLEASE REFER TO UPDATED DRAWINGS A-1.0 & LA2, INDICATING THE EXISTING 5' WIDE GREEN BUFFER WITH 48" HIGH SHRUB SCREEN. THE EXISTING SHRUB IS MATURE AND HAS ACHIEVED DENSITY OF 24" ON CENTER OR LESS.**

**PLEASE NOTE STATEMENT INDICATING FOR ANY AREAS OF SHRUBS WITH LESS DENSITY THAN 24" O.C., AND / OR LESS HEIGHT THAN 4 FEET, TO BE FILLED WITH SHRUB PLANTING MATERIAL IN COMPLIANCE WITH THE CODE REQUIREMENTS.**

**TREES ALONG SW 77<sup>TH</sup> AVENUE AFFECTED BY THE PROPOSED RIGHT TURN LANE SHALL BE MITIGATED ACCORDINGLY. (PLEASE SEE LANDSCAPE DISPOSITION PLAN LA2-01). THIS RIGHT-TURN LANE IS REQUIRED AS PART OF THE TRAFFIC ENGINEERING IMPROVEMENTS FOR THE INTERSECTION AND LEVEL OF SERVICE.**

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**COMMENT-15:** THE SHADED AREA OUTLINED WITH A RED DASHED LINE IS INDICATED AS PERVIOUS. WILL THE PROPOSED SPORTS COURT WITHIN THIS AREA BE PERVIOUS OR IMPERVIOUS? PLEASE CLARIFY.

**RESPONSE:**

**APPLICANT WILL COMPLY; PLEASE NOTE THAT THE EXISTING SPORTS COURT IN THE RECREATION FIELD IS IMPERVIOUS, AND AS SUCH, IT IS EXCLUDED FROM PERVIOUS AREAS CALCULATIONS.**

**THIS SPORTS COURT IS INCLUDED IN THE 140,748 SF FIGURE OF "IMPERVIOUS" SITE AREAS REPRESENTED IN THE ZONING CALCULATIONS.**

**FUNALLY, THE PROPOSED PERVIOUS AND IMPERVIOUS AREAS UNDER THIS APPLICATION, FULLY COMPLY WITH THE VILLAGE'S CODE REQUIREMENTS.**

**COMMENT-16:** A VARIANCE WAS APPROVED IN 1986 TO ALLOW A REDUCTION IN REQUIRED OUTDOOR RECREATION AREA FROM 82,450 SQUARE FEET TO 49,972 SQUARE FEET. THE PROPOSED INCREASE IN STUDENT ENROLLMENT FURTHER INCREASES THE NEED FOR OUTDOOR RECREATION AREA. REVISED PLANS SHOW 9,500 SQUARE FEET OF OUTDOOR RECREATION AREA FOR PREK, K, AND AFTERSCHOOL PROGRAMS AND CIVICA'S RESPONSE LETTER INDICATES 28,000 SQUARE FEET OF OUTDOOR RECREATION AREA FOR STUDENTS IN GRADES K THROUGH 8. COMBINED, THIS IS LESS THAN THE AMOUNT OF SPACE REQUIRED BY THE APPROVED 1986 VARIANCE. PLEASE PROVIDE CALCULATIONS DEMONSTRATING THAT THE EXISTING AND PROPOSED OUTDOOR RECREATION AREA IS SUFFICIENT TO MEET THE REQUIREMENTS OF THE STATE REQUIREMENTS FOR EDUCATIONAL FACILITIES (SREF); AND CONSISTENT WITH DIVISION 5.10 OF THE VILLAGE'S LAND DEVELOPMENT REGULATIONS FOR THE PRE-SCHOOL, DAY NURSERY, AND AFTER SCHOOL CARE USES.

**RESPONSE:**

**APPLICANT WILL COMPLY, PLEASE NOTE THAT AS A CHARTER SCHOOL, THE K-8<sup>TH</sup> FACILITY (575 MAX STUDENTS PROPOSED) IS EXEMPT FROM COMPLIANCE WITH SREF REQUIREMENTS, AND THUS, SHALL NOT REQUIRE ANY SPECIFIC RECREATION AREA SIZE.**

**THE ONLY APPLICABLE RECREATION AREA UNDER THIS APPLICATION SHALL BE MANDATED FOR THE 300 STUDENTS ENROLLED IN PRESCHOOL, NURSERY, AND AFTERCARE PROGRAM. THIS CASE YIELDS 150 STUDENTS WHICH IS HALF OF 300 STUDENTS.**

**THUS, THE APPLICABLE EQUATION SHALL BE:.....150 x 45 SQ.FT. = 6,750 SQ.FT.**

**UNDER THIS PROPOSED APPLICATION THE MINIMUM AREA REQUIRED IS 6,750 SQ.FT.; HOWEVER, THIS SITE ALREADY OFFERS A RECREATION AREA OF 9,500 SQ.FT. OF RECREATION WHICH IS BEING PROPOSED.**

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**COMMENT-17:** IN THE EVENT YOU WILL BE IMPACTING ANY EXISTING UTILITIES IN THE PUBLIC RIGHT-OF-WAY, PLEASE ADVISE IF YOU HAVE COORDINATED WITH UTILITY PROVIDERS AND/OR RECEIVED CONSENT FOR INSTALLATION OR MODIFICATION OF THE UTILITIES.

**RESPONSE:**

**THE APPLICANT WILL COMPLY; PLEASE NOTE THAT THE APPLICANT HAS SECURED A SPECIALIZED CONSULTANT TO ASSIST WITH THIS ITEM, AND HAS ALREADY ESTABLISHED DIALOG WITH FP&L.**

**THE UTILITY COMPANY HAS CONFIRMED THAT THE EXISTING OVERHEAD INFRASTRUCTURE AND POWERPOLES CAN BE SHIFTED OR BURIED UNDERGROUND. ALL FUTURE UTILITY WORK SHALL BE COORDINATED AHEAD OF TIME PRIOR TO CONSTRUCTION OF ANY ROW IMPROVEMENTS.**

**COMMENT-18:** PLEASE CORRECT THE YELLOW SHADED AREA ON THE LOCATION MAP ON THE COVER SHEET TO EXCLUDE THE MIAMI PALMETTO SENIOR HIGH SCHOOL PARKING LOT.

**RESPONSE:**

**THE APPLICANT WILL COMPLY; WE APOLOGIZE FOR THIS INCONVENIENCE. PLEASE SEE THE REVISED LOCATION MAP ON COVER SHEET INDICATING CORRECTED GEOMETRY.**

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END OF RESPONSES TO COMMENT

DEAR MR. OLMSTED, THANKS TO YOU AND TO THE DEPARTMENT FOR THE THOROUGH REVIEW AND KIND COMMENTS.

WE TRUST THAT THESE DETAILED RESPONSES TO COMMENTS, THE REVISED DRAWINGS, AND THE UPDATED TRAFFIC STUDY WILL ALLOW FOR APPROVAL OF THIS APPLICATION.

PLEASE DO NOT HESITATE TO CONTACT US WITH ANY QUESTIONS, OR FOR ANY ASSISTANCE.

*Respectfully Submitted*

*Ivan A. Fajardo  
Project Manager*



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3 September 2024

Mr. Stephen R. Olmsted, AICP, LEED-GA,  
Planning Director  
Building and Planning Department  
Village of Pinecrest

**Re: Traffic Impact Study – Response to Comments**  
**Project Name: True North Academy Pinecrest at Bet Shira**  
**Langan Project No.: 330118701**

Dear Mr. Olmsted:

Please accept this letter as the response to the comments received on January 22, 2024 for the above-mentioned project.

## I. Traffic Impact Study

*Comment 2. Section 1.3:*

1. Due to the presence of Palmetto High School across the study location, a 30-minute gap between the drop-off and pick-up shifts for the student grades may not be enough. Exhibits 1 and 2 from Google Maps show stop-and-go traffic conditions on SW 120 Street for both directions between Palmetto Road and SW 72 Avenue from approx. 6:30 AM to 8:00 AM, and from approx. 1:45 PM to 3:30 PM. Additionally, the Synchro output reports show longer morning queues for Build Conditions for the southbound left-turn and northbound approach at the intersection of SW 120 Street and Palmetto Road. As discussed in the next bullet, the school speed zone will be increased during the arrival and dismissal periods from 45-minutes to 2 hours, 7:00 AM to 9:00 AM and 2:00 PM to 4:00 PM. Vehicles traveling to/from the school during the arrival/dismissal periods will be further delayed due to the longer queues and 2 hour 15-mph speed zone periods. Please identify a mitigation strategy.

a) *1<sup>st</sup> Follow up 01/22/2024: In PDF page 38, the proposed site plan does not show an egress route to driveway #3 (connection to Palmetto Road) as stated in PDF page 8 (or report page 3) – “The driveway connection to Palmetto Road (Driveway 3) will operate as an ingress and egress right-turn only driveway for school staff and parents during drop-off/pick-up times.” Additionally, add labels, arrowhead symbols, or a legend to clarify the ingress and egress vehicular travel paths within the school.*

**Response 1.3-a: The site plan shows pavement markings at the connection of Driveway 3 and Palmetto Road to ensure the ingress and egress will be a right-turn only. Additionally, a raised channelized island will be installed at the driveway entrance to ensure left turns are prohibited. The traffic operations plan map also details the route for parents to follow during drop-off and pick up times using Driveway 3.**

b) *1<sup>st</sup> Follow up 01/22/2024: Throughout the report, such as in Section 5.1 and 5.3, clarify the operations of driveway #3. The sentence states “the driveway connection to Palmetto Road (Driveway 3) will operate as right-turn only driveway...”. Clarify that the driveway allows both ingress and egress movements as right-turns only. Similarly,*

**Response 11-3-a: We revised Section 7.0 of the report to include the date when the queuing data was collected.**

- b) a) 1st Follow up 01/22/2024: Per the queue accumulation on SW 120 Street, the eastbound direction has approximately a remaining storage capacity of 11 vehicles during the morning peak period and no capacity during the afternoon peak period. The capacity for Zone 4 from SW 77 Avenue to Zone 3 is approximately 35 vehicles (782 feet). However, the existing eastbound queue in the morning is 24 vehicles and in the afternoon is +22 vehicles, as presented in the next excerpt. Please identify mitigation strategies.

**Response 11-3-b: The school has agreed to construct an exclusive eastbound right-turn lane at the connection of driveway 2 and SW 120<sup>th</sup> Street to alleviate the eastbound traffic along SW 120<sup>th</sup> Street.**

- c) a) 1st Follow up 01/22/2024: Please explain why there is 2 or less vehicles in queue for the westbound direction from SW 77 Avenue to Zone 2, but in Zone 1 there is a queue of +14 vehicles.

**Response 11-3-c: The queue in Zone 1 has more than 14 vehicles in the westbound direction due to the existing traffic generated from the Miami Palmetto Senior High School. It is likely that the westbound traffic in zone 1 when the snapshot queue was taken was waiting for traffic to enter/exit from the east driveway to the parking lot on the south side of SW 120<sup>th</sup> Street (which serves Miami Palmetto Senior High School). This is likely the reason for the sudden decline in traffic between the two zones.**

Comment 12. Section 6.0:

2. Per Florida Statute Section 316.1895(6), any school zone in which the signing has been replaced, a sign stating SPEEDING FINES DOUBLED shall be installed within the school zone. These signs were not observed in the field.

1st Follow up 01/22/2024: Please clarify in Section 8.0 when discussing the need to replace and upgrade the school speed zone signage to the latest standards.

**Response 12.2: We revised the text of the report in Section 8.0 to clarify that the school has committed to upgrading the existing school speed zone to comply with the Florida Statute Section 316.1895(6),**

3. Due to the proximity of the three midblock pedestrian crossings on SW 120 Street between Palmetto Road and SW 74 Court to the school, identify the following needs:
- Determine whether the traffic control at these crossings needs to be upgraded per pedestrian volumes.
  - Install stop bars on SW 120 Street.
  - Install Pedestrian Warning signs with Downward Arrow plaques on each approach of the crossings.
  - Install Stop Here for Pedestrians signs on each approach of the crossings.
  - Determine whether on street parking on the north side of SW 120 Street on both sides of the crossings should be prohibited to provide adequate visibility to pedestrians within the crosswalk markings.

*1st Follow up 01/22/2024: Please note that if these crossings are to be utilized by students of the proposed school expansion, the traffic controls at these crossings must be evaluated. If these crossings are not to be used, then please indicate in the report how this would be enforced.*

**Response 12-3: The three midblock pedestrian crossings currently serve the Miami Palmetto Senior High School, and the easternmost crosswalk currently has rapid flashing beacons to the east and west of the mid-block crossing. The proposed school expansion will serve students from kindergarten through 8<sup>th</sup> grade; as such, we do not anticipate any parents or students will walk to school and utilized the crossings during arrival and dismissal periods. The school will have the authority to enforce this condition.**

4. Per Google Earth street-view image March 2019 (see below), the southbound left-turn from SW 74 Avenue onto SW 120 Street is prohibited on school days from 6:45 AM to 7:45 AM. Please document in the report and determine whether these times need to be modified due to the school expansion. If so, the sign panel would have to be replaced.

*1st Follow up 01/22/2024: Please clarify and add into report's recommendations. Appendix A only includes up to figure 9.*

**Response 12-4: We revised Figure 10 that shows an aerial detailing the proposed school speed zone upgrades of the existing pavement markings and signage and confirmed that the sign showing the southbound left-turn from SW 74 Avenue onto SW 120 Street will be updated with the new school schedule. Please refer to Figure 10.**

6. Per FDOT Speed Zoning Manual (August 2018), the beginning point of the school zone shall be identified using an approved school zone speed limit device assembly as shown to the right. The current assemblies observed in the field do not include the flashing beacon.

*1st Follow up 01/22/2024: Please update the "School Speed Zone Figure" and report text to discuss the addition of flashing beacons at the existing and proposed school zone speed limit assemblies.*

**Response 12-6: We revised the text of the report to call out that the school will install flashing beacons at the existing and proposed school zone speed limit assemblies to meet the current FDOT standards and ensure that drivers know about the presence of the school so they can properly adjust their speed. Please refer to Figure 10.**

7. Recommend coordination for additional parking enforcement to prevent vehicles from blocking the sidewalk, if the parking activity observed through Google Earth street-view is occurring during school operations, as presented in the next exhibit. Vehicles are shown blocking the sidewalk path for pedestrians.

*1st Follow up 01/22/2024: Please update Section 7.0 to include a recommendation to address the observed illegal parking.*

**Response 12-7: We revised the text of Section 7.0 to call out that the school will construct curb and gutter along the south side of SW 120<sup>th</sup> Street to restrict the on-street parking. Please refer to Appendix B for a Concept Sketch.**

Comment 15. Traffic Operations Plan:

2. Pedestrian Access Point 1 occurs on driveway concurrently with vehicles entering and exiting the school. Please provide a separate access point for pedestrians from the sidewalk on the south side of SW 120 Street onto school.

*1<sup>st</sup> Follow up 01/22/2024: Both pedestrian access points provided in the TOP plan conflict with the vehicles entering and exiting the school. Please identify separate access points for pedestrians or clarify in the report how conflicts will be avoided with vehicular traffic at the driveways.*

**Response 15-2: The site plan has been revised to show a pedestrian connectivity to avoid any vehicular conflicts.**

*Comment 16. Other Items:*

1. Please indicate how vehicular movements will be controlled at the third driveway servicing the school during the arrival and dismissal periods. A stop sign and stop bar should be provided for exiting traffic.

*1<sup>st</sup> Follow up 01/22/2024: Please add into report recommendations the strategies to enforce the directional operation of all driveways.*

**Response 16-1: 5. We revised the report to include that the proposed school intends to design and reconstruct all proposed and existing driveways to operate as right-turn only driveways through the construction of raised channelized islands and through improvements to pavements markings, stop bars and stop signs at all driveway connections.**

2. To encourage walking, bicycling, transit-use, and other modes of transportation, consider and evaluate the possibility to add traffic control measures such as:
  - a. Creation of an Employee Transportation Coordinator position to run the transportation demand management (TDM) programs.
  - b. Designated scooter/motorcycle parking spaces
  - c. Carpool incentive program for employees
  - d. Subsidized transit passes for employees
  - e. Providing a Citibike station or bike sharing/rentals
  - f. Providing on-site carsharing for residents
  - g. Providing on-site scooter sharing for residents
  - h. Car/vanpooling designated parking spaces
  - i. Bicycle racks
  - j. 8-foot sidewalks around school property
  - k. Lockers for bicyclists to store a change of clothes will be provided on-site
  - l. Shower facility, bicyclist can use will be provided on-site

*1<sup>st</sup> Follow up 01/22/2024: Please discuss some of these possible strategies with the school personnel and add into the report recommendations.*

**Response 16-2: A Transportation and Demand Management Strategies section was added to the report.**

*Comment 17. Table 7: Please identify mitigation strategy for SW 77 Avenue northbound due to level of service degrading from D to F, due to the 241 school trips (Comment 19).*

**Response 17:** Based on the roadway capacity conducted the expected failure is due to the outgoing traffic from the proposed school new driveway onto SW 77<sup>th</sup> Avenue. Therefore, to mitigate the expected condition, the school has agreed to construct an exclusive northbound right-turn lane at the connection of Driveway 3 to SW 77<sup>th</sup> Avenue, and an additional northbound right turn lane that continues to the intersection of SW 77<sup>th</sup> Avenue and SW 120<sup>th</sup> Street. In doing this, the proposed improvements will increase the capacity of the roadway and the intersection to mitigate the project’s impacts. In addition, it should be noted that these improvements will also benefit traffic for the overall community because as shown on our analysis SW 77<sup>th</sup> Avenue is operating almost at capacity. Also, it should be noted that our analysis is conservative because the trip generation is not taking into account the existing users that are currently attending the school. As such, with the new right turn lane, the roadway is expected to operate within an acceptable LOS.

*Comment 18.* Section 1.0 and Section 2.1 obtain contradictory statements regarding the number of current student enrollment at the school. Please identify mitigation strategy for SW 77 Avenue northbound due to level of service degrading from D to F, due to the 241 school trips (Comment 19).

- a) Section 1.0: “The school currently operates as the Bet Shira Early Childhood Center with a 44-student enrollment from 3 months to Pre-Kindergarten.”

**Response 18-a: Acknowledged. The report was updated to state that “the school opened in the summer of 2023 with a 228-student enrollment”.**

- b) Section 2.1: “It should be noted that based on their current 325-student approval the school opened in the summer of 2023 with a 228-student enrollment and the data collected on 2023 accounts for the traffic currently being generated by the school.”

**Response 18-b: Acknowledged**

*Comment 19.* Section 5.1

- a) Consider rephrasing the sentence “the proposed school is expected to generate 489 net-new morning peak-hour trips...” as it implies that the 489 are all new trips when in actuality some of those trips are already being exhibited in the field.

**Response 19-a: Acknowledged. As mentioned on the study, although the school currently operates with 228-students, we did not account for the approved school nor the existing preschool to provide a conservative analysis. We added additional language to the text of the report to make this clear.**

- b) In Section 7.0, this is contradicted in the second paragraph in report page 18 where it states, “we anticipate a total of 248 net-new project-generated trips during the morning peak hour.”

**Response 19-b: In section 7.0, the total of 248 net-new project-generated trips during the morning peak hour refers to the number of project trips that will travel along SW 120<sup>th</sup> Street between Palmetto Road and SW 74<sup>th</sup> Court. We added additional language to the text of the report to further clarify this. Please refer to Table 7 in the report which calls out the 248 project generated trips that travel in the eastbound direction of SW 120<sup>th</sup> Street during the morning peak hour.**

Comment 20. Section 7.0

- a) In the following sentence revise “Afternoon peak hour” to “morning peak hour”: “We note that during the afternoon peak hour, westbound queueing begins around 6:55 AM ...” (last paragraph, second sentence).

**Response 20-a: We applied the above revisions to the text of the report.**

- b) In the second paragraph in report page 18, the following statement contradicts the finding presented in Section 6.0: “As shown on the roadway capacity analysis the roadway segment is expected to operate within its roadway capacity for the future conditions.”

**Response 20-b: The second paragraph on report page 18 refers to the segment of SW 120<sup>th</sup> Street between Palmetto Road and SW 74<sup>th</sup> Court which is expected to operate within its adopted LOS with the addition of the trips generated by the school expansion.**

Sincerely,

**Langan Engineering and Environmental Services, LLC**



Maximo G. Polanco, P.E.  
Senior Project Manager

MGP:mgp

FL Certificate of Authorization No. 6601

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## **RESPONSES TO ZONING DEPARTMENT COMMENTS :**

PROJECT NAME..... True North Classical Academy at Pinecrest Campus

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DISCIPLINE: DEPARTMENT OF BUILDING & PLANNING  
REVIEWER: STEPHEN R. OLMSTED, AICP  
PROCESS NUMBER Z-68024  
DATE OF THIS DOCUMENT 11.25.2024  
PREPARED BY: IVAN A. FAJARDO

---

### **November 8, 2024 P&Z COMMENTS**

1. The revised traffic study has been reviewed by Choice Engineering, the Village's Transportation engineering consultant. A copy of the traffic study review comments provided by Choice Engineering is attached.

*RESPONSE TO COMMENT: The Applicant will comply.*

*The traffic study shall be resubmitted with the recent data. The updated study will demonstrate compliance with all concurrency requirements.*

---

2. Adjusting and staggering of school hours is recommended in accordance with a schedule that will avoid conflict with drop-off and pick up times at Miami-Palmetto High School on the north side of SW 120 Street.

*RESPONSE TO COMMENT-2: The Applicant will comply.*

*The proposed arrival and dismissal under this application shall not conflict with Palmetto Senior High School's updated schedule.*

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3. Please indicate if True North Academy proposes to provide bus service.

*RESPONSE TO COMMENT-3: The Applicant will comply.*

*Please note that NO bus service is being assumed as part of the Traffic Study Analysis for this site. Please see revised analysis included in this resubmittal.*

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C I V I C A

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TEL: 305.593.9959

4. The Village's Public Works Department has identified the following concerns and requirements:
- a. Existing Palm trees in the right-of-way may not be used to meet the street tree requirements of the Village of Pinecrest.

**RESPONSE TO COMMENT:** *The Applicant will comply.*

*Please see revised landscaping drawing LA2-01 indicating shade trees provided along the property line.*

*As requested, palms trees are not being counted towards the street tree requirement. All trees spacing and specifications at the time of planting shall comply with the Village's requirements for street trees.*

---

- b. Show the southern limits of the construction of the right-turn deceleration lane in SW 77 Avenue.

**RESPONSE TO COMMENT:** *The Applicant will comply.*

*Please refer to revised site plan drawings A-1.0, A-1.1 indicating proposed right-turn lane on SW 77th Avenue.*

---

- c. Please provide detailed dimensions and a cross-section of the SW 77 Avenue roadway adjacent to the Temple Bet Shira. Note: A left turn lane should be 10 feet wide, the through lane should be 11 feet wide and the right turn lane should be 10 feet wide.

**RESPONSE TO COMMENT:** *The Applicant will comply.*

*Please refer to revised site plan drawings A-1.0, A-1.1, & A-7.1 indicating all adjacent ROW widths. Please see also new drawing A-7.2 indicating proposed ROW profiles. Please note that the dimensions of lane width provided exceed the ones requested.*

---

- d. Please show the required 20-foot-wide utility easement within the SW 77 Avenue right-of-way

**RESPONSE TO COMMENT:** *The Applicant will comply.*

*Please note that there were no records found of Utility Easements (E.U.) within the SW 77th Avenue R.O.W. Please refer to the surveys indicating the various 6' U.E within the property.*

---

- e. Curb, gutter, and a pedestrian sidewalk are required to be provided on the east side of SW 77 Avenue adjacent to the subject property.

**RESPONSE TO COMMENT:** *The Applicant will comply.*

*Please refer to revised site plan drawings A-1.0, A-1.1, & A-7.1 indicating an 8'-2" wide concrete sidewalk and Curb-&-Gutter assembly on the east side of the property (fronting SW 77th Ave).*

---

C I V I C A

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- f. If there is insufficient room for a drainage swale adjacent to SW 77 Avenue, positive drainage consisting of a French drain and inlets will be required.

*RESPONSE TO COMMENT: The Applicant will comply.*

*Please note that there will not be a swale space available on the east side of SW 77th Ave; however, ROW drainage and inlet structures approved by the jurisdiction during the permitting process can be provided as required.*

*This shall be a typical condition of approval under the Public Works Department recommendation Memo.*

---

- g. Consider adding a second lane on the south side of SW 120 Street to better accommodate traffic turning right and left on to SW 120 Street from SW 77 Avenue.

*RESPONSE TO COMMENT: Applicant will comply.*

*Only one lane had to be kept at the corner due to the Public Works Department request to increase the green streetscape concept and reduce asphalt footprint, [as per Gulliver Academy]; however, please note that in order to satisfy this comment, the turning radius at the corner has been increased so as to facilitate the right-turn movement at that intersection.*

*Please refer to revised site plan drawings A-1.0, A-1.1, & A-7.1.*

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- h. Enhancement of the SW 120 Street landscaping and streetscape is recommended. Please contact the Public Works Director to review and discuss specific recommendations.

*RESPONSE TO COMMENT: The Applicant will comply.*

*We would like to thank the Department for meeting with the applicant and the design team, and for providing specific guidance in regard to the increase of green areas and the reduction of asphalt footprint.*

*Please see revised A-1.0, A-1.1, A-7.1 and revised landscaping plans LA2-01 presenting an organic, meandering sidewalk design with landscaping matching the design concept implemented at Gulliver Academy.*

---

- i. Please demonstrate that traffic planning to exit the proposed right lane from the Temple to SW 77 Avenue will not be unduly delayed by the traffic queued in SW 77 Avenue.

*RESPONSE TO COMMENT-4i: The Applicant will comply.*

*Please see revised A-1.0 and A-1.1, indicating that the driveway on SW 77th Avenue has been removed from the project.*

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5. Consistent with the Applicant's statement that the proposed increase in students is "to a total of 600 students, which total of 600 students reflects both the enrollment in the True North public charter and the Temple's child care programs", and "at no time will the number of children in the Temple's program exceed 100 of the total 600", please amend your response to former Comments 5, 9, and 10 and the information in the table on Sheet 5.0, to reflect that the proposed maximum number of students in grades K through 8 shall not exceed 500 students.

***RESPONSE TO COMMENT: The Applicant will comply.***

***As per our meeting with the department, at NO point shall the total number of students exceed 600 under this application. The proposed conditions shall be as per below:***

- ***BET SHIRA PRE-SCHOOL DAYCARE + TRUE NORTH CHARTER = 600 MAX STUDENTS***
- ***THE PRE-SCHOOL /DAYCARE SHALL NEVER EXCEED 100 STUDENT ENROLLMENT. THE ENROLLEMNT CAN BE LESS THAN 100 STUDENTS BUT NEVER MORE THAN 100.***
- ***K-8th GRADE SCHOOL ENROLLMENT SHALL NEVER EXCEED 575 STUDENTS.***

***The applicant would like the ability to have the aforementioned flexibility in case that it is needed.***

---

6. Please provide a description of the plant species and specifications for the existing "VUA buffer hedge". Crepe Myrtle and Sabal Palms exist adjacent to SW 77 Avenue and are proposed to be relocated to the east to make room for the proposed right-turn lane. It is recommended that these trees be replaced with canopy trees as required by the Village's Land Development Regulations at the rate of one tree per 30 feet, each tree measuring 14 feet in height, 3 inches diameter at time of planting.

***RESPONSE TO COMMENT-6: The Applicant will comply.***

***Please note that the "VUA" nomenclature is no longer implemented, and its reference has been removed from the revised plans.***

***As per the public works department request, a right-turn lane with sidewalk and curb-&-gutter has been provided along SW 77 Ave; thus, the east landscaping buffer (along SW 77th Avenue) shall be achieved on-site.***

***Appropriate buffers in compliance with the Village Code. Tree species were selected per the FPL Right Tree Right Place guidelines due to the overhead wires along Palmetto Road. All trees shall have a minimum 3" caliper, 14' high at time of planting.***

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7. A separate tree removal permit will be required prior to the removal of any tree.

***RESPONSE TO COMMENT-7: The Applicant will comply.***

***A tree removal permit application shall be submitted to Miami-Dade County Tree Division prior to the removal of any landscaping material.***

---

C I V I C A

8323 NW 12 STREET, SUITE 106, DORAL, FL 33126

TEL: 305.593.9959

8. The existing/proposed landscape buffer along the southern property boundary is inconsistent with requirements of the Land Development Regulations for buffers between dissimilar uses, and inconsistent with the requirements of Resolution 4-ZAB-90-76, Condition 3, that requires "dense and adequate landscaping" to be installed at the rear to serve as a buffer to the property to the south. The existing 48-inch VUA buffer hedge does not appear to be adequate, particularly given the proposed increase in student enrollment. The existing VUA buffer hedge is 48 inches in height several years after planting. Please provide a "buffer between dissimilar uses" pursuant Division 6.1 of the Village's Land Development Regulations as follows:

Buffers between dissimilar land uses. Where dissimilar land uses exist on adjacent properties, and where such areas will not be entirely visually screened by an intervening building or structure from abutting property, that portion of such area not so screened shall be provided with a buffer consisting of a six feet wall or fence with a life expectancy of at least ten years, together with shrubs which normally grow to a minimum height of six feet or more and shall be maintained at that height. Where chain link fencing is used, shrubs shall also be required to cover such fence. Shrubs used as a buffer shall be planted at a minimum of 24 inches on center and 30 inches in height at time of planting. The buffer shall form a continuous unbroken and solid visual screen between the dissimilar land uses within one year after planting. Buffers screening dissimilar uses shall include mahogany or oak trees, or similar hardwood canopy trees acceptable to the administrative official, planted at a maximum average spacing of 25 feet on center and shall have a height of at least 16 feet within the required setback. Shrubs required as a buffer shall not be trimmed shorter than 72 inches except where required in order to maintain a safe site distance.

Note: much of the clouded landscaping along the southern boundary is located on the adjoining property. Required landscaping is required to be planted on the subject property.

***RESPONSE TO COMMENT: The applicant will comply.***

***Please note that the south property line requires (29) trees, with an average spacing of 25' o.c. The revised landscaping plans exceed this requirement by proposing (44) trees along this south boundary.***

***In addition, the tree species were selected per the FPL Right Tree Right Place guidelines due to the overhead wires along the boundary. The existing hedge is proposed to be replaced with a new hedge of Clusia (with a couple clusters of Red Stopper in double-row) which are all the minimum 30" height at time of installation and shall easily reach 6'-0" in height.***

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*End of Comment Responses*

Dear Chief Director Olmstead, we would like to thank you, and the various Village Departments for the kind assistance throughout this process. We trust that the revised drawings, the revised traffic study, and the responses to comments submitted will demonstrate compliance with the Village requirements.

We are happy to report that under this revision, we have added 15 more trees than the minimum number required along the perimeter buffers; in addition, entire new hedges of larger species have been specified along the property line buffers, while all of the landscaping material specified consist of 100% natives species.

We have revised the ROW improvements indicating denser and wider green areas, with organic geometrical design of sidewalks. Based on these various design changes we kindly and respectfully look forward to the support of this application.

***Respectfully Submitted***

***Ivan A. Fajardo***  
***CIVICA, LLC.***



**CIVICA**

8323 NW 12 STREET, SUITE 106, DORAL, FL 33126

TEL: 305.593.9959

9 January 2025

Mr. Stephen R. Olmsted, AICP, LEED-GA,  
Planning Director  
Building and Planning Department  
Village of Pinecrest

**Re: Traffic Impact Study – Response to Comments**  
**Project Name: True North Academy Pinecrest at Bet Shira**  
**Langan Project No.: 330118701**

Dear Mr. Olmsted:

Please accept this letter as the response to the comments received on November 6, 2024 for the above-mentioned project.

## 1. Traffic Operations Plan:

*Comment a. Entry and Exit Driveway Enforcement:*

The report indicates that vehicles from the east and south will enter via the Palmetto Road driveway and exit on SW 120 Street, while vehicles from the north and west will enter from SW 120 Street and exit on Palmetto Road. Please clarify how this entry/exit pattern will be enforced during drop-off and pick-up times.

**Response a: Per discussions with the Village of Pinecrest, we are enforcing right-turn only driveways along SW 120<sup>th</sup> Street. All ingress traffic will enter the site through the eastern driveway and all traffic will egress the site through the western driveway along SW 120<sup>th</sup> Street. The right turn only will be enforced through the installation of a raised pork-chop median at the driveways.**

*Comment b. Alternative Exiting Routes:*

Vehicles exiting through Palmetto Road will either proceed north or merge into the northbound left-turn lane at the Palmetto Road/SW 120 Street intersection, while those exiting on SW 120 Street will enter the eastbound queue. To reduce congestion, please explore alternative exit routes to prevent vehicles from joining these queues at the intersection.

**Response b: Please see revised traffic study which has removed the proposed driveway along Palmetto Road. As such, all traffic will utilize SW 120<sup>th</sup> Street to enter and exit the site.**

*Comment c. School Shift Adjustment*

To avoid traffic overlap with Palmetto High School, where significant queuing was observed on October 23 and 28, 2024, we recommend adjusting the school's start and end times by 30 minutes.

**Response c: We confirmed that the high school is changing their school start and end times. As such, we did not change the school shifts and start and end times for True North.**

## 2. Growth Rate and Future Intersection Level of Service:

Comment 2: The report states that the 2025 No-Build traffic volumes were projected using a compounded growth rate of 1.74% based on 2022 data. However, data from 2022 to 2023 shows higher growth at key intersections near the subject school. For example, northbound and southbound through volumes at the Palmetto Road/SW 120 Street intersection increased by 54% (828 to 1279 vehicles) and 71% (296 to 507 vehicles), respectively, and the eastbound through volume at SW 120 Street/SW 74 Avenue rose by 40% (420 to 588 vehicles). Please review and adjust the growth rate to reflect these trends, as future traffic volumes may be underestimated.

**Response 2: We recollected ATR data at five key roadway segments in the surrounding area and compared the data to previously collected ATR data from 2023 along the same road segments. The findings yielded a negative growth trend. To assess the growth rate in a different way, we also recollected traffic data at the intersection of SW 120<sup>th</sup> Street and Palmetto Road and compared the traffic volumes in 2024 to the previously collected traffic volumes in 2023 and in 2022 at the same intersection. Based on the findings, during the morning peak hour, the growth rate showed a minimal growth increase, and during the afternoon peak hour, the growth rate should a 3.03% increase in traffic. As such, we used the afternoon growth rate of 3.03% to represent the growth rate for the analysis. This is an increase from the 1.74% growth rate we had calculated in our earlier iteration of the traffic study.**

## 3. 2025 Morning Peak Hour Roadway Capacity Analysis:

Comment 3: The future traffic conditions may indeed be underestimated. Currently, the northbound link on Palmetto Road approaching SW 120 Street is projected to operate at a level of service "F," even with the addition of a northbound right-turn bay. Please identify an additional mitigation strategy to achieve the adopted level of service "D."

**Response 3: We recollected the traffic data for the peak hour roadway capacity analysis in December 2024 when the existing school was operating with 280 students in class on the day of the data collection. Based on the existing traffic volume in 2024, all roadways analyzed are operating at LOS D or better during the peak hour periods. We then grew the traffic to the build year and applied the project generated traffic for the additional 320 students that will be enrolled at full occupancy. The future build conditions demonstrated that the analyzed roadways are all expected to operate at LOS D or better during both peak periods. Please see revised traffic study which outlines the roadway capacity analysis.**

## 4. Introduction:

Comment 4: Please add the following statement to the report's introduction, as noted in the executive summary: "The proposed school is requesting an increase of 275 students, raising the maximum enrollment to 600 students, and will offer classes from kindergarten through eighth grade."

**Response 4: Please see revised traffic study which includes the above statements.**

Sincerely,

**Langan Engineering and Environmental Services, LLC**

A handwritten signature in blue ink, consisting of a stylized 'M' and 'P' with a long horizontal stroke extending to the right.

Maximo G. Polanco, P.E.  
Senior Project Manager

MGP:mgp

FL Certificate of Authorization No. 6601

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**Date:** January 24, 2025\*

**\*(Revised on 1/31/2025 per meeting held on 1/30/2025 and responses received on 1/31/2025; The people in attendance included Mr. Stephen Olmsted (Village of Pinecrest), Mr. Carlos Francis (Choice), Mr. Maximo Polanco (Langan), Mr. Joe Goldberg (Langan), and Mr. Eugenio Lopez (Choice)).**

**To:** Mr. Stephen R. Olmsted, AICP, LEED-GA, Planning Director  
Building and Planning Department  
Village of Pinecrest

**From:** Eugenio S. Lopez, MSCE, P.E., PTOE; Jessica Garcia, MSCE, P.E., RSP

**Subject:** True North Academy Pinecrest – Traffic Study Comments

**FPID No.:** 415239-4-32-01      **Choice Project No.:** 2303      **Task No.:** 17

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The Village of Pinecrest assigned Choice Engineering Consultants, Inc. to review the traffic study completed by Langan Engineering in August 2024. Below are comments one through four, with Langan’s responses provided after each comment.

For reference, **Attachment A** includes Langan’s responses from 1/9/2025 and 1/31/2025. The intent of this document is to evaluate their responses and provide feedback while also reviewing the latest revised traffic study submitted by Langan in December 2024.

## **1. Traffic Operations Plan:**

### **a. Entry and Exit Driveway Enforcement:**

- The report indicates that vehicles from the east and south will enter via the Palmetto Road driveway and exit on SW 120 Street, while vehicles from the north and west will enter from SW 120 Street and exit on Palmetto Road. Please clarify how this entry/exit pattern will be enforced during drop-off and pick-up times.
- **Langan’s Response 1a (1/9/2025):** Per discussions with the Village of Pinecrest, we are enforcing right-turn only driveways along SW 120 Street. All ingress traffic will enter the site through the eastern driveway and all traffic will egress the site through the western driveway along SW 120 Street. The right turn only will be enforced through the installation of a raised pork-chop median at the driveways.
- Response accepted and comment closed.
- **Additional Comment 1aa:** Although the raised pork-chop islands will restrict northbound and westbound left-turn movements at the western and eastern driveways fronting SW 120 Street, instruct school personnel 4 and 7, as indicated on the traffic operations plan, to actively prevent

vehicles from attempting these restricted movements when entering or exiting the school property at both driveways. Please add information to traffic operations plan.

- **Langan’s Response 1aa (1/31/2025): Agreed. Please refer to the updated TOP forms and figures for both scenarios in Appendix D. The duties for SP4 and SP7 were updated to show their responsibilities during drop-off/pick-up operations.**
- Response accepted and comment closed.

**b. Alternative Exiting Routes:**

- Vehicles exiting through Palmetto Road will either proceed north or merge into the northbound left-turn lane at the Palmetto Road/SW 120 Street intersection, while those exiting on SW 120 Street will enter the eastbound queue. To reduce congestion, please explore alternative exit routes to prevent vehicles from joining these queues at the intersection.
- **Langan’s Response 1b (1/9/2025): Please see revised traffic study which has removed the proposed driveway along Palmetto Road. As such, all traffic will utilize SW 120 Street to enter and exit the site.**
- Response accepted and comment closed.

**c. School Shift Adjustment:**

- To avoid traffic overlap with Palmetto High School, where significant queuing was observed on October 23 and 28, 2024, we recommend adjusting the school’s start and end times by 30 minutes.
- **Langan’s Response 1c (1/9/2025): We confirmed that the high school is changing their school start and end times. As such, we did not change the school shifts and start and end times for True North.**
- Response not accepted. Please refer to the follow-up comment below.
- **Follow-Up Comment 1cc:** Based on drone footage captured during the morning and afternoon school periods for both Palmetto High School and True North, it was determined that vehicular queue build-ups along SW 120 Street and SW 77 Avenue due to Palmetto High School dissipated by 7:30 AM during arrival and by 3:00 PM during dismissal. **Attachment B** includes the drone footage and field notes for reference.

To ensure that the traffic conditions from True North do not further worsen congestion along SW 120 Street and SW 77 Avenue—already impacted by Palmetto High School—it is recommended to implement the following shift changes:

**Scenario 1 (Before House Bill 733 Takes Effect):**

- K thru 1<sup>st</sup> Grade: shift from 7:30 AM-2:30 PM to **7:45 AM-2:45 PM**
- 2<sup>nd</sup> Grade thru 5<sup>th</sup> Grade: shift from 8:00 AM-3:00 PM to **8:15 AM-3:15 PM**
- 6<sup>th</sup> thru 8<sup>th</sup> Grade: shift from 8:30 AM-3:30 PM to **8:45 AM-3:45 PM**

**Scenario 2 (If House Bill 733 Takes Effect on July 1, 2026, and Palmetto High Adjusts Dismissal Time Accordingly):**

- K thru 1<sup>st</sup> Grade: shift from 7:30 AM-2:30 PM to **7:15 AM-2:15 PM**
- 2<sup>nd</sup> Grade thru 5<sup>th</sup> Grade: shift from 8:00 AM-3:00 PM to **7:45 AM-2:45 PM**
- 6<sup>th</sup> thru 8<sup>th</sup> Grade: shift from 8:30 AM-3:30 PM to **8:15 AM-3:15 PM**

House Bill 733, enacted in 2023 and set to take effect on July 1, 2026, mandates that all public and charter middle schools in Florida must start no earlier than 8:00 AM, and high schools no earlier than 8:30 AM. Consequently, Palmetto High School would be required to shift its start time to 8:30 AM.

However, as of January 2025, there is ongoing debate among Florida lawmakers regarding this mandate. Some legislative efforts are aiming to repeal or modify the bill due to concerns about transportation, scheduling, and logistical challenges.

Given this uncertainty, it is crucial to document both shift scenarios to account for whether the bill is implemented or not.

**Additionally, if House Bill 733 is enacted and Palmetto High School does not adjust its dismissal period, the proposed end times for True North must be reviewed and approved by the Village. Some schools may shorten passing periods, reduce lunch breaks, or introduce flexible scheduling to maintain the same school day length without delaying dismissal. Please ensure that both scenarios and the final paragraph (in bold) are incorporated into the body of the traffic study, the traffic operations plan, and the Executive Summary.**

- **Langan’s Response 1cc (1/31/2025): Agreed. Please refer to the updated Report (Executive Summary), TOP forms and figures for both scenarios in Appendix D. We created two scenarios showing the shift’s start and end times before and after implementing the House Bill 733. Per our discussions, the only proposed change for Scenario 2 (compared to the proposal from the Village) is that the shift for K thru 1st Grades will be from 7:15 AM – 2:30 PM.**
- Response accepted and comment closed. Based on a virtual meeting held on 1/30/2025, it was discussed and agreed to shift the recommended end time for Scenario 2 from 2:15 PM to 2:30 PM. This was a request made by Mr. Maximo Polanco.

**2. Growth Rate and Future Intersection Level of Service:**

The report states that the 2025 No-Build traffic volumes were projected using a compounded growth rate of 1.74% based on 2022 data. However, data from 2022 to 2023 shows higher growth at key intersections near the subject school. For example, northbound and southbound through volumes at the Palmetto Road/SW 120 Street intersection increased by 54% (828 to 1279 vehicles) and 71% (296 to 507 vehicles), respectively, and the eastbound through volume at SW 120 Street/SW 74 Avenue rose by 40% (420 to 588 vehicles). Please review and adjust the growth rate to reflect these trends, as future traffic volumes may be underestimated.

**Langan’s Response 2 (1/9/2025):** We recollected ATR data at five key roadway segments in the surrounding area and compared the data to previously collected ATR data from 2023 along the same road segments. The findings yielded a negative growth trend. To assess the growth rate in a different way, we also recollected traffic data at the intersection of SW 120 Street and Palmetto Road and compared the traffic volumes in 2024 to the previously collected traffic volumes in 2023 and in 2022 at the same intersection. Based on the findings, during the morning peak hour, the growth rate showed a minimal growth increase, and during the afternoon peak hour, the growth rate should a 3.03% increase in traffic. As such, we used the afternoon growth rate of 3.03% to represent the growth rate for the analysis. This is an increase from the 1.74% growth rate we had calculated in our earlier iteration of the traffic study.

- Response accepted and comment closed.

**3. 2025 Morning Peak Hour Roadway Capacity Analysis:**

The future traffic conditions may indeed be underestimated. Currently, the northbound link on Palmetto Road approaching SW 120 Street is projected to operate at a level of service “F,” even with the addition of a northbound right-turn bay. Please identify an additional mitigation strategy to achieve the adopted level of service “D.”

**Langan’s Response 3 (1/9/2025):** We recollected the traffic data for the peak hour roadway capacity analysis in December 2024 when the existing school was operating with 280 students in class on the day of the data collection. Based on the existing traffic volume in 2024, all roadways analyzed are operating at LOS D or better during the peak hour periods. We then grew the traffic to the build year and applied the project generated traffic for the additional 320 students that will be enrolled at full occupancy. The future build conditions demonstrated that the analyzed roadways are all expected to operate at LOS D or better during both peak periods. Please see revised traffic study which outlines the roadway capacity analysis.

- Response accepted. However, please refer to the follow-up comment below.
- **Follow-Up Comment 3a:** Please confirm that the volumes shown in **Figures 4 (2025 No Build), 6 (Project Traffic), and 7 (2025 Build Traffic Volumes)** align with the volumes presented in **Table 7** of the report. Unless we are misinterpreting the data, there should not be any project trips in the westbound direction on SW 120 Street, as the northbound left-turn movement is not permitted for the driveways serving the school. However, **Table 7** currently shows project trips in the westbound direction. Additionally, **Figure 6** indicates 255 project trips traveling eastbound from the SW 77 Avenue signal, but **Table 7** only accounts for 103 project trips in the eastbound direction. Please verify all figures and update **Table 7 and Figures 4, 6, and 7** as necessary. Please submit them for confirmation before proceeding with the revision and resubmission of the traffic report. Furthermore, please confirm the values in **Table 8**, which also lists trips for the westbound direction along SW 120 Street. Lastly, please revise “AM” to “PM” for the peak hour (fourth column) in **Table 8** to match title of table.
- **Langan’s Response 3 (1/31/2025):** Per our discussion, the values for **Tables 7 and 8** do not align because the analysis was based on the net-new project trips (accounting for the existing enrollment). In addition, we revised the analysis to reflect the proper distribution with the

proposed site access. In addition, we performed a worst-case scenario roadway capacity analysis based on the 600-student enrollment project trips and demonstrate that the roadways are expected to operate within their adopted LOS. Please refer to Tables 9 and 10 of the revised report.

- Response accepted and comment closed.

#### 4. Introduction:

Please add the following statement to the report’s introduction, as noted in the executive summary: “The proposed school is requesting an increase of 275 students, raising the maximum enrollment to 600 students, and will offer classes from kindergarten through eighth grade.”

**Langan’s Response (1/9/2025): Please see revised traffic study which includes the above statements.**

- Response accepted and comment closed.

#### The following are new comments:

5. **Sidewalk Connectivity:** Please provide sidewalk connectivity from SW 120 Street to the school, or a designated pedestrian entrance, to avoid students walking through the driveways at the same time as the vehicles. This concern has been discussed previously and remains a priority for pedestrian safety.

**Langan’s Response (1/31/2025): Agreed. Please refer to the revised traffic study report.**

- Response accepted and comment closed.

#### 6. Traffic Circulation:

- a. Please add cones to the south side of the traffic circle inside the school to prevent vehicles in the circulation from going left around the traffic circle and travel northbound.

- **Langan’s Response (1/31/2025): Agreed. Please refer to the updated TOP form and figure for both Scenarios.**

- Response accepted and comment closed.

- b. Please reconcile the statement listed on page ii of the Executive Summary and anywhere else in the traffic report— “Reconstruct a portion of the site’s parking lot to provide a mini-roundabout to allow vehicles to turnaround within the site to facilitate the school’s pick-up/drop-off operations”—with the traffic operations plan, which no longer reflects this circulation patten.

- **Langan’s Response (1/31/2025): Agreed. Please refer to the updated report. Roundabout statement was reconciled.**

- Response accepted and comment closed.

7. **Synchro Output Reports:** Please perform a Synchro analysis for Build Conditions 2025 with geometric improvements at SW 120 Street and SW 77 Avenue signal. The output report in the appendix of the traffic report shows the northbound lane geometry as one shared through/right-turn lane and the 95<sup>th</sup> queue length being very long at #1,247 feet. The exclusive right-turn lane that is being proposed should be evaluated in Synchro to show the potential benefits to reduce and accommodate the queue.

**Langan's Response (1/31/2025):** Please refer to the updated Synchro reports for the build conditions in Appendix G. The exclusive northbound right-turn lane at the intersection of Palmetto Road and SW 120 Street was analyzed under optimization and mitigation conditions.

- Response accepted and comment closed.

## 8. Proposed Improvements:

Please address the following revisions:

### (Sheet A-1.0)

- a. Show the extension and/or length of the northbound right-turn and southbound left-turn lanes at SW 120 Street and SW 77 Avenue, ensuring they accommodate the queues under project conditions. It is currently unclear where the lanes end.

- **Langan's Response (1/31/2025):** Please note that the improvements shown on sheet A -1.0 are conceptual plans. Nevertheless, the lanes will be designed with the following lengths to accommodate the expected queues. The proposed northbound right-turn lane should be designed with a total length of 205-feet (105' deceleration lane + 50' taper + 50' queue). The southbound left-turn lane should be extended approximately 125-feet to provide a total length of 280-feet (105' deceleration lane + 50' taper + 125' queue).

- Response accepted and comment closed.

- b. Reconcile the sidewalk width of 8 feet- 2 inches with the 6 feet-2 inches dimension shown in the typical section for SW 77 Avenue.

- **Langan's Response (1/31/2025):** Per our discussion, all proposed sidewalks will be designed with a minimum width of 6-feet and in compliance with the Village of Pinecrest Standards. Design plans for all proposed roadway improvements will be submitted to the Village for review and approval.

- Response accepted and comment closed.

- c. Modify the turning radius for the right-in and right-out movements to maximize the channelization island size and prevent northbound and westbound left-turn movements at both school driveways fronting SW 120 Street. Ensure the island size complies with FDOT's minimum area requirement.

- **Langan's Response (1/31/2025):** Per our discussion, design plans for all proposed roadway improvements will be submitted to the Village for review and approval. We will ensure that

proper turning radius will be provided at the driveways to ensure that the proposed channelized islands comply with FDOT's minimum requirements. In addition, please note the plans will be designed in compliance with the Village of Pinecrest Standards.

- Response accepted and comment closed.

d. Label the sidewalk bordering True North to be a minimum of 6 feet wide.

- **Langan's Response (1/31/2025): Acknowledged. All proposed sidewalks along SW 77 Avenue and SW 120 Street shall be minimum 6-foot wide.**

- Response accepted and comment closed.

e. To prevent the westbound through lane from becoming a left-turn trap lane at the SW 77 Avenue signal, add a note specifying that the left-turn lane should be tapered instead of the through lane, as currently exists. This modification should be addressed during the design phase.

- **Langan's Response (1/31/2025): Agreed. Per our discussion, proper taper will be provided for the westbound through movement to avoid creating a trap lane.**

- Response accepted and comment closed.

(Executive Summary (page ii) and Conclusion (page 21))

f. Add that a minimum sidewalk width of 6-feet is provided around the entire True North school property.

- **Langan's Response (1/31/2025): Agreed. Please refer to the revised study.**

- Response accepted and comment closed.

g. Add that a raised pork-shop island at both True North driveways bordering SW 120 Street, in compliance with FDOT requirements must be provided, to restrict the northbound and westbound left-turn movements.

- **Langan's Response (1/31/2025): Acknowledged. Please refer to the revised study.**

- Response accepted and comment closed.

## 9. School Speed Zone Figure 10:

Please replace the school zone period times with "XX:XX" and add a note indicating that the periods must align with the times specified under Comment 1cc, both before and after House Bill 733 takes effect. Additionally, ensure that the times displayed on the school zone signs are coordinated with Palmetto High School, as they share the study roadways with True North.

**Langan's Response (1/31/2025): Agreed. Please refer to the updated Figure 10 in Appendix A.**

- Response accepted and comment closed.

**10. Roadway Capacity and Intersection Analyses:**

Please add text to the traffic report in the Executive Summary and Conclusion sections stating that the increase in student enrollment must be implemented gradually, as previously agreed with the Village. This increase should be supplemented with roadway capacity and intersection analyses to demonstrate that the additional vehicular trips generated by the added students can be accommodated by the existing roadways and proposed improvements, subject to the Village's review and approval.

Additionally, emphasize that these analyses will become even more critical due to the anticipated changes in school schedules for both Palmetto High School and True North following the implementation of House Bill 733.

**Langan's Response (1/31/2025): Agreed. Please refer to the updated report.**

- [Response accepted and comment closed.](#)

Please response and submit supporting documentation for the above comments before updating and resubmitting the traffic report. Feel free to reach out if you need any clarification.

Best Regards,

Eugenio S. Lopez, MSCE, P.E., PTOE  
Choice Senior Traffic Engineer

**Attachment A – Langan’s Response to Comments**

31 January 2025

Mr. Stephen R. Olmsted, AICP, LEED-GA,  
Planning Director  
Building and Planning Department  
Village of Pinecrest

**Re: Traffic Impact Study – Response to Comments**  
**Project Name: True North Academy Pinecrest at Bet Shira**  
**Langan Project No.: 330118701**

Dear Mr. Olmsted:

Please accept this letter as the response to the comments received on November 6, 2024 for the above-mentioned project.

1. Traffic Operations Plan:

*Comment a. Entry and Exit Driveway Enforcement:*

The report indicates that vehicles from the east and south will enter via the Palmetto Road driveway and exit on SW 120 Street, while vehicles from the north and west will enter from SW 120 Street and exit on Palmetto Road. Please clarify how this entry/exit pattern will be enforced during drop-off and pick-up times.

*Additional Comment 01/24/2025: Although the raised pork-chop islands will restrict northbound and westbound left-turn movements at the western and eastern driveways fronting SW 120 Street, instruct school personnel 4 and 7, as indicated on the traffic operations plan, to actively prevent vehicles from attempting these restricted movements when entering or exiting the school property at both driveways. Please add information to traffic operations plan.*

**Response a: Agreed. Please refer to the updated TOP forms and figures for both scenarios in Appendix D. The duties for SP4 and SP7 were updated to show their responsibilities during drop-off/pick-up operations.**

*Comment c. School Shift Adjustment*

To avoid traffic overlap with Palmetto High School, where significant queuing was observed on October 23 and 28, 2024, we recommend adjusting the school's start and end times by 30 minutes.

*Follow-Up Comment 01/24/2025: Based on drone footage captured during the morning and afternoon school periods for both Palmetto High School and True North, it was determined that vehicular queue build-ups along SW 120 Street and SW 77 Avenue due to Palmetto High School dissipated by 7:30 AM during arrival and by 3:00 PM during dismissal. Attachment B includes the drone footage and field notes for reference.*

*To ensure that the traffic conditions from True North do not further worsen congestion along SW 120 Street and SW 77 Avenue—already impacted by Palmetto High School—it is recommended to implement the following shift changes:*

*Scenario 1 (Before House Bill 733 Takes Effect):*

- *K thru 1<sup>st</sup> Grade: shift from 7:30 AM-2:30 PM to 7:45 AM-2:45 PM*
- *2<sup>nd</sup> Grade thru 5<sup>th</sup> Grade: shift from 8:00 AM-3:00 PM to 8:15 AM-3:15 PM*

- 6<sup>th</sup> thru 8<sup>th</sup> Grade: shift from 8:30 AM-3:30 PM to 8:45 AM-3:45 PM

*Scenario 2 (If House Bill 733 Takes Effect on July 1, 2026, and Palmetto High Adjusts Dismissal Time Accordingly):*

- K thru 1<sup>st</sup> Grade: shift from 7:30 AM-2:30 PM to 7:15 AM-2:30 PM
- 2<sup>nd</sup> Grade thru 5<sup>th</sup> Grade: shift from 8:00 AM-3:00 PM to 7:45 AM-2:45 PM
- 6<sup>th</sup> thru 8<sup>th</sup> Grade: shift from 8:30 AM-3:30 PM to 8:15 AM-3:15 PM

*House Bill 733, enacted in 2023 and set to take effect on July 1, 2026, mandates that all public and charter middle schools in Florida must start no earlier than 8:00 AM, and high schools no earlier than 8:30 AM. Consequently, Palmetto High School would be required to shift its start time to 8:30 AM. However, as of January 2025, there is ongoing debate among Florida lawmakers regarding this mandate. Some legislative efforts are aiming to repeal or modify the bill due to concerns about transportation, scheduling, and logistical challenges.*

*Given this uncertainty, it is crucial to document both shift scenarios to account for whether the bill is implemented or not.*

*Additionally, if House Bill 733 is enacted and Palmetto High School does not adjust its dismissal period, the proposed end times for True North must be reviewed and approved by the Village. Some schools may shorten passing periods, reduce lunch breaks, or introduce flexible scheduling to maintain the same school day length without delaying dismissal. Please ensure that both scenarios and the final paragraph (in bold) are incorporated into the body of the traffic study, the traffic operations plan, and the Executive Summary.*

**Response c: Agreed. Please refer to the updated Report (Executive Summary), TOP forms and figures for both scenarios in Appendix D. We created two scenarios showing the shift's start and end times before and after implementing the House Bill 733. Per our discussions, the only proposed change for Scenario 2 (compared to the proposal from the Village) is that the shift for K thru 1<sup>st</sup> Grades will be from 7:15AM – 2:30PM.**

### 3. 2025 Morning Peak Hour Roadway Capacity Analysis:

Comment 3: The future traffic conditions may indeed be underestimated. Currently, the northbound link on Palmetto Road approaching SW 120 Street is projected to operate at a level of service "F," even with the addition of a northbound right-turn bay. Please identify an additional mitigation strategy to achieve the adopted level of service "D."

*Follow-Up Comment 01/24/2025: Please confirm that the volumes shown in Figures 4 (2025 No Build), 6 (Project Traffic), and 7 (2025 Build Traffic Volumes) align with the volumes presented in Table 7 of the report. Unless we are misinterpreting the data, there should not be any project trips in the westbound direction on SW 120 Street, as the northbound left-turn movement is not permitted for the driveways serving the school. However, Table 7 currently shows project trips in the westbound direction. Additionally, Figure 6 indicates 255 project trips traveling eastbound from the SW 77 Avenue signal, but Table 7 only accounts for 103 project trips in the eastbound direction. Please verify all figures and update Table 7 and Figures 4, 6, and 7 as necessary. Please submit them for confirmation before proceeding with the revision and resubmission of the traffic report.*

Furthermore, please confirm the values in Table 8, which also lists trips for the westbound direction along SW 120 Street. Lastly, please revise “AM” to “PM” for the peak hour (fourth column) in Table 8 to match title of table.

**Response:** Per our discussion, the values for Tables 7 and 8 do not align because the analysis was based on the net-new project trips (accounting for the existing enrollment). In addition, we revised the analysis to reflect the proper distribution with the proposed site access. In addition, we performed a worst-case scenario roadway capacity analysis based on the 600-student enrollment project trips and demonstrate that the roadways are expected to operate within their adopted LOS. Please refer to Tables 9 and 10 of the revised report.

### **NEW COMMENTS**

5. Sidewalk Connectivity. Comment 5: Please provide sidewalk connectivity from SW 120 Street to the school, or a designated pedestrian entrance, to avoid students walking through the driveways at the same time as the vehicles. This concern has been discussed previously and remains a priority for pedestrian safety.

**Response: Agreed. Please refer to the revised traffic study report.**

6. Traffic Circulation.

Comment 6-a): Please add cones to the south side of the traffic circle inside the school to prevent vehicles in the circulation from going left around the traffic circle and travel northbound.

**Response a) Agreed. Please refer to the updated TOP form and figure for both Scenarios.**

b. Please reconcile the statement listed on page ii of the Executive Summary and anywhere else in the traffic report— “Reconstruct a portion of the site’s parking lot to provide a mini-roundabout to allow vehicles to turnaround within the site to facilitate the school’s pick-up/drop-off operations”—with the traffic operations plan, which no longer reflects this circulation pattern.

**Response b) Agreed. Please refer to the updated report. Roundabout statement was reconciled.**

7. Synchro Output Reports. Comment 7: Please perform a Synchro analysis for Build Conditions 2025 with geometric improvements at SW 120 Street and SW 77 Avenue signal. The output report in the appendix of the traffic report shows the northbound lane geometry as one shared through/right-turn lane and the 95th queue length being very long at #1,247 feet. The exclusive right-turn lane that is being proposed should be evaluated in Synchro to show the potential benefits to reduce and accommodate the queue.

**Response: Please refer to the updated Synchro reports for the build conditions in Appendix G. The exclusive northbound right-turn lane at the intersection of Palmetto Road and SW 120<sup>th</sup> Street was analyzed under optimization and mitigation conditions.**

8. Proposed Improvements:  
(Sheet A-1.0)

- a. Show the extension and/or length of the northbound right-turn and southbound left-turn lanes at SW 120 Street and SW 77 Avenue, ensuring they accommodate the queues under project conditions. It is currently unclear where the lanes end.

**Response a: Please note that the improvements shown on sheet A -1.0 are conceptual plans. Nevertheless, the lanes will be designed with the following lengths to accommodate the expected queues.**

- The proposed northbound right-turn lane should be designed with a total length of 205-feet (105' deceleration lane + 50' taper + 50' queue).
- The southbound left-turn lane should be extended approximately 125-feet to provide a total length of 280-feet (105' deceleration lane + 50' taper + 125' queue).

- b. Reconcile the sidewalk width of 8 feet- 2 inches with the 6 feet-2 inches dimension shown in the typical section for SW 77 Avenue.

**Response b: Per our discussion, all proposed sidewalks will be designed with a minimum width of 6-feet and in compliance with the Village of Pinecrest Standards. Design plans for all proposed roadway improvements will be submitted to the Village for review and approval.**

- c. Modify the turning radius for the right-in and right-out movements to maximize the channelization island size and prevent northbound and westbound left-turn movements at both school driveways fronting SW 120 Street. Ensure the island size complies with FDOT's minimum area requirement.

**Response c: Per our discussion, design plans for all proposed roadway improvements will be submitted to the Village for review and approval. We will ensure that proper turning radius will be provided at the driveways to ensure that the proposed channelized islands comply with FDOT's minimum requirements. In addition, please note the plans will be designed in compliance with the Village of Pinecrest Standards.**

- d. Label the sidewalk bordering True North to be a minimum of 6 feet wide.

**Response d: Acknowledged. All proposed sidewalks along SW 77<sup>th</sup> Avenue and SW 120<sup>th</sup> Street shall be minimum 6-foot wide.**

- e. To prevent the westbound through lane from becoming a left-turn trap lane at the SW 77 Avenue signal, add a note specifying that the left-turn lane should be tapered instead of the through lane, as currently exists. This modification should be addressed during the design phase.

**Response e: Agreed. Per our discussion, proper taper will be provided for the westbound through movement to avoid creating a trap lane.**

(Executive Summary (page ii) and Conclusion (page 21))

- f. Add that a minimum sidewalk width of 6-feet is provided around the entire True North school property.

**Response f: Agreed. Please refer to the revised study.**

- g. Add that a raised pork-shop island at both True North driveways bordering SW 120 Street, in compliance with FDOT requirements must be provided, to restrict the northbound and westbound left-turn movements.

**Response g: Acknowledged. Please refer to the revised study.**

9. School Speed Zone Figure 10: Comment 9: Please replace the school zone period times with "XX:XX" and add a note indicating that the periods must align with the times specified under Comment 1cc, both before and after House Bill 733 takes effect. Additionally, ensure that the times displayed on the school zone signs are coordinated with Palmetto High School, as they share the study roadways with True North.

**Response: Agreed. Please refer to the updated figure 10 in Appendix A**

10. Roadway Capacity and Intersection Analyses: Comment 10: Please add text to the traffic report in the Executive Summary and Conclusion sections stating that the increase in student enrollment must be implemented gradually, as previously agreed with the Village. This increase should be supplemented with roadway capacity and intersection analyses to demonstrate that the additional vehicular trips generated by the added students can be accommodated by the existing roadways and proposed improvements, subject to the Village's review and approval.

Additionally, emphasize that these analyses will become even more critical due to the anticipated changes in school schedules for both Palmetto High School and True North following the implementation of House Bill 733.

**Response: Agreed. Please refer to the updated report.**

Sincerely,

**Langan Engineering and Environmental Services, LLC**



Maximo G. Polanco, P.E.  
Senior Project Manager

MGP:mgp

FL Certificate of Authorization No. 6601

\\langan.com\data\FTL\Data\7330118701\Project Data\Correspondence\Transmittals\2025-01-31 Comment Response Letter – Pinecrest at Bet Shira - City.docx

# TRUE NORTH CLASSICAL ACADEMY : PINECREST CAMPUS

PROJECT ADDRESS:  
7500 SW 120TH STREET  
PINECREST, FL 33156  
FOLIO# : 20-5014-004-0050

APPLICANT:  
TRUE NORTH CLASSICAL ACADEMY  
9393 SW 72ND STREET  
MIAMI, FLORIDA 33173

DATE: Sept. 2022 - Feb. 2025  
SUBMITTAL: CRC  
CIVICA PROJECT #: 209906

## CIVICA

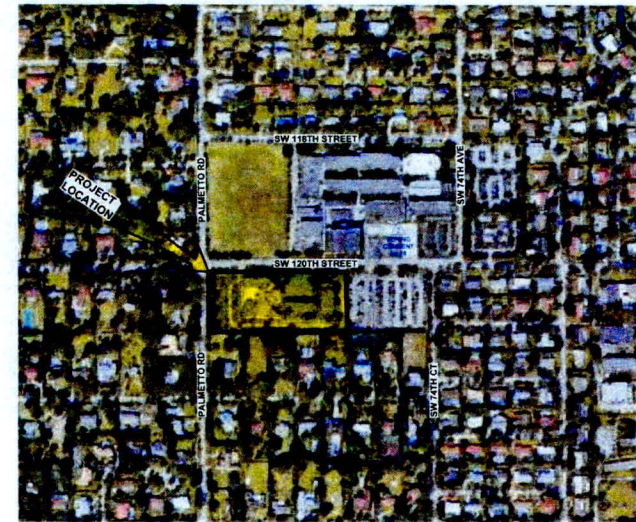
ARCHITECTURE

8323 NW 12TH ST  
SUITE 106  
DORAL, FL 33126  
PH. 305.593.9959  
FX. 305.593.9855

### DRAWING INDEX:

ARCHITECTURAL	COVER	
	SURVEY	
	A-1.0	PROPOSED SITE PLAN
	A-1.1	FIRE ENGINE ACCESS SITE PLAN
	A-2.0	EXISTING CLASSROOM BUILDING - FLOOR PLANS
	A-3.0	EXISTING CLASSROOM BUILDING - ELEVATIONS
	A-4.0	EXISTING SOCIAL HALL BLDG - FLOOR PLAN & ELEVATIONS
	A-5.0	CLASSROOMS LOCATION DIAGRAM
	A-6.0	PRE-SANCTUARY DEMOLITION PLAN
	A-6.1	EXISTING SANCTUARY BLDG. 1st FLOOR PLAN
	A-6.2	EXISTING SANCTUARY BLDG. 2nd FLOOR PLAN
	A-6.3	EXISTING SANCTUARY - SEWERAGE SYSTEM PLAN
	A-6.4	EXISTING SANCTUARY - PAVING, GRADING & DRAINAGE PLAN
	A-6.5	EXISTING SANCTUARY - WATER DISTRIBUTION PLAN
A-6.6	EXISTING SANCTUARY - FORCEMAIN CONNECTION PLAN	
A-6.7	EXISTING SITE PLAN - ILLUMINATION & PHOTOMETRIC SITE PLAN	
A-7.0	EXISTING SIGNAGE AND FENCE DETAILS	
A-7.1	OFF-SITE DIMENSIONED SITE PLAN	
A-7.2	R.O.W. IMPROVEMENTS - PROFILE DETAILS	
LANDSCAPING	LA1-01	EXISTING TREE DISPOSITION PLAN
	LA2-01	PROPOSED PLANTING PLAN
	LA2-02	PLANTING SPECS & DETAILS

### LOCATION MAP



08-18-23	PROPOSED COMMENTS	MS / JAF
02-22-24	PROPOSED COMMENTS	MS / JAF
11-08-24	PROPOSED COMMENTS	MS / JAF

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# TRUE NORTH CLASSICAL ACADEMY: PINECREST CAMPUS

PROJECT ADDRESS:  
7500 SW 120TH STREET  
PINECREST, FL 33156  
FOLIO#: 20-5014-004-0050

APPLICANT:  
TRUE NORTH CLASSICAL ACADEMY  
9393 SW 72ND STREET  
MIAMI, FLORIDA 33173

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ARCHITECTURE  
8323 NW 12TH ST  
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	LA2-01 PROPOSED PLANTING PLAN
	LA2-02 PLANTING SPECS & DETAILS

ARCHITECTURAL

LANDSCAPING

## LOCATION MAP



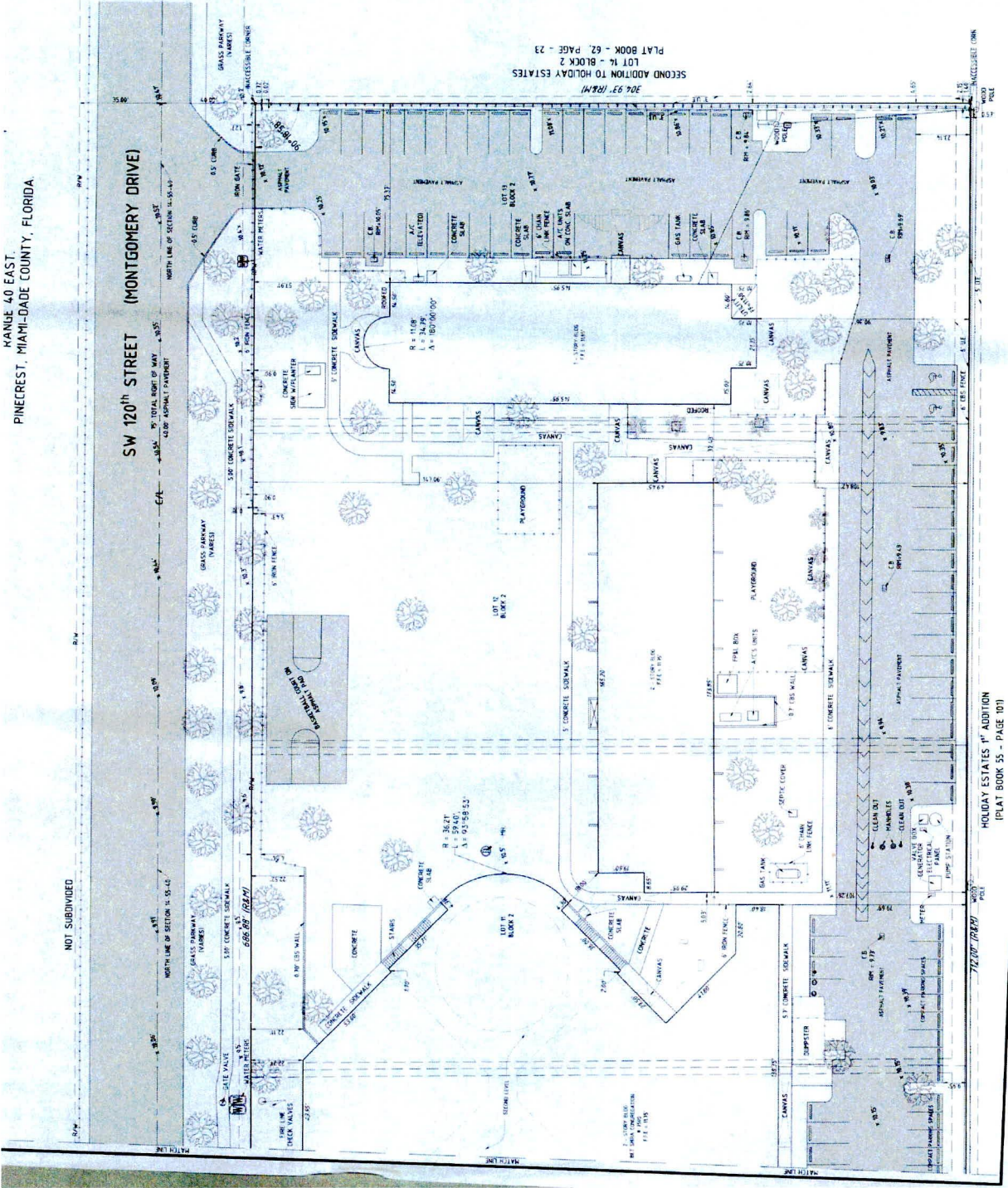
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2	02.22.24	PRELIMINARY COMMENTS	MS / JAF
3	11.08.24	PRELIMINARY COMMENTS	MS / JAF



RANGE 40 EAST  
PINECREST, MIAMI-DADE COUNTY, FLORIDA

NOT SUBDIVIDED

SW 120<sup>th</sup> STREET (MONTGOMERY DRIVE)



SECOND ADDITION TO HOLIDAY ESTATES  
LOT 14 - BLOCK 2  
PLAT BOOK - 62, PAGE - 23

HOLIDAY ESTATES 1<sup>st</sup> ADDITION  
(PLAT BOOK 55 - PAGE 101)

**SYMBOLS LEGEND**

- 1. CONCRETE SLAB
- 2. CONCRETE SLAB WITH FINISH
- 3. CONCRETE SLAB WITH FINISH AND CURB
- 4. CONCRETE SLAB WITH FINISH AND CURB AND SIDEWALK
- 5. CONCRETE SLAB WITH FINISH AND CURB AND SIDEWALK AND GRASS PARKWAY
- 6. CONCRETE SLAB WITH FINISH AND CURB AND SIDEWALK AND GRASS PARKWAY AND WATER PIPES
- 7. CONCRETE SLAB WITH FINISH AND CURB AND SIDEWALK AND GRASS PARKWAY AND WATER PIPES AND ELECTRICAL
- 8. CONCRETE SLAB WITH FINISH AND CURB AND SIDEWALK AND GRASS PARKWAY AND WATER PIPES AND ELECTRICAL AND GAS
- 9. CONCRETE SLAB WITH FINISH AND CURB AND SIDEWALK AND GRASS PARKWAY AND WATER PIPES AND ELECTRICAL AND GAS AND AIR CONDITIONING
- 10. CONCRETE SLAB WITH FINISH AND CURB AND SIDEWALK AND GRASS PARKWAY AND WATER PIPES AND ELECTRICAL AND GAS AND AIR CONDITIONING AND LANDSCAPE
- 11. CONCRETE SLAB WITH FINISH AND CURB AND SIDEWALK AND GRASS PARKWAY AND WATER PIPES AND ELECTRICAL AND GAS AND AIR CONDITIONING AND LANDSCAPE AND SIGNAGE
- 12. CONCRETE SLAB WITH FINISH AND CURB AND SIDEWALK AND GRASS PARKWAY AND WATER PIPES AND ELECTRICAL AND GAS AND AIR CONDITIONING AND LANDSCAPE AND SIGNAGE AND FENCE
- 13. CONCRETE SLAB WITH FINISH AND CURB AND SIDEWALK AND GRASS PARKWAY AND WATER PIPES AND ELECTRICAL AND GAS AND AIR CONDITIONING AND LANDSCAPE AND SIGNAGE AND FENCE AND LIGHTING
- 14. CONCRETE SLAB WITH FINISH AND CURB AND SIDEWALK AND GRASS PARKWAY AND WATER PIPES AND ELECTRICAL AND GAS AND AIR CONDITIONING AND LANDSCAPE AND SIGNAGE AND FENCE AND LIGHTING AND SECURITY
- 15. CONCRETE SLAB WITH FINISH AND CURB AND SIDEWALK AND GRASS PARKWAY AND WATER PIPES AND ELECTRICAL AND GAS AND AIR CONDITIONING AND LANDSCAPE AND SIGNAGE AND FENCE AND LIGHTING AND SECURITY AND UTILITY
- 16. CONCRETE SLAB WITH FINISH AND CURB AND SIDEWALK AND GRASS PARKWAY AND WATER PIPES AND ELECTRICAL AND GAS AND AIR CONDITIONING AND LANDSCAPE AND SIGNAGE AND FENCE AND LIGHTING AND SECURITY AND UTILITY AND PAVEMENT
- 17. CONCRETE SLAB WITH FINISH AND CURB AND SIDEWALK AND GRASS PARKWAY AND WATER PIPES AND ELECTRICAL AND GAS AND AIR CONDITIONING AND LANDSCAPE AND SIGNAGE AND FENCE AND LIGHTING AND SECURITY AND UTILITY AND PAVEMENT AND DRIVEWAY
- 18. CONCRETE SLAB WITH FINISH AND CURB AND SIDEWALK AND GRASS PARKWAY AND WATER PIPES AND ELECTRICAL AND GAS AND AIR CONDITIONING AND LANDSCAPE AND SIGNAGE AND FENCE AND LIGHTING AND SECURITY AND UTILITY AND PAVEMENT AND DRIVEWAY AND BIKEWAY
- 19. CONCRETE SLAB WITH FINISH AND CURB AND SIDEWALK AND GRASS PARKWAY AND WATER PIPES AND ELECTRICAL AND GAS AND AIR CONDITIONING AND LANDSCAPE AND SIGNAGE AND FENCE AND LIGHTING AND SECURITY AND UTILITY AND PAVEMENT AND DRIVEWAY AND BIKEWAY AND TRAIL
- 20. CONCRETE SLAB WITH FINISH AND CURB AND SIDEWALK AND GRASS PARKWAY AND WATER PIPES AND ELECTRICAL AND GAS AND AIR CONDITIONING AND LANDSCAPE AND SIGNAGE AND FENCE AND LIGHTING AND SECURITY AND UTILITY AND PAVEMENT AND DRIVEWAY AND BIKEWAY AND TRAIL AND PLAYGROUND



NOT VALID UNLESS IT BEARS THE  
REGISTERED PROFESSIONAL ENGINEER  
SEAL AND THE ORIGINAL  
SURVEYING AND MAPS  
DEPARTMENT FILE NUMBER



REGISTERED PROFESSIONAL ENGINEER  
STATE OF FLORIDA  
LICENSE NO. 12528  
EXPIRES 12/31/2024

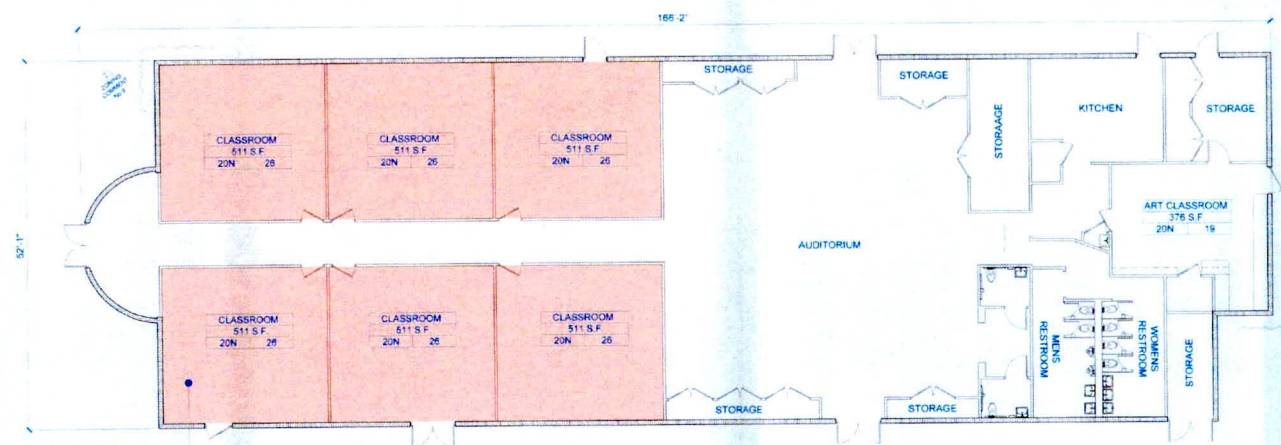




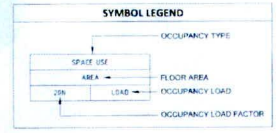




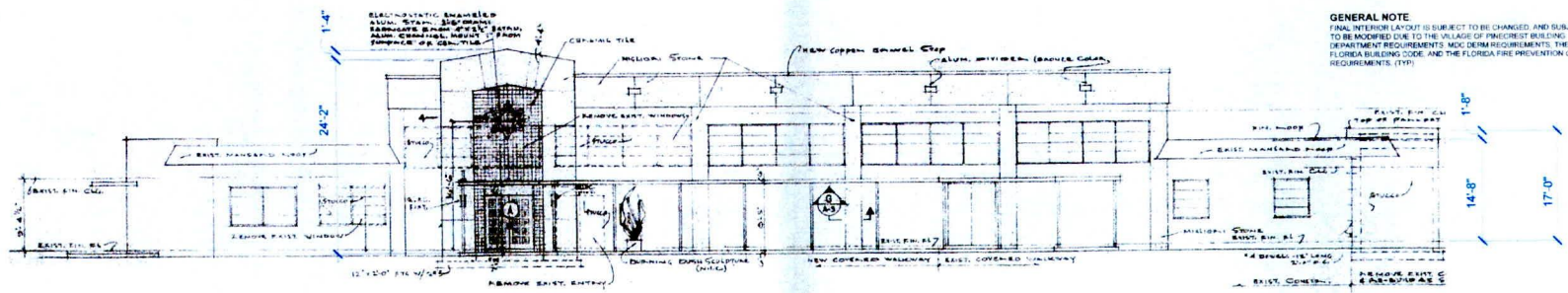
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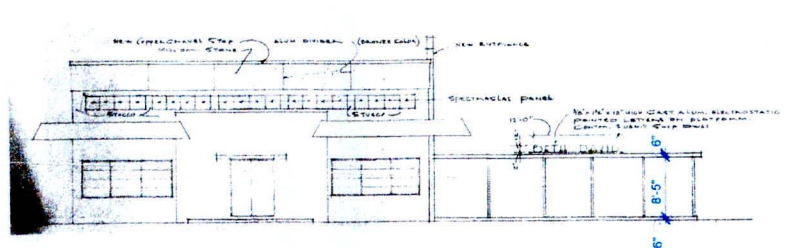
**CLASSROOM & SOCIAL HALL BLDG. FLOOR PLAN**  
SCALE: 1/8" = 1'-0"



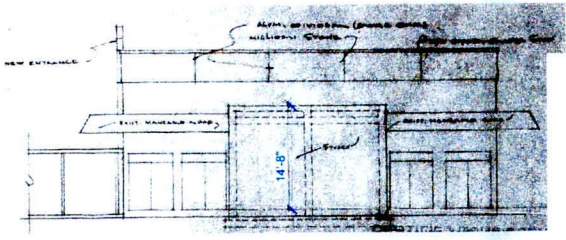
**GENERAL NOTE**  
FINAL INTERIOR LAYOUT IS SUBJECT TO BE CHANGED, AND SUBJECT TO BE MODIFIED DUE TO THE RELEASE OF PINECREST BUILDING DEPARTMENT REQUIREMENTS, MDC DESIGN REQUIREMENTS, THE FLORIDA BUILDING CODE, AND THE FLORIDA FIRE PREVENTION CODE REQUIREMENTS, (IFC).



**EXISTING WEST ELEVATION**  
SCALE: 1/8" = 1'-0"



**EXISTING NORTH ELEVATION**  
SCALE: 1/8" = 1'-0"



**EXISTING SOUTH ELEVATION**  
SCALE: 1/8" = 1'-0"

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AA #060393

**PROJECT**  
TRUE NORTH  
CLASSICAL ACADEMY  
PINECREST CAMPUS  
7500 SW 12TH STREET  
MIAMI, FLORIDA 33156  
YOLDF# 20-5014-004-0050  
**APPLICANT**  
TRUE NORTH CLASSICAL ACADEMY  
9043 SW 12ND STREET  
MIAMI, FLORIDA 33173



CIVICA PROJECT No.  
**209906**

ISSUED FOR:  
**CRC SUBMITTAL**

No.	DATE	REVISION	BY
1	08-15-23	CRC ZONING COMMENTS	MS / JAF
2	02-23-24	CRC ZONING COMMENTS	MS / JAF

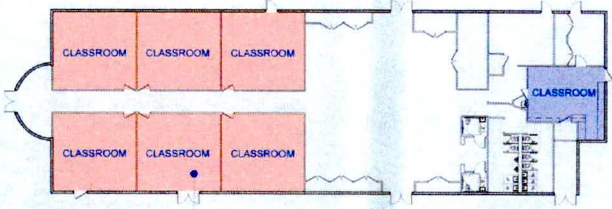
**DRAWN BY:** [Blank]  
**APPROVED BY:** [Blank]  
**DATE:** [Blank]  
**SCALE:** [Blank]  
**KEY PLAN:** [Blank]

**PERMIT SUBMITTAL**  
**SEAL SIGNATURE**  
  
**ROLANDO LEASES**  
AR: 2013190  
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**SHEET TITLE**  
EXISTING  
SOCIAL HALL BLDG  
FLOOR PLAN &  
ELEVATIONS

**SHEET NUMBER**  
A-4.0

P:\2020\209906\209906\_ARCH\17-CRC\209906\_A-01-17P-17.dwg, 1/15/2025 8:54:08 PM, DOWN TO PAPER



SHADED AREAS INDICATES FUTURE CLASSROOMS AT EXISTING OPEN SPACE

CLASSROOM & SOCIAL HALL BLDG. FLOOR PLAN

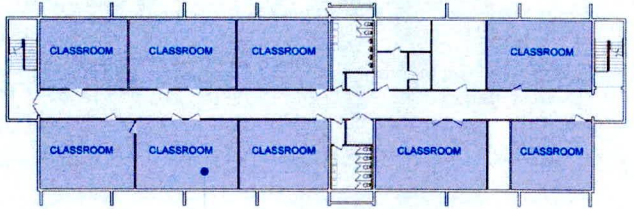
SCALE: 1/16" = 1'-0"

<b>MAXIMUM TOTAL ENROLLMENT PER SERVICE NOT TO EXCEED 600</b>	
PRE-SCHOOL / DAYCARE	100 MAX
K-8TH	575 MAX

<b>EXTRACURRICULAR ENROLLMENT</b>	
AFTER SCHOOL CARE	150
EXTRACURRICULAR ENROLLMENT DOES NOT IMPACT THE MAX ENROLLMENT OF THE FACILITY AS IT CONSISTS OF ALREADY ENROLLED STUDENTS STAYING PAST THE TIME OF DISMISSAL FOR COORDINATED CARE BY THE SCHOOL.	

<b>PROPOSED K-8th GRADES CLASSROOM BREAKDOWN</b>	
	<b>TOTAL CLASSROOMS</b>
SANCTUARY BLDG	NO K-8th CLASSROOMS
1-STORY CLASSROOM BLDG	7 CLASSROOMS (New Proposed)
2-STORY CLASSROOM BLDG	22 CLASSROOMS (Existing)
<b>GRAND TOTAL:</b>	<b>29 CLASSROOMS</b>

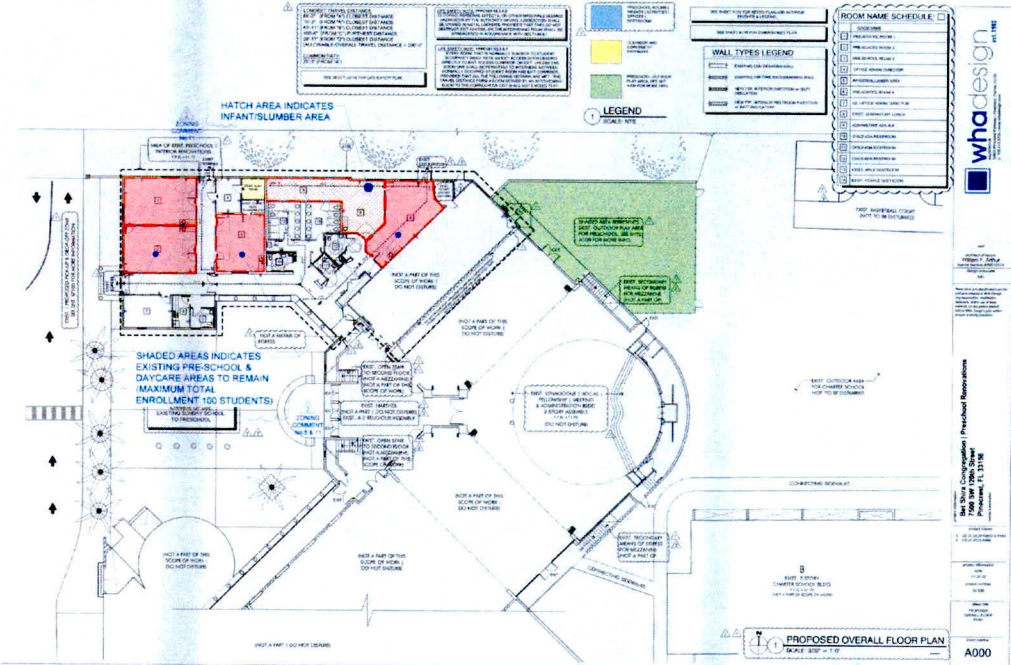
**CLASSROOM NOTE:**  
A TOTAL OF 29, K-8th GRADE CLASSROOMS SHALL BE PROPOSED WITH A CAPACITY TO ACCOMMODATE 725, STUDENTS. UNDER THIS APPLICATION A MAXIMUM OF 575 STUDENTS K-8th ARE BEING PROPOSED.



SHADED AREAS INDICATES (K-8th GRADES) CLASSROOMS AT EXISTING BUILDINGS

EXISTING CLASSROOM BLDG

SCALE: 1/16" = 1'-0"



**GENERAL NOTE**  
FINAL INTERIOR LAYOUT IS SUBJECT TO BE CHANGED AND SUBJECT TO BE MODIFIED DUE TO THE VILLAGE OF PINECREST BUILDING DEPARTMENT REQUIREMENTS, BOCA CODE REQUIREMENTS, THE FLORIDA BUILDING CODE, AND THE FLORIDA FIRE PREVENTION CODE REQUIREMENTS. (TYP)

RENOVATED SANCTUARY BLDG

SCALE: 1/8" = 1'-0"

**CIVICA**  
ARCHITECTURAL & PLANNING  
8323 NW 12th St, Suite 108  
Doral, FL 33126  
tel: 305.563.9958  
www.civica-group.com  
FL REG. NO. 1505

PROJECT:  
**TRUE NORTH CLASSICAL ACADEMY PINECREST CAMPUS**  
7500 SW 120TH STREET  
MIAMI, FLORIDA 33156  
FOLDER: 20-5014-004-006  
APPLICANT:  
**TRUE NORTH CLASSICAL ACADEMY**  
9363 SW 72ND STREET  
MIAMI, FLORIDA 33173



CIVICA PROJECT No:  
**209906**

ISSUED FOR

**CRC SUBMITTAL**

No.	DATE	REVISION	BY
01	08.15.23	CRC DRAWING COMMENTS	MS / JAF
02	02.22.24	CRC DRAWING COMMENTS	MS / JAF

DRAWN BY: [Signature]  
APPROVED BY: [Signature]  
DATE: 2023  
SCALE: AS SHOWN  
KEY PLAN



ROLANDO LIANES  
AS 02/13/25

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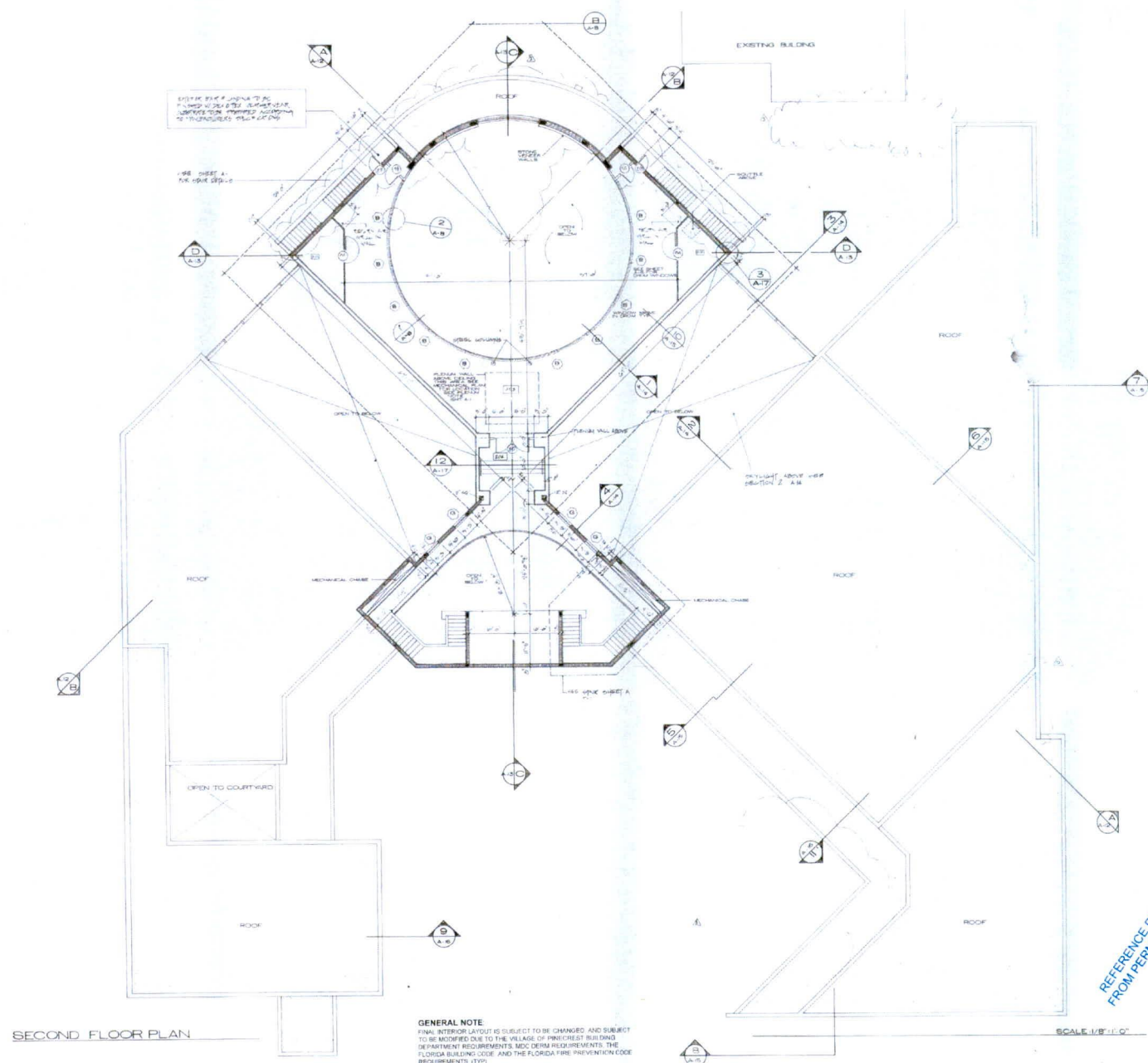
SHEET TITLE:  
**CLASSROOMS LOCATION DIAGRAM**

SHEET NUMBER:  
**A-5.0**





P:\2009\2099\209906-A2\A211.dwg 1/19/2009 8:55:42 PM DWG To PDF.plt



SECOND FLOOR PLAN

**GENERAL NOTE**  
 FINAL INTERIOR LAYOUT IS SUBJECT TO BE CHANGED AND SUBJECT TO BE MODIFIED DUE TO THE VILLAGE OF PINECREST BUILDING DEPARTMENT REQUIREMENTS, MCC DEMO REQUIREMENTS, THE FLORIDA BUILDING CODE, AND THE FLORIDA FIRE PREVENTION CODE REQUIREMENTS (199)

SCALE 1/8" = 1'-0"

REFERENCE DRAWING FROM PERMIT RECORDS

**CIVICA**  
 CONSULTING ENGINEERS

8323 NW 12th St. Suite 106  
 Doral, FL 33126  
 tel: 305 593 9959  
 www.civicaeng.com  
 AA-60001093

PROJECT  
 TRUE NORTH  
 CLASSICAL ACADEMY  
 PINECREST CAMPUS

1005 SW 120TH STREET  
 MIAMI, FLORIDA 33156  
 FOLLOW 25-5014-054-0950

APPLICANT:  
 TRUE NORTH  
 CLASSICAL ACADEMY  
 9193 SW 72ND STREET  
 MIAMI, FLORIDA 33173



CIVICA PROJECT No  
 209906

ISSUED FOR  
 CRC SUBMITTAL

No.	DATE	REVISION	BY
02	10.23	CRK ZONING COMMENTS	ME, IAF

DRAWN BY: APPROVED BY:  
 RL  
 DATE: SCALE:  
 2009 AS SHOWN  
 KEYPLAN

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 SEAL / SIGNATURE

ROLANDO LLANES  
 PE - 12022  
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SHEET TITLE  
 EXISTING  
 SANCTUARY BLDG  
 2nd FLOOR PLAN

SHEET NUMBER  
 A-6.2

Bet Shira Congregation

ZYSCOVICH & GRAFTON  
 ARCHITECTS  
 1101 N. MIAMI AVE., SUITE 1000  
 MIAMI, FLORIDA 33130  
 (305) 388-8222



A2

No.	DATE	REVISION	BY
1	08/13/20	ISSUED FOR PERMITS	MSS/JAP

**APPROVED BY:**  
 DATE: 08/13/20  
 SCALE: AS SHOWN  
 KEY PLAN:

**PERMIT IS INITIAL**  
 SEAL SIGNATURE

**REGISTERED PROFESSIONAL ENGINEER**  
 STATE OF FLORIDA  
 No. 12872  
 EXPIRES 08/13/2025

**REGISTERED PROFESSIONAL ENGINEER**  
 STATE OF FLORIDA  
 No. 12872  
 EXPIRES 08/13/2025

**REGISTERED PROFESSIONAL ENGINEER**  
 STATE OF FLORIDA  
 No. 12872  
 EXPIRES 08/13/2025

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**REGISTERED PROFESSIONAL ENGINEER**  
 STATE OF FLORIDA  
 No. 12872  
 EXPIRES 08/13/2025

**REGISTERED PROFESSIONAL ENGINEER**  
 STATE OF FLORIDA  
 No. 12872  
 EXPIRES 08/13/2025

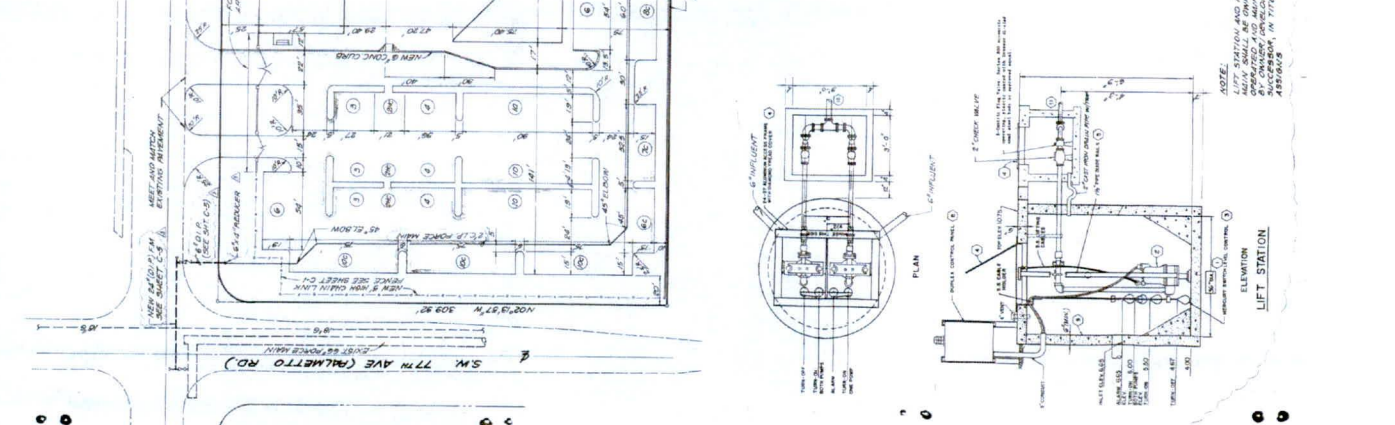
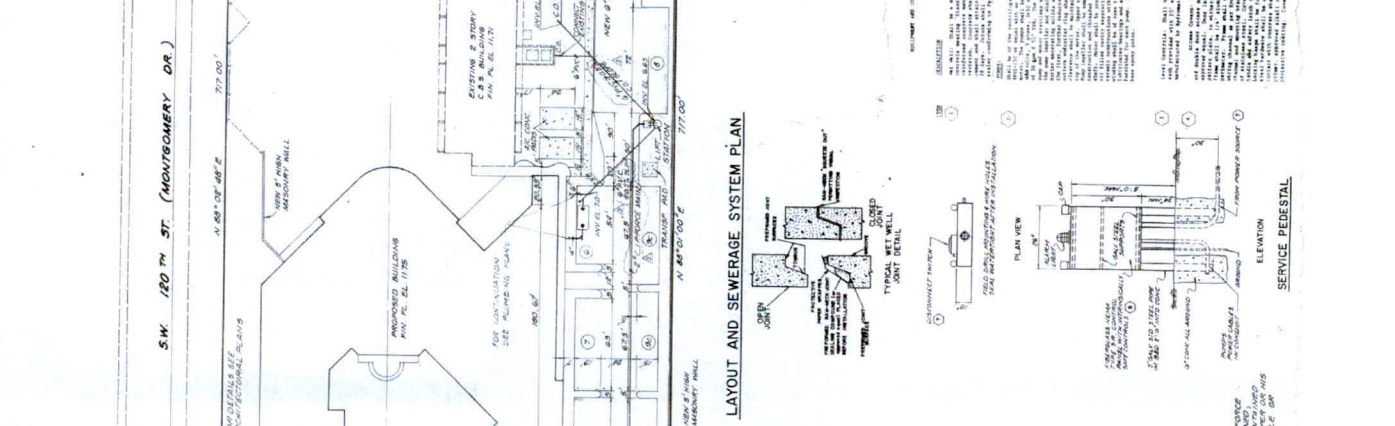
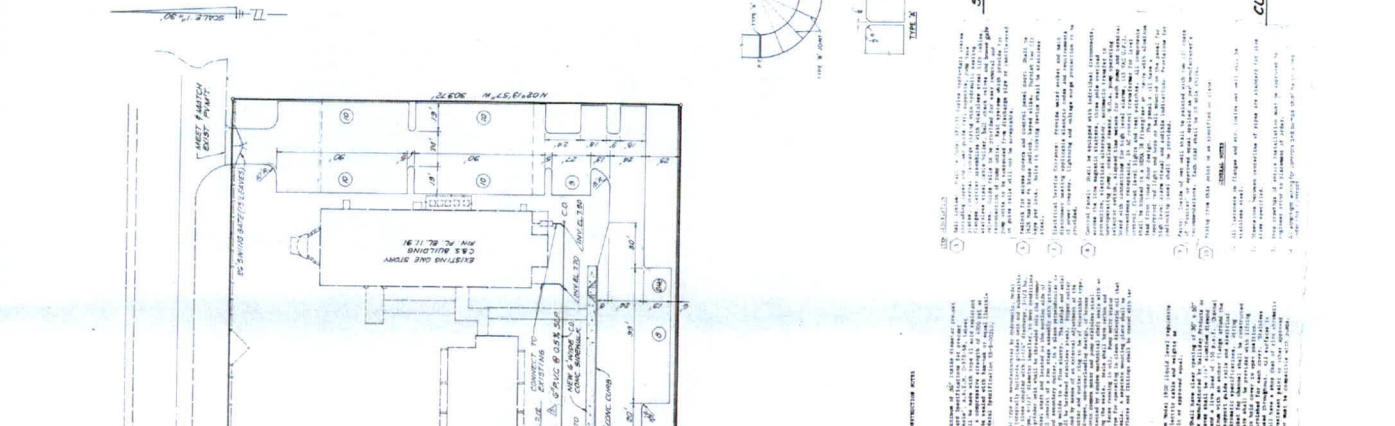
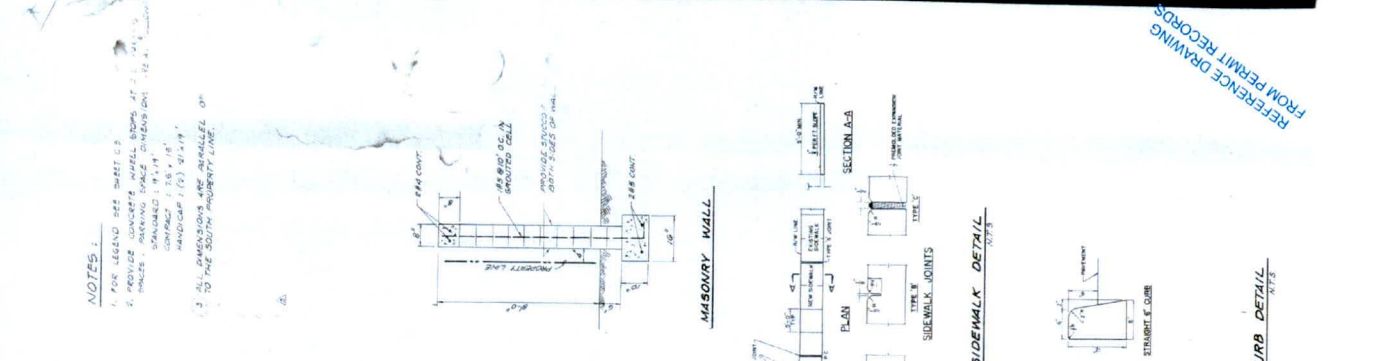
**ZYSOVICH & GRAFTON**  
 ARCHITECTS  
 INTERIOR DESIGNERS  
 69 S.W. STREET  
 MIAMI BEACH, FL 33139  
 (305) 558-2322

**Bet Shira Congregation**

**WOLFBERG AND ASSOCIATES**  
 CONSULTING ENGINEERS

**EXISTING SANCTUARY SEWERAGE SYSTEM PLAN**

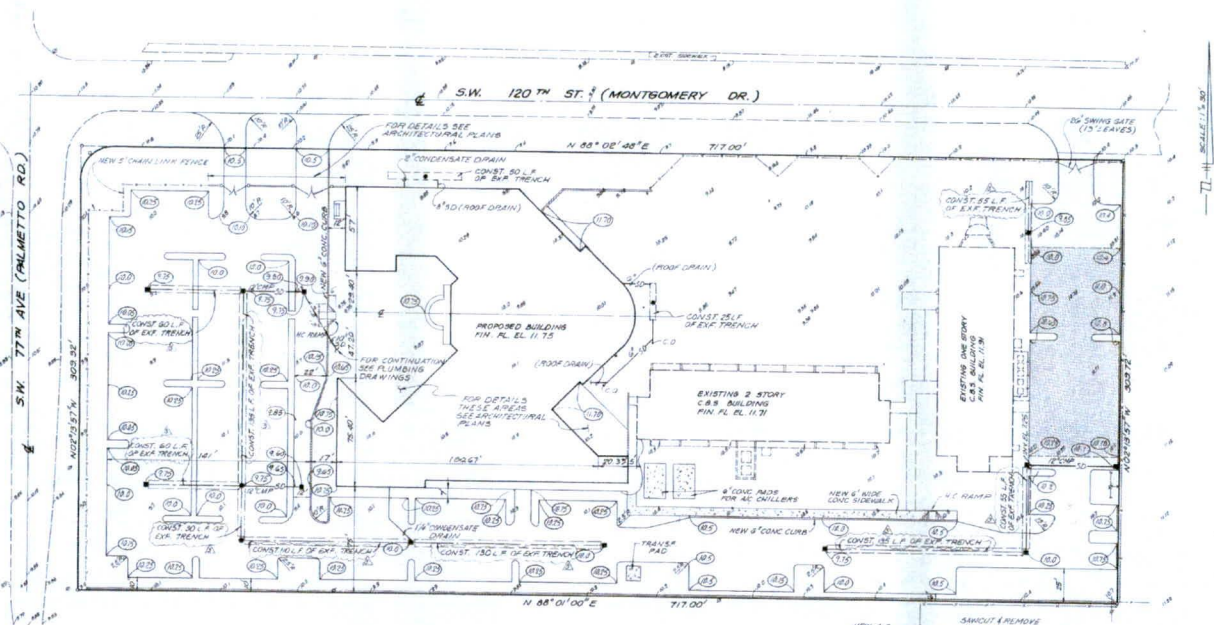
**A-6.3**



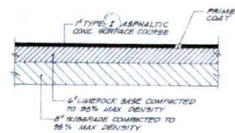
**NOTES:**

1. FOR LEGEND SEE SHEET C-3.
2. PROVIDE CONCRETE WALL STOPS AT 2'-0" FROM EXISTING WALL STOPS.
3. ALL DIMENSIONS ARE SHOWN TO THE SOUTH PROPERTY LINE.

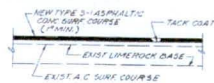
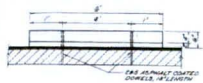
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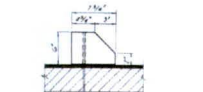
**PAVING, GRADING AND DRAINAGE PLAN**



**ASPHALTIC PAVEMENT DETAIL**  
N.T.S.



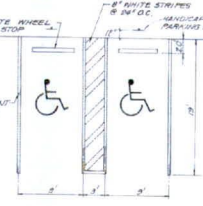
**CONNECTION OF NEW TO EXIST. PAVMT.**  
N.T.S.



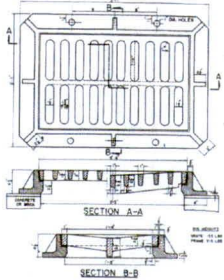
**CONCRETE WHEEL STOP**  
N.T.S.



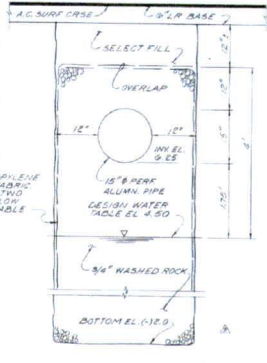
**HANDICAPPED PARKING SIGN**  
N.T.S.



**HANDICAPPED PARKING DETAIL**  
N.T.S.



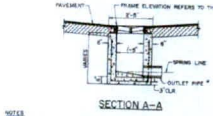
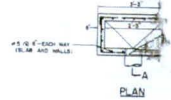
**CATCH BASIN FRAME & GRATE**  
N.T.S.



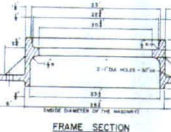
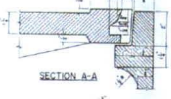
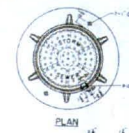
**EXFILTRATION TRENCH**  
N.T.S.

**LEGEND**

EXISTING	PROPOSED	DESCRIPTION
(Symbol)	(Symbol)	SPOT ELEVATION
(Symbol)	(Symbol)	WOOD LIGHT POLE
(Symbol)	(Symbol)	CATCH BASIN
(Symbol)	(Symbol)	STORM SWAYED MANHOLE
(Symbol)	(Symbol)	NUMBER OF STANDARD PARKING SPACES
(Symbol)	(Symbol)	NUMBER OF COMPACT PARKING SPACES
(Symbol)	(Symbol)	NUMBER OF PARKING SPACES
(Symbol)	(Symbol)	CONCRETE
(Symbol)	(Symbol)	ASPHALTIC CONCRETE IN CONCRETE OR CHAIN LINK A



**CATCH BASIN**  
N.T.S.



**MANHOLE FRAME & COVER**  
N.T.S.

BY	DATE	DESCRIPTION



**Bet Shira Congregation**

**ZYSCOVICH & GRAFTON**  
ARCHITECTS  
INTERIOR DESIGNERS  
PLANNERS  
18 S.W. 11TH STREET  
MIAMI, FLORIDA 33136  
(305) 358-3332

REGISTERED PROFESSIONAL ENGINEER  
STATE OF FLORIDA  
NO. 12000  
EXPIRES 12/31/2025

**C-3**

**CIVICA**  
ARCHITECTS & URBAN DESIGNERS

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Doral, FL 33126  
Tel: 305.583.9658  
www.civicaarch.com  
AA #2801093

**PROJECT**  
TRUE NORTH  
CLASSICAL ACADEMY  
PINECREST CAMPUS

7500 SW 120TH STREET  
MIAMI, FLORIDA 33156  
PHONE: 202-914-004-0050

**APPLICANT**  
TRUE NORTH  
CLASSICAL ACADEMY  
3333 SW 72ND STREET  
MIAMI, FLORIDA 33173



**CIVICA PROJECT NO.**  
209906

**ISSUED FOR**  
CRC SUBMITTAL

No.	DATE	REVISION	BY


**DRAWN BY** APPROVED BY  
DATE SCALE  
2023 AS SHOWN  
KEYPLAN

**PERMIT SUBMITTAL**



ROLANDO LLANES  
AR - 0011190

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**SHEET TITLE**  
EXISTING SANCTUARY  
PAVING, GRADING  
&  
DRAINAGE PLAN

**SHEET NUMBER**  
A-6.4

REFERENCE DRAWING FROM PERMIT RECORDS





P:\2023\20230906\20230906\_A020101\_CIVICA.dwg, 1/16/2023 8:41:04 AM, DWG, 100%

**CIVICA**

8323 NW 12th St, Suite 106  
Doral, FL 33128  
Tel: 305.593.9656  
www.civicaonline.com  
AA #25517933

PROJECT  
**TRUE NORTH CLASSICAL ACADEMY PINECREST CAMPUS**  
7500 SW 107TH STREET  
MIAMI, FLORIDA 33196  
FOLIO# 20-0014-304-0050

APPLICANT  
**TRUE NORTH CLASSICAL ACADEMY**  
893 SW 72ND STREET  
MIAMI, FLORIDA 33173



CIVICA PROJECT No:  
**209906**

ISSUED FOR  
**CRC SUBMITTAL**

No.	DATE	REVISION	BY
1	06/16/23	CRC ZONING COMMENTS	MS/AF
2	11/08/24	CRC ZONING COMMENTS	MS/AF

DRAWN BY: \_\_\_\_\_ APPROVED BY: \_\_\_\_\_  
DATE: 2023 SCALE: AS SHOWN  
KEYPLAN:

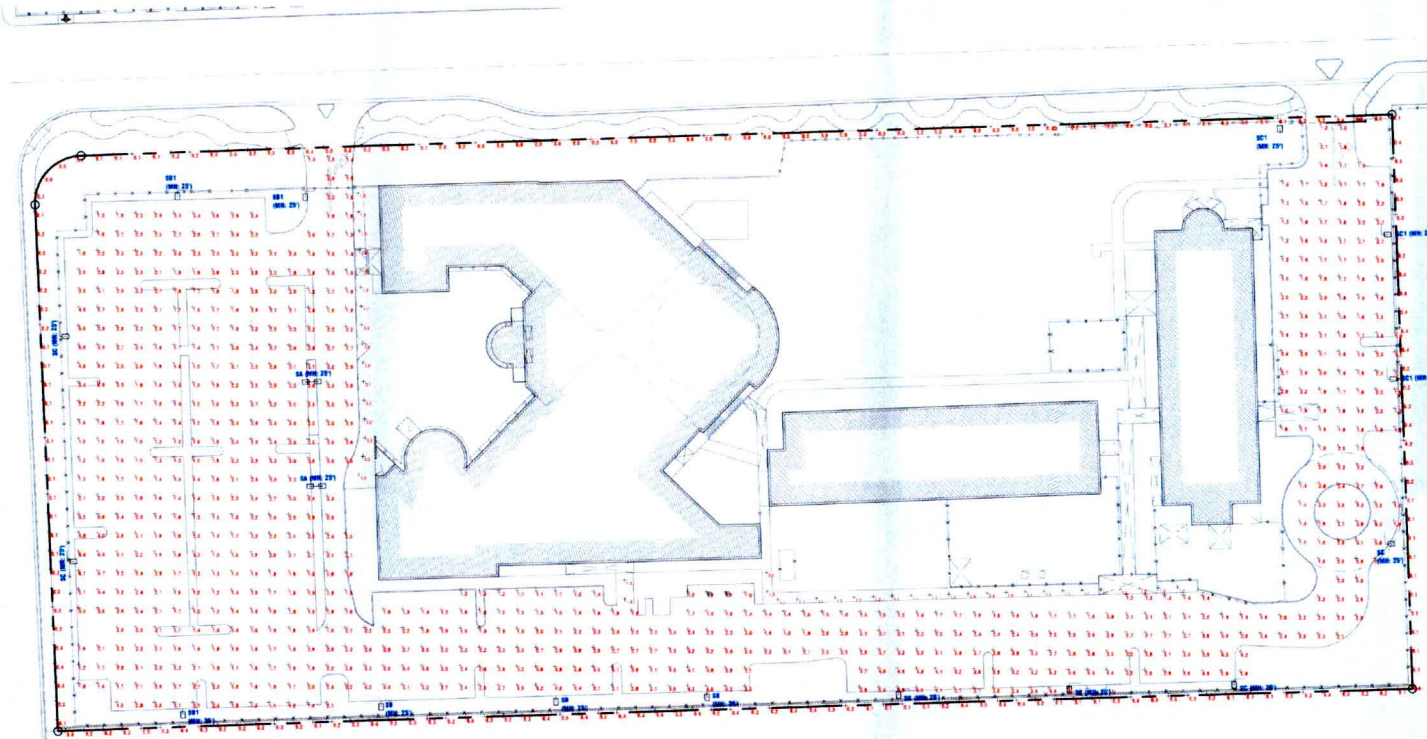
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SEAL & SIGNATURE



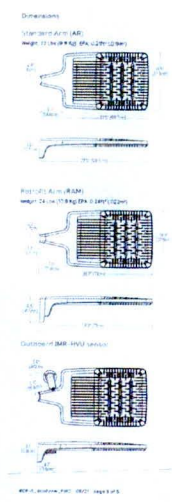
ROLANDO LLANES  
AS: 0000100  
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SHEET TITLE  
**EXISTING SITE PLAN: ILLUMINATION & PHOTOMETRIC**

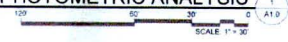
SHEET NUMBER



**ECF-S EcoForm wildlife**  
Site & Area



**EXISTING SITE PLAN: PROPOSED ILLUMINATION & PHOTOMETRIC ANALYSIS**



Symbol	Qty	Label	Arrangement	Lum. Lumens	LLD	Luminaire Watts	Total Watts	Description
[Symbol]	2	SA	Back-Back	15443	0.900	121.6	486.4	GARDCO (2)ECF-S-32L-1_2A-NW-G2-5W POLE MOUNT 25' AFG
[Symbol]	3	SB	Single	18176	0.900	182.71	548.13	GARDCO ECF-S-48L-1_2A-NW-G2-4-HIS POLE MOUNT 25' AFG
[Symbol]	3	SB1	Single	12183	0.900	121.6	364.8	GARDCO ECF-S-32L-1_2A-NW-G2-4-HIS POLE MOUNT 25' AFG
[Symbol]	6	SC	Single	16145	0.900	180.0032	1080.019	GARDCO ECF-S-48L-1_2A-NW-G2-BLC POLE MOUNT 25' AFG
[Symbol]	3	SC1	Single	6930	0.900	71.82	215.46	GARDCO ECF-S-32L-700-NW-G2-BLC POLE MOUNT 25' AFG
[Symbol]	1	SD	Single	7445	0.900	72.9	72.9	GARDCO ECF-S-32L-700-NW-G2-2-HIS POLE MOUNT 25' AFG

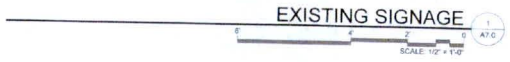
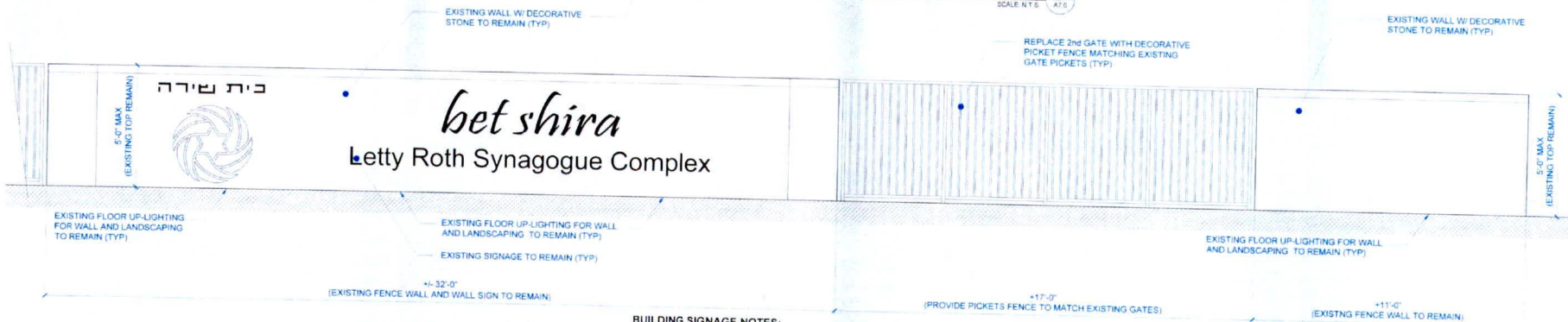
Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min	Grid Z
PARK & DRIVE	Illuminance	Fc	2.35	6.1	1.0	2.35	6.10	0
PROPERTY LINE	Illuminance	Fc	0.20	0.5	0.0	N.A	N.A	N.A



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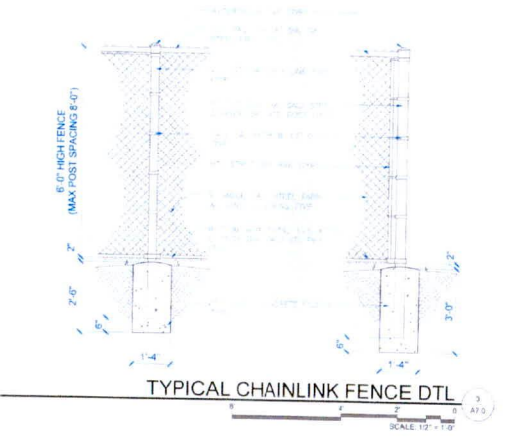
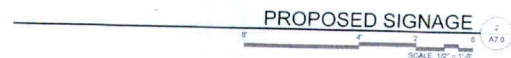


**EXISTING SIGNAGE & ROW FENCE DETAIL** SCALE: N.T.S. A7.0



**BUILDING SIGNAGE NOTES:**

- 1) PLEASE NOTE THAT AS PER FLORIDA FIRE PREVENTION CODE AND MIAMI-DADE COUNTY FIRE RESCUE, BUILDINGS WITHIN THE SAME PROPERTY SHALL BE EASILY IDENTIFIED WITH A SPECIFIC DESIGNATION LETTER, NUMBER, OR NAME THAT WILL BE ALSO TIED TO THE FIRE ALARM.
- 2) POLICE DEPARTMENT AND FIRST-RESPONDERS SHALL CONDUCT LOCK-DOWN DRILLS ESTABLISH SAFETY PROTOCOLS BASED ON THESE DESIGNATION.
- 3) UNDER THIS APPLICATION ALL EXISTING BUILDINGS ON THIS PROPERTY SHALL BE USED HAVE AN IDENTIFICATION SIGN. SIGNAGE SHALL NOT FACE THE PUBLIC ROW.



EXISTING 8\"/>

PROVIDE NEW SWIRLS AND ADDITIONAL AS PER LANDSCAPING SO AS TO COMPLY WITH GREEN BUFFER REQUIREMENTS. PLEASE SEE LANDSCAPING (DWG) (TYP.)



**DISSIMILAR USE BUFFER EXISTING CMU FENCE DETAIL** SCALE: 1/2\"/>

**CIVICA**

8323 NW 12th St. Suite 106  
 Coral FL 33129  
 Tel: 305 593 9959  
 www.civica.com  
 AA #26001052

PROJECT  
**TRUE NORTH CLASSICAL ACADEMY FINECREST CAMPUS**  
 7505 SW 103TH STREET  
 MIAMI, FLORIDA 33156  
 FOLLOW 20-5514-604-0050

APPLICANT  
**TRUE NORTH CLASSICAL ACADEMY**  
 5983 SW 72ND STREET  
 MIAMI, FLORIDA 33173



CIVICA PROJECT No  
**209906**  
 ISSUED FOR  
**CRS SUBMITTAL**

No.	DATE	REVISION	BY
08-18-23	08-18-23	CRS DRAWING COMMENTS	MS-JAF
02-22-24	02-22-24	CRS FINANCIAL COMMENTS	MS-JAF
11-20-24	11-20-24	CRS FINANCIAL COMMENTS	MS-JAF

DRAWN BY: [Blank]  
 APPROVED BY: [Blank]  
 DATE: [Blank]  
 2023 SCALE: AS SHOWN  
 KEYPLAN: [Blank]

PERMIT SUBMITTAL  
 SEAL & SIGNATURE



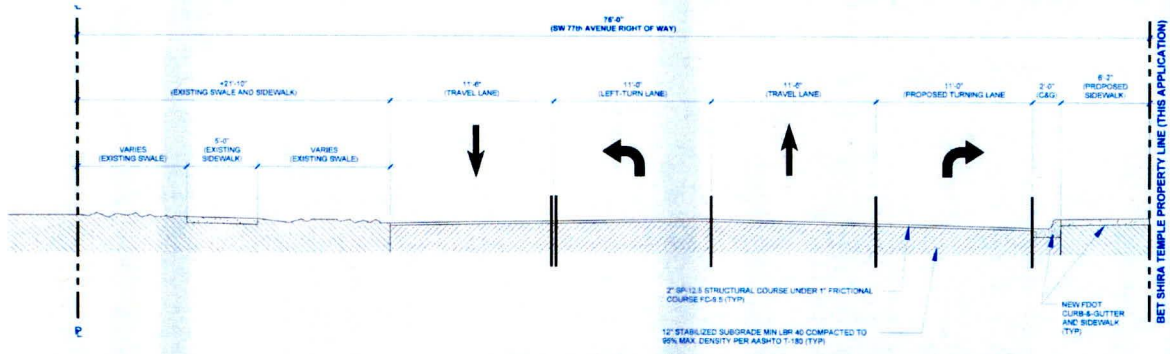
ROBERT L. LANES  
 PE, 12513  
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SHEET TITLE  
**EXISTING SIGNAGE AND FENCE DETAILS**

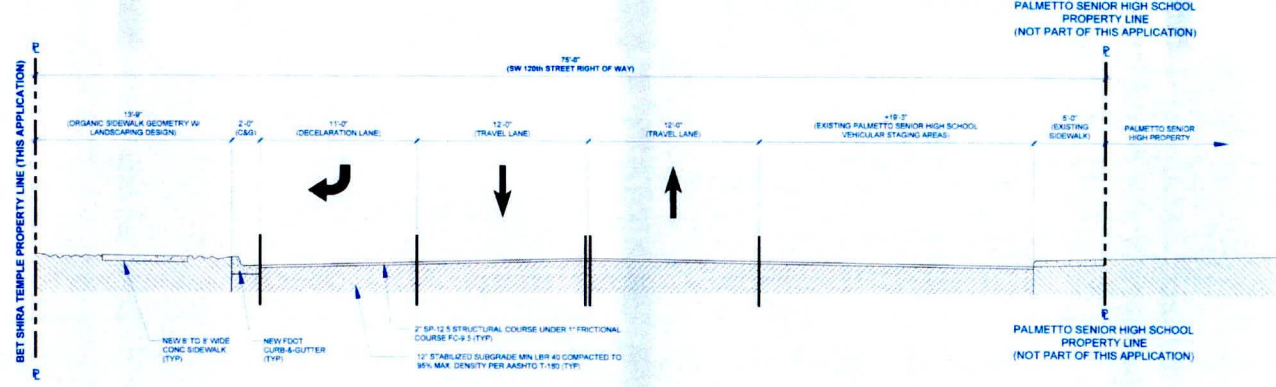
SHEET NUMBER



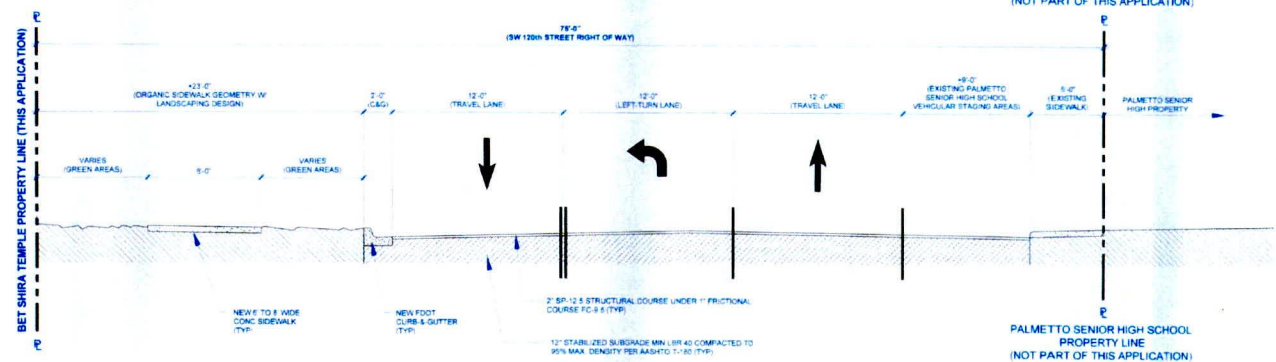
P:\2025\209906\209906.dwg (11/11/2025) 11:00:00 AM (11/11/2025) 11:00:00 AM



S.W. 77 AVENUE ROW - PROPOSED STREET SECTION



S.W. 120th STREET ROW - PROPOSED STREET SECTION



S.W. 120th STREET ROW - PROPOSED STREET SECTION



**CIVICA**  
 PROFESSIONAL ENGINEERS  
 8323 NW 12th St, Suite 106  
 Doral, FL 33126  
 tel: 305.593.9956  
 www.civicaprop.com  
 AIA #00031963

PROJECT  
 TRUE NORTH  
 CLASSICAL ACADEMY  
 PINECREST CAMPUS  
 7306 SW 120th STREET  
 MIAMI, FLORIDA 33156  
 FOLIO#: 20-501-4034-0050

APPLICANT  
 TRUE NORTH  
 CLASSICAL ACADEMY  
 8363 SW 72nd STREET  
 MIAMI, FLORIDA 33173



CIVICA PROJECT No:  
 209906

ISSUED FOR  
 CRC SUBMITTAL

No.	DATE	REVISION	BY
1	11/08/24	DR: ZENET COMMENTS	MS: JAF

DRAWN BY: KEYPLAN  
 APPROVED BY: RL  
 DATE: 2023  
 SCALE: A1 SHOWN

PERMIT SUBMITTAL  
 SEAL / SIGNATURE



ROLANDO LLANES  
 AR - 0013190

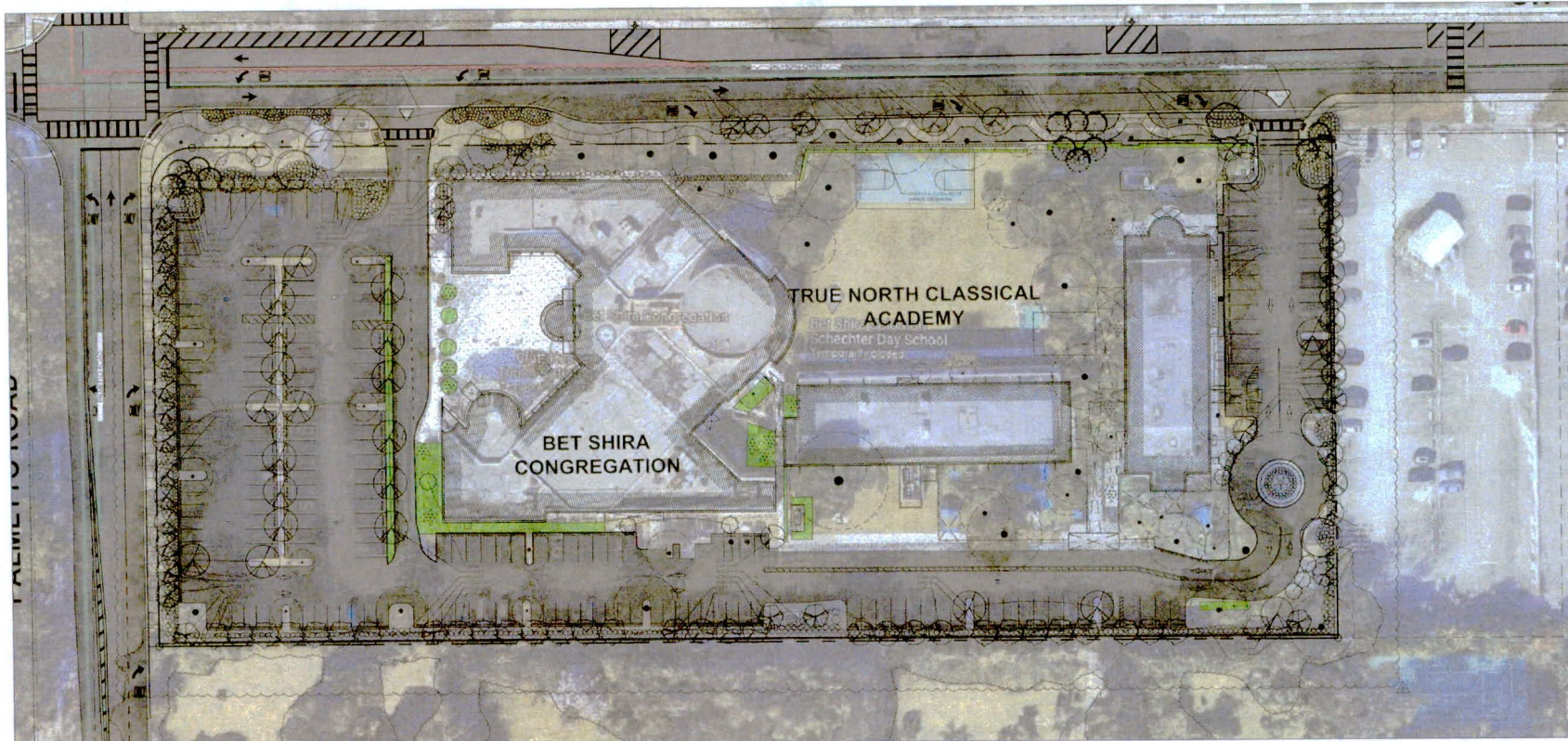
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SHEET TITLE  
 R.O.W.  
 IMPROVEMENTS:  
 PROFILE  
 DETAILS

SHEET NUMBER  
 A-7.2





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PROJECT  
**TRUE NORTH CLASSICAL ACADEMY**  
**PINECREST CAMPUS**  
 7500 SW 150TH STREET  
 MIAMI, FLORIDA 33156  
 FOLLOW 305.814.004-0050

APPLICANT  
**TRUE NORTH CLASSICAL ACADEMY**  
 3055 SW 72ND STREET  
 MIAMI, FLORIDA 33173



CIVICA PROJECT No  
**209906**

ISSUED FOR  
**CRC SUBMITTAL**

NO	DATE	REVISION	BY
1	01/08/20	VEGETATION CHANGES	TMS



DRAWN BY: **RS**  
 APPROVED BY: **AS**

DATE: **02/07/2023**  
 SCALE: **AS SHOWN**

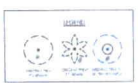
KEYPLAN

LANDSCAPE LEGEND - PS Zoning			
Code Section	Description	Required	Permitted
Div. 6.1. (6)3	Vegetation Survey	yes	yes
Div. 6.1. (6)4	Irrigation Plan	no	no
Div. 6.1. (6)1a	Open Area	86,785 sq ft	71,693 sq ft
	40% max. of open area		
Div. 6.1. (6)1b	Minimum vegetation for trees and grass/porch	140	140
	28 trees per acre (58 ac. = 140 trees)		
	22% min. green space (Excludes of parking islands)	47,734 sq ft	76,223 sq ft
Div. 6.1. (6)2	Irrigation System	no	existing
Div. 6.1. (6)3a	Trees	yes	yes
	All trees must be 12" tall, 3" cal.		
Div. 6.1. (6)3b-d	Small Trees	34	34
	All trees must be 14" tall, 3" cal. installed 30' w.c. (1006 11)		
Div. 6.1. (6)3e	Palms	42	42
	Palms covering an shade trees must be 12" tall, 3" cal. (on a 2 palms = 1 tree basis) max. 30% of required trees = 140 required trees = 30% = 42 max. palms (34 @ 2 1/2)		
Div. 6.1. (6)3f	Native Trees	47	140
	1/3 of all required trees must be native species, 140 required trees = 30% = 42 native trees		
	1/3 of required native trees must be Dade County State Parks, Company trees, or other trees = 47 required native trees = 33% = 16 trees		
Div. 6.1. (6)4a	Shrubs	1,400	1,532
	140 trees x 10 = 1400 shrubs		
	30% of all required shrubs must be native species, 1,400 x 30% = 420 native shrubs		
Div. 6.1. (6)4b	Shrubs between dissimilar trees	356	356
	Shrubs between dissimilar trees must be installed 24" o.c. and 30" tall, 712 1/4 / 24" = 356 shrubs		
Div. 6.1. (6)4c	Shrubs between dissimilar trees	29	29
	Times between dissimilar trees must be installed 25" o.c. and 16" tall, 712 1/4 / 25" = 29 trees		
Div. 6.1. (6)7	Walls	yes	no
	Walls shall be screened by a hedge installed 24" o.c. and 30" tall		
	<b>TOTAL TREES REQUIRED</b>	<b>174</b>	<b>226 (84 exist)</b>
	<b>TOTAL SHRUBS REQUIRED</b>	<b>1,756</b>	

\*Measuring existing hedges and shrub areas @ 24" o.c. avg. spacing.

PLANT LIST			
KEY	PLANT NAME	QTY	HT./SZ
CSP	Callisaurus spicatus (Callisaurus)	9	12' tall x 2' spread, 3" DBH, 4' CT max.
COOI	Crotona diversifolia (Pigeon Plum)	117	14' tall x 3' spread, 3" DBH, 4' CT max.
COEK	Crotona erica (Green Buttonwood)	31	12' tall x 2' spread, 3" DBH, 4' CT max.
COES	Crotona erica 'Serena' (Silver Buttonwood)	7	12' tall x 2' spread, 3" DBH, 4' CT max.
COSE	Crotona erica (Silver Buttonwood)	20	14' tall x 3' spread, 3" DBH, 4' CT max.
GUSA	Guzmania (Orange Queen)	2	12" tall x 3' spread, 3" DBH, 4' CT max.
GUTL	Guzmania (Orange Queen)	23	12" tall x 3' spread, 3" DBH, 4' CT max.
PEL	Pinus allouyi (Sandy Pine)	14	14' tall x 3' spread, 3" DBH, 4' CT max.
GLUM	Glossy Starburst (Glossy Starburst)	4	14" tall x 3' spread, 3" DBH, 4' CT max.
SOL	Solenanthe (Parrot Tree)	2	14" tall x 3' spread, 3" DBH, 4' CT max.
KEY	PLANT NAME	QTY	HT./SZ
SAPA	Salvia palmata (Salvia Palm)	26	16" tall CA, rounded top, 10' spread, 4' CT max.

PLANT LIST			
KEY	PLANT NAME	QTY	HT./SZ
CHRU	Chrysalidocarpus (Coral Tree)	396	18' tall
CHRI	Chrysalidocarpus (Coral Tree)	285	12' tall x 18' spread
CHIS	Chrysalidocarpus (Coral Tree)	246	30' tall
ELSM	Elaeagnus (Red Shrimp)	48	30' tall
HANO	Hamelia (Red Shrimp)	149	24' tall
MUCA	Mulberry (Red Shrimp)	278	3' tall, 10' cal.
REBA	Rapanea (Red Shrimp)	11	3' tall, 10' cal.
SEBA	Sesuvium (Red Shrimp)	30	30' tall
SEEE	Sesuvium (Red Shrimp)	16	24' tall, 10' cal.
TRDA	Trichostema (Red Shrimp)	139	3' tall, 10' cal.
KEY	PLANT NAME	QTY	HT./SZ
NEEA	Nerium (Red Shrimp)	3044	1' tall, 10' cal., 12' high
MISC/UNUSUAL			
	Planting Soil	no	ms. 10' deep and backfill 18" depth in all new planting areas
	20% Silver Sand	20%	20% in all new planting areas
	10% Standard Product	10%	10% in all new planting areas
	St. Augustine Grass	no	10' tall, 10' cal., 12' high
	Shrubbed Mulch	no	10' tall, 10' cal., 12' high



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SEAL/SIGNATURE

Kristi Gamble  
 F.L.A. #0687208

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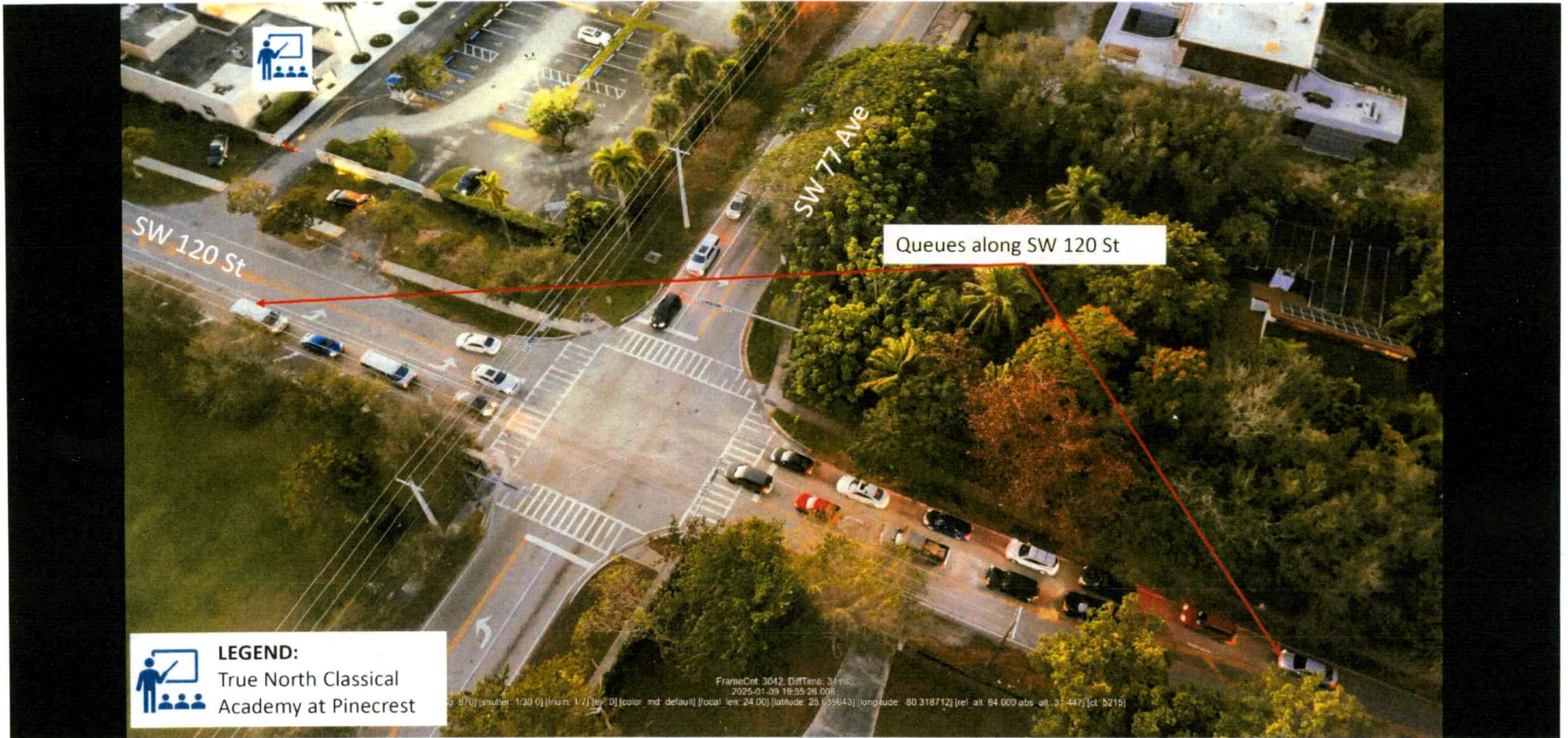
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SHEET TITLE  
**PLANTING PLAN**

SHEET NUMBER: **as noted**  
 LA2-01



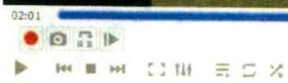
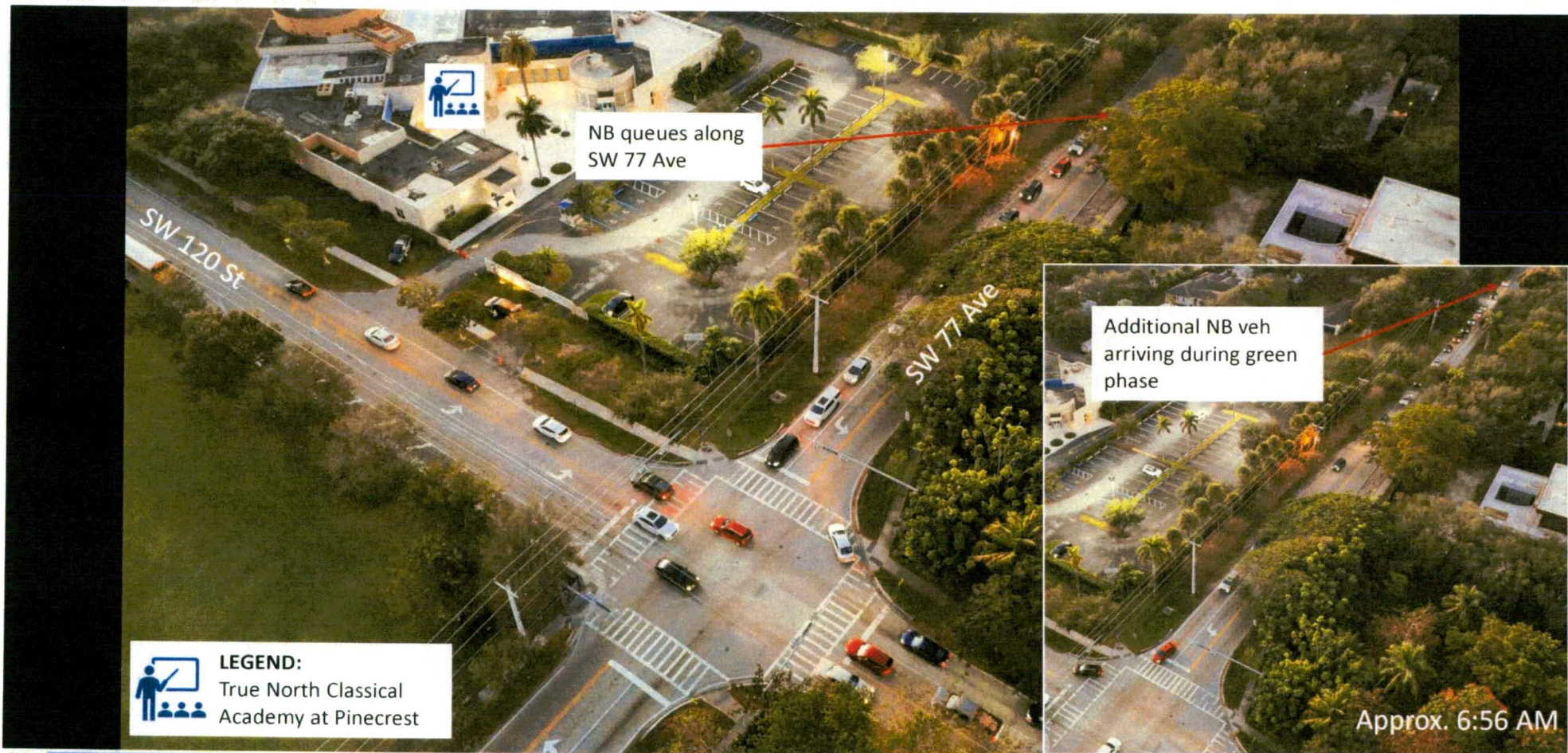
Drone Field Observations (Jan. 9, 2025) for True North Classical Academy Study



Approx. 6:55 AM

05:25

Drone Field Observations (Jan. 9, 2025) for True North Classical Academy Study



Approx. 6:55 AM



Drone Field Observations (Jan. 9, 2025) for True North Classical Academy Study



**LEGEND:**  
True North Classical  
Academy at Pinecrest

EB queues along  
SW 120 St

SW 77 Ave

FrameCut 2025-01-09 06:57:25  
2025-01-09 06:57:25  
[{"x": 100, "y": 100, "z": 100}, {"x": 200, "y": 100, "z": 100}, {"x": 300, "y": 100, "z": 100}, {"x": 400, "y": 100, "z": 100}, {"x": 500, "y": 100, "z": 100}, {"x": 600, "y": 100, "z": 100}, {"x": 700, "y": 100, "z": 100}, {"x": 800, "y": 100, "z": 100}, {"x": 900, "y": 100, "z": 100}, {"x": 1000, "y": 100, "z": 100}]]



Approx. 6:57 AM

05:25

Drone Field Observations (Jan. 9, 2025) for True North Classical Academy Study

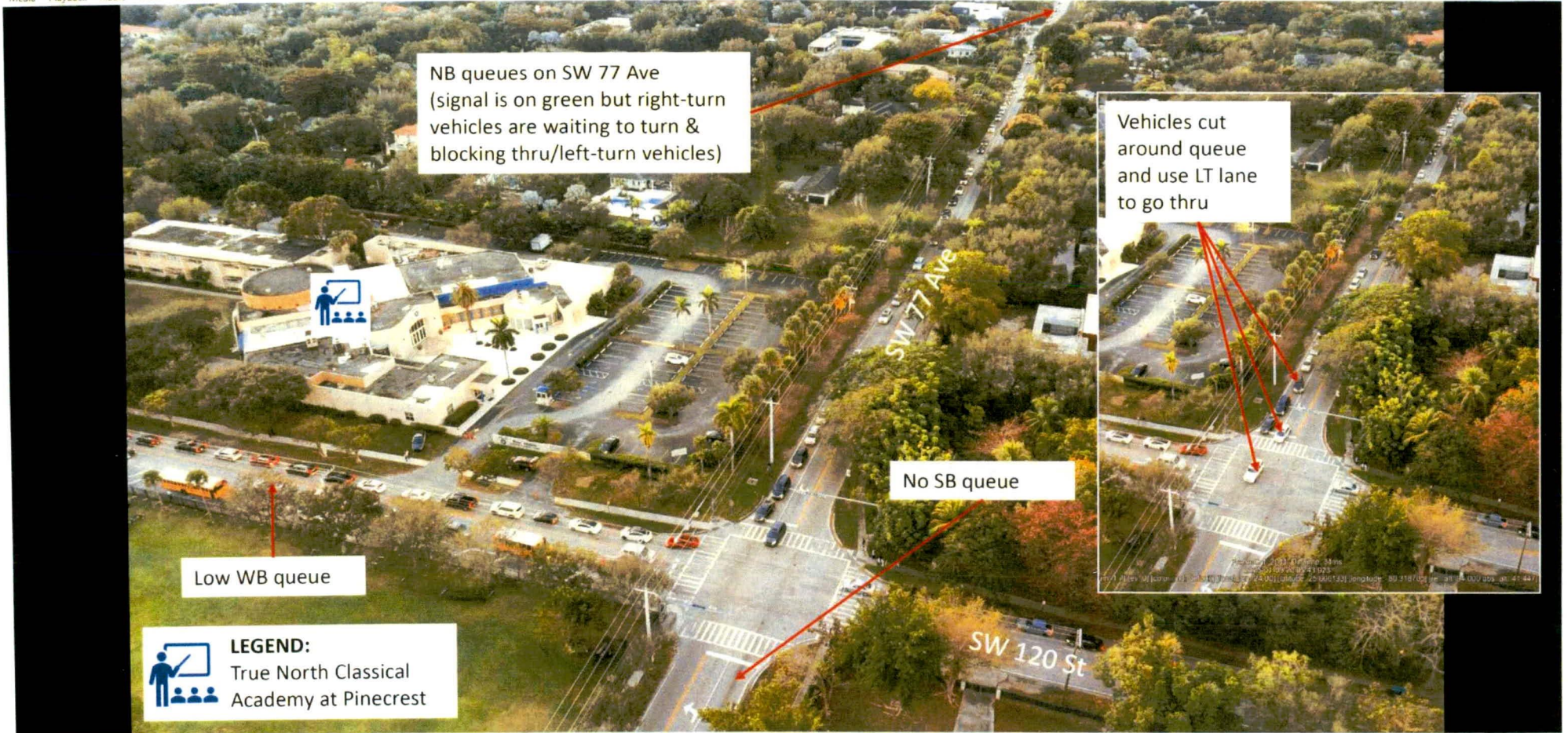


**LEGEND:**  
True North Classical  
Academy at Pinecrest

EB queues along  
SW 120 St

Drone Field Observations (Jan. 9, 2025) for True North Classical Academy Study

DJI\_20250109200435\_0008\_D.MP4 - VLC media player  
Media Playback Audio Video Subtitle Tools View Help



00:47  
VLC media player playback controls

05:25

Approx. 7:05 AM

### Drone Field Observations (Jan. 9, 2025) for True North Classical Academy Study



Drone Field Observations (Jan. 9, 2025) for True North Classical Academy Study

DJI\_20250109200435\_0008\_D\_MP4 - VLC media player

Media Playback Audio Video Subtitle Tools View Help



NB right-turning vehicles conflicting with EB vehicles

EB queue along SW 120 St spills into intersection

No SB queue

**LEGEND:**  
True North Classical Academy at Pinecrest

Frame: 01\_062, Duration: 33 ms  
2025-01-09 20:07:04  
216, 571M, 1.31 x 2.17 | lens: 21mm, f/2.8 | ISO: 100 | altitude: 25.956781 | longitude: -80.318523 | latitude: 25.956781

02:28  
VLC media player playback controls

Approx. 7:07 AM

05:25  
Battery level indicator

### Drone Field Observations (Jan. 9, 2025) for True North Classical Academy Study




Drone Field Observations (Jan. 9, 2025) for True North Classical Academy Study

DJI\_20250109201001\_0009\_D.MP4 - VLC media player  
Media Playback Audio Video Subtitle Tools View Help



Student dropped off on SW 77 Ave

Same student walking thru traffic toward the school

 **LEGEND:**  
True North Classical Academy at Pinecrest

04:22  
▶ ⏪ ⏩ ⏴ ⏵ ⏶ ⏷ ⏸

Approx. 7:15 AM



### Drone Field Observations (Jan. 9, 2025) for True North Classical Academy Study



Diminished NB  
queues along  
SW 77 Ave

**LEGEND:**  
True North Classical  
Academy at Pinecrest

Approx. 7:17 AM

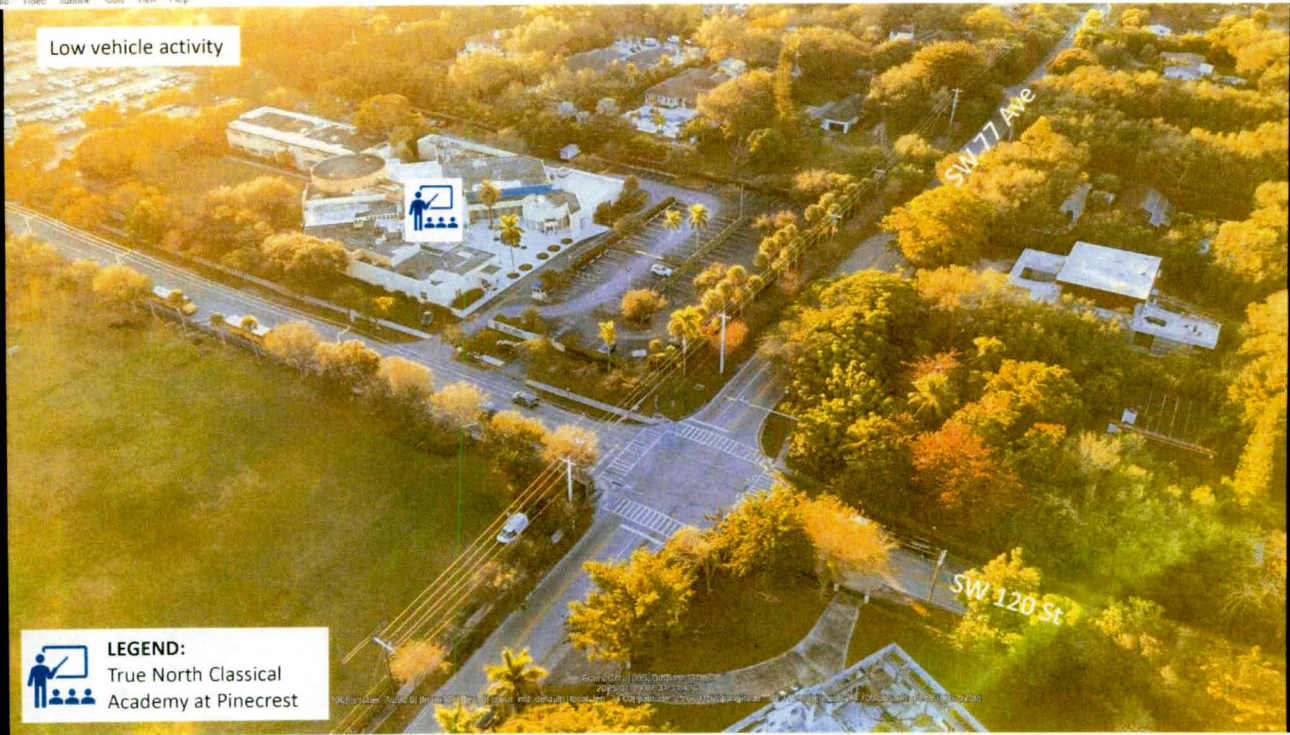
Drone Field Observations (Jan. 9, 2025) for True North Classical Academy Study

DP\_20250109073054\_0021\_D1MP4 - VLC media player  
Media Playback Audio Video Subtitle Tools View Help

Low vehicle activity



**LEGEND:**  
True North Classical  
Academy at Pinecrest



Approx. 7:30 AM

06:25

### Drone Field Observations (Jan. 9, 2025) for True North Classical Academy Study

Queues pick up again after 7:40 AM

NB queues along SW 77 Ave (roadway related)



Vehicles entering the school



**LEGEND:**  
True North Classical Academy at Pinecrest


Frame: 1000 / 1000 (FPS: 30) | Time: 00:05:25  
[00] [Status: 5526] [Filename: 1\_1\_100\_1000\_30ms\_30FPS] [Resolution: 1920x1080] [Position: 25.960134 | longitude: -80.316792] [Latitude: 25.960134] [Altitude: 10.0m] [Zoom: 100%]

Drone Field Observations (Jan. 9, 2025) for True North Classical Academy Study

DJI\_20250109075624\_0024\_D.MP4 - VLC media player  
Media Playback Audio Video Subtitle Tools View Help



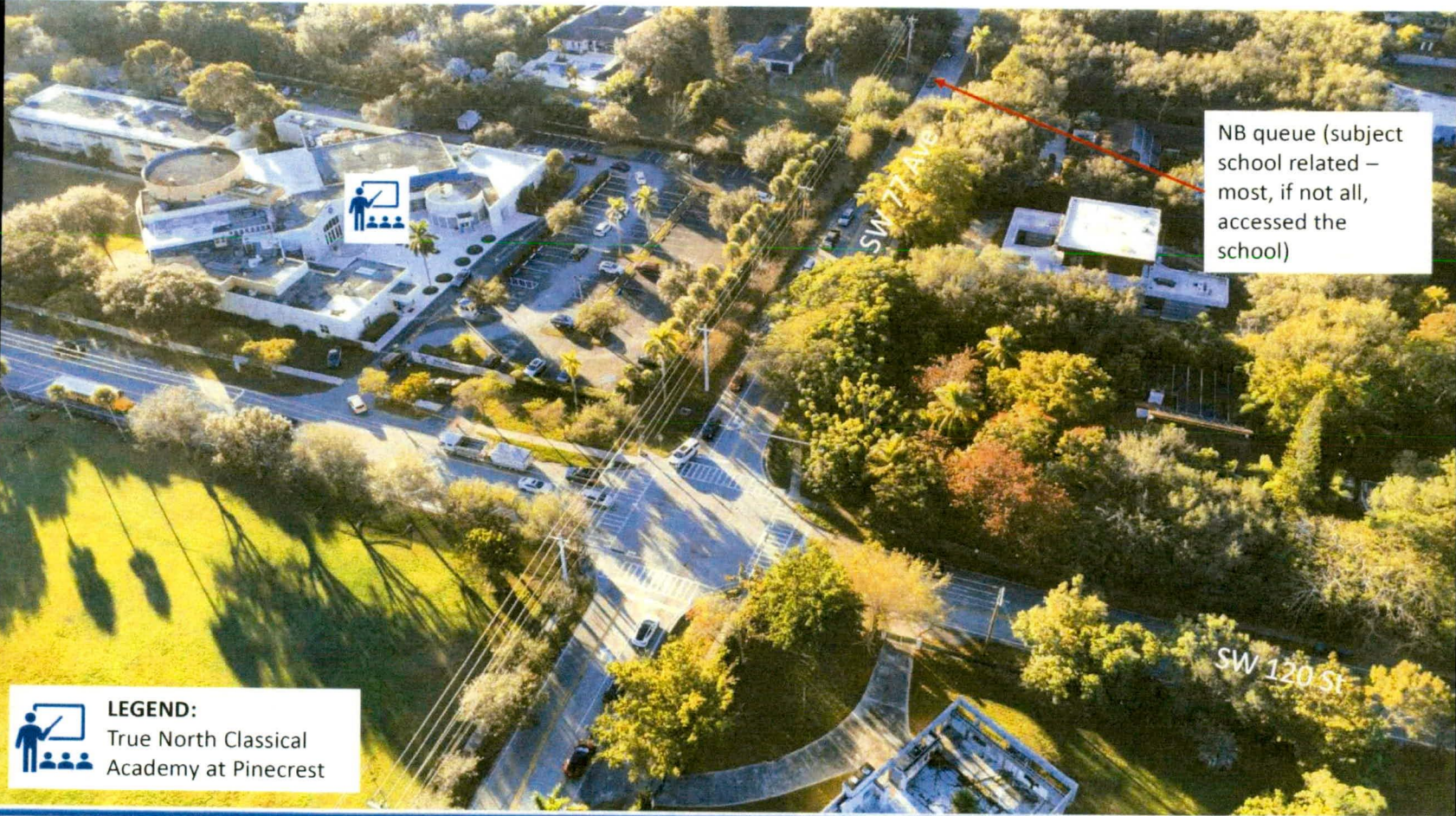
Number of vehicles  
accessing the subject  
school increases around  
7:57 am

 **LEGEND:**  
True North Classical  
Academy at Pinecrest

02:40  


Approx. 7:59 AM

05:25




NB queue (subject school related - most, if not all, accessed the school)

**LEGEND:**  
True North Classical Academy at Pinecrest

Drone Field Observations (Jan. 9, 2025) for True North Classical Academy Study



Vehicles conduct  
WBLT into school &  
delay EB traffic

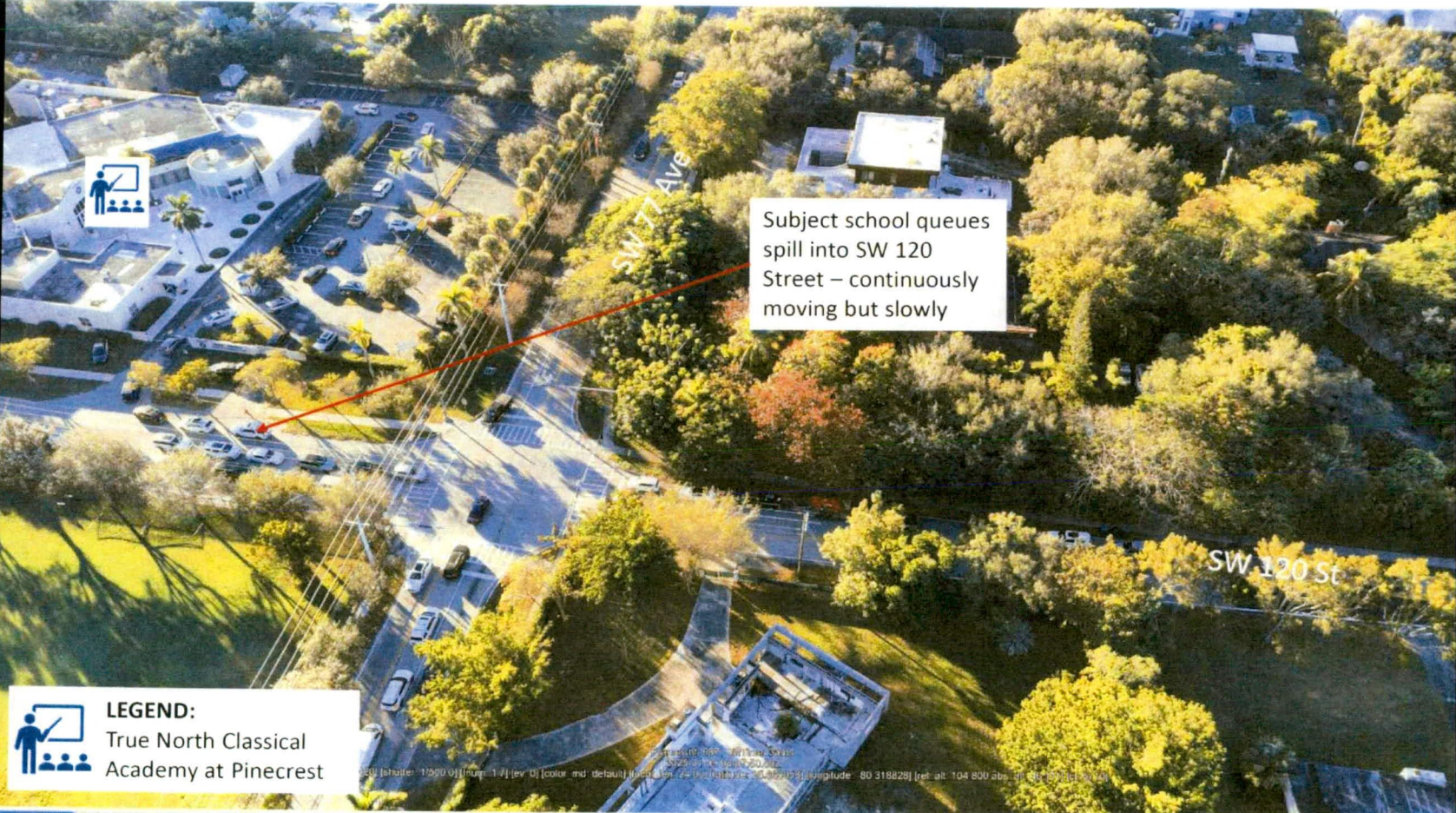
 **LEGEND:**  
True North Classical  
Academy at Pincrest



Approx. 8:00 AM



# Drone Field Observations (Jan. 9, 2025) for True North Classical Academy Study

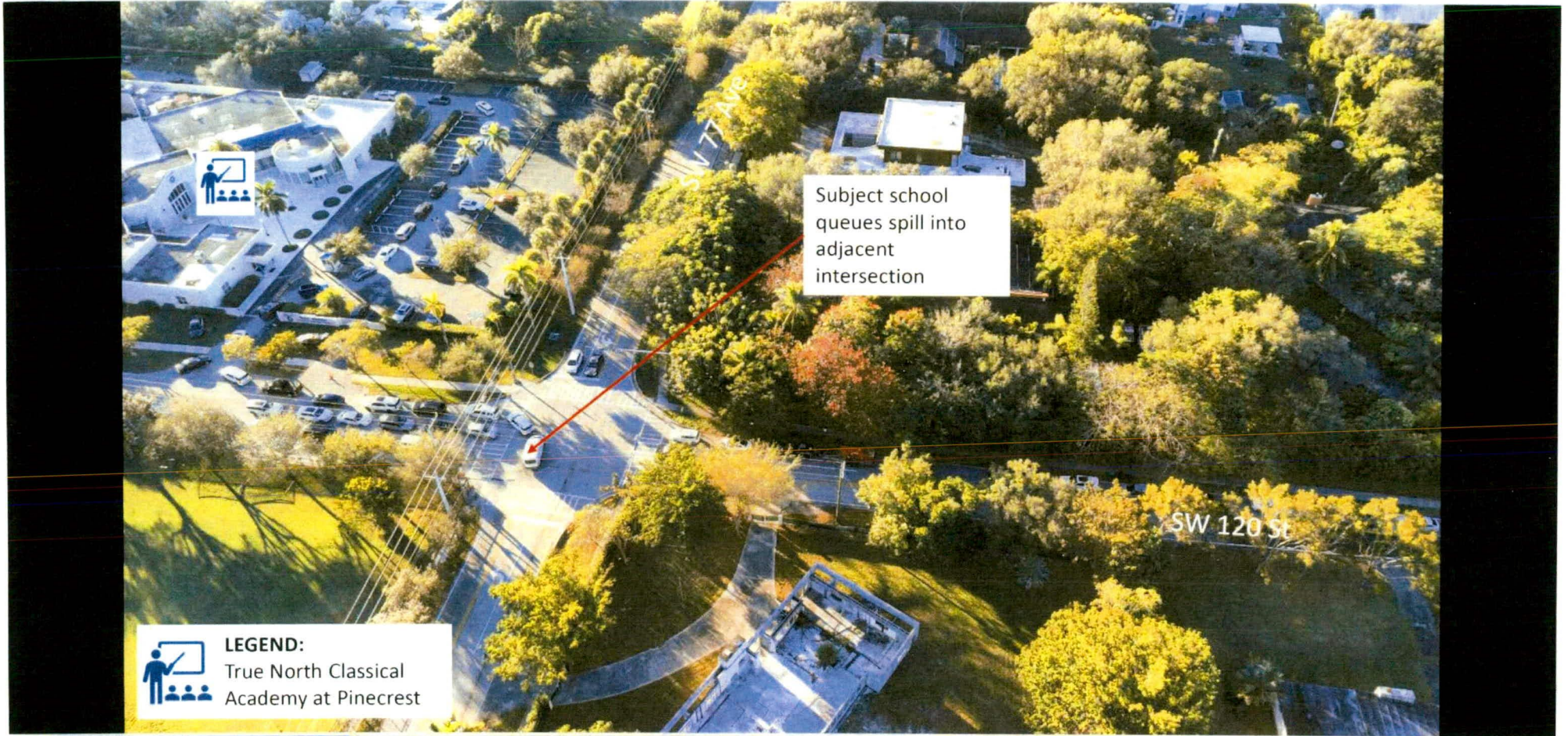


Subject school queues spill into SW 120 Street – continuously moving but slowly

**LEGEND:**  
True North Classical Academy at Pinecrest

Drone Field Observations (Jan. 9, 2025) for True North Classical Academy Study

DJI\_20250109080716\_0026\_D:MP4 - VLC media player  
Media Playback Audio Video Subtitle Tools View Help

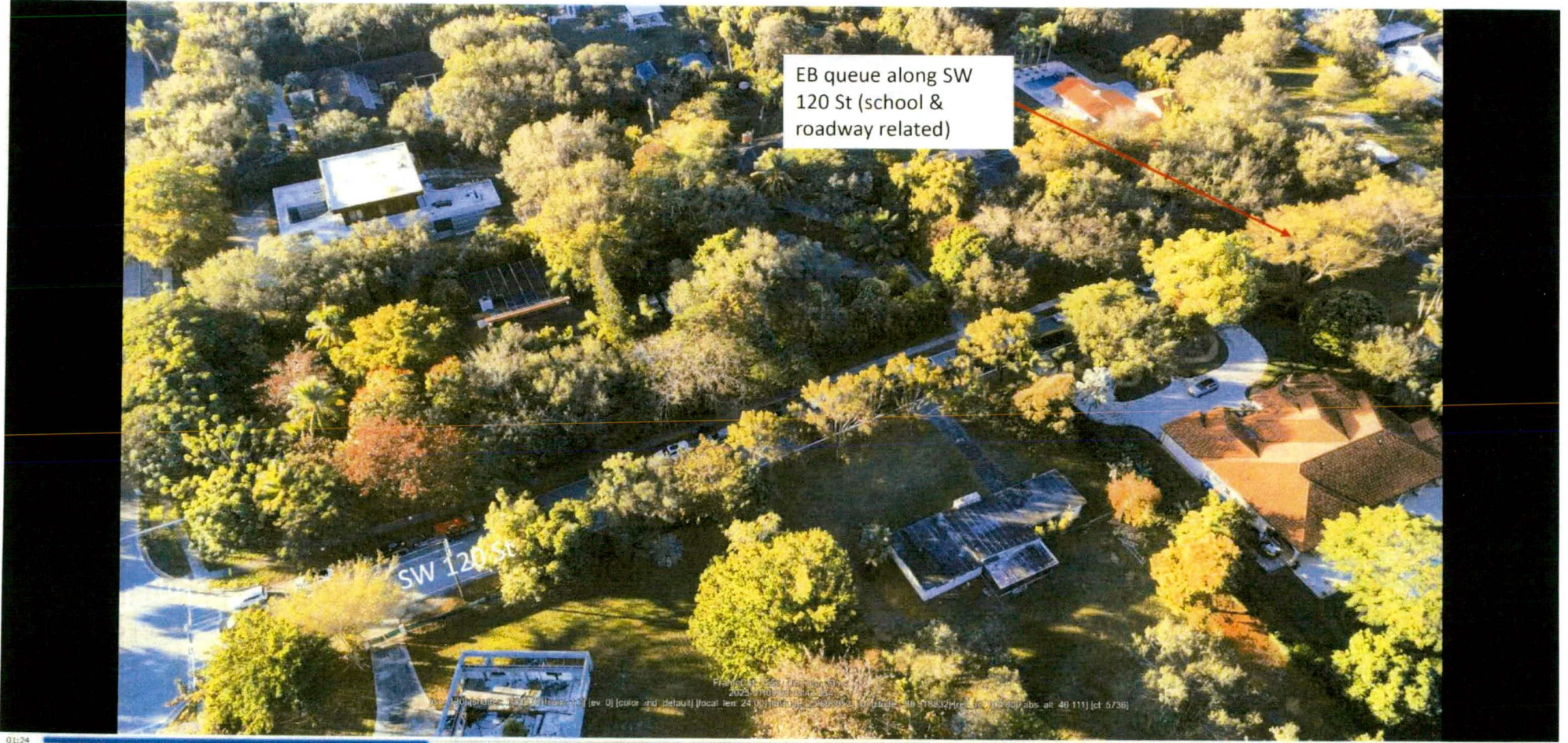


01:07  
VLC media player playback controls: play, stop, previous, next, full screen, playlist, extended settings, repeat, shuffle, volume, and close.

Approx. 8:08 AM

05:25  
System tray icons: network, volume, and power.

### Drone Field Observations (Jan. 9, 2025) for True North Classical Academy Study



Approx. 8:08 AM

Drone Field Observations (Jan. 9, 2025) for True North Classical Academy Study

DJI\_20250109081243\_0027\_D.MP4 - VLC media player

Media Playback Audio Video Subtitle Tools View Help

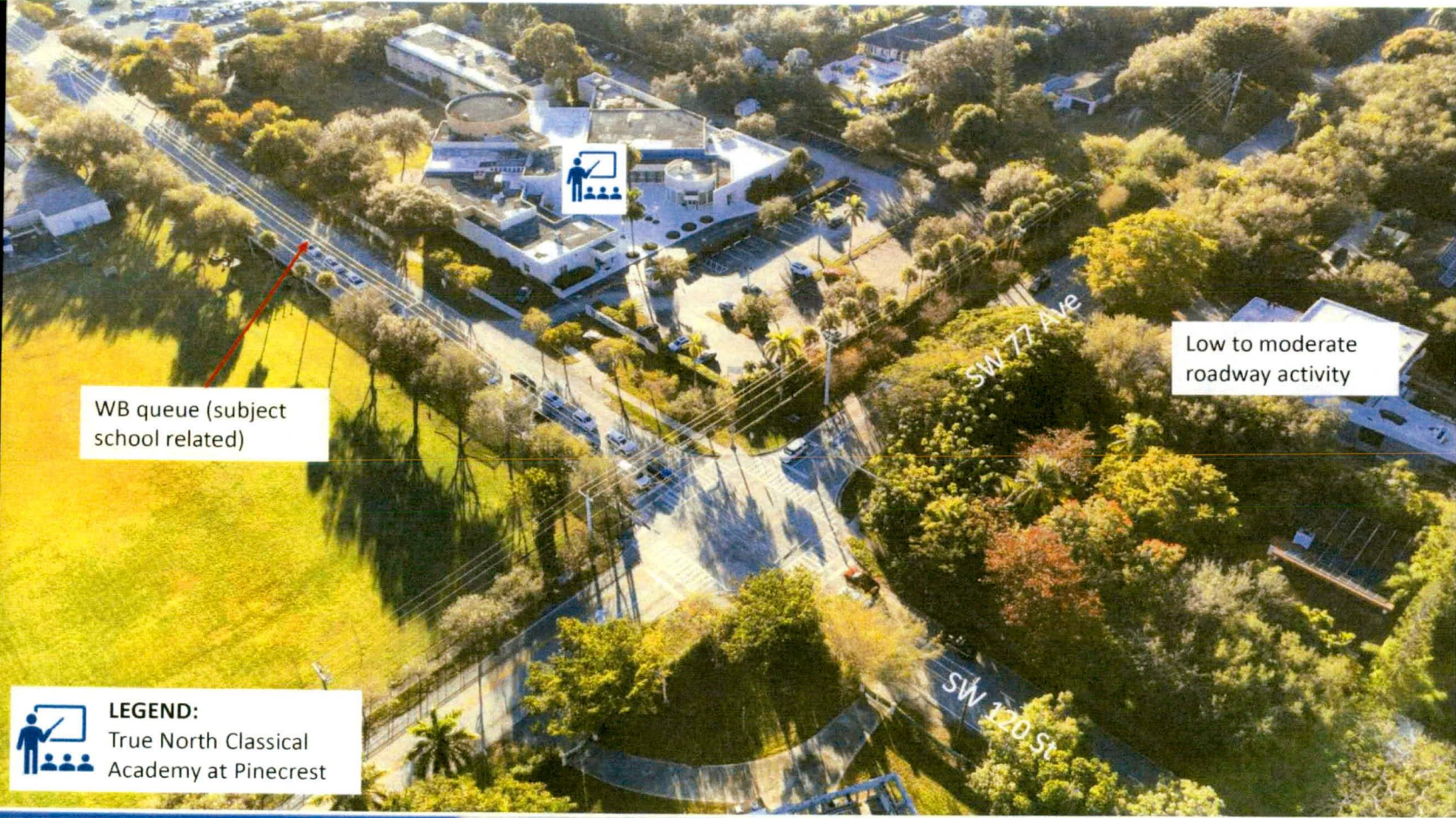


00:07

⏪ ⏩ ⏴ ⏵ ⏸ ⏹


Approx. 8:13 AM

05:13



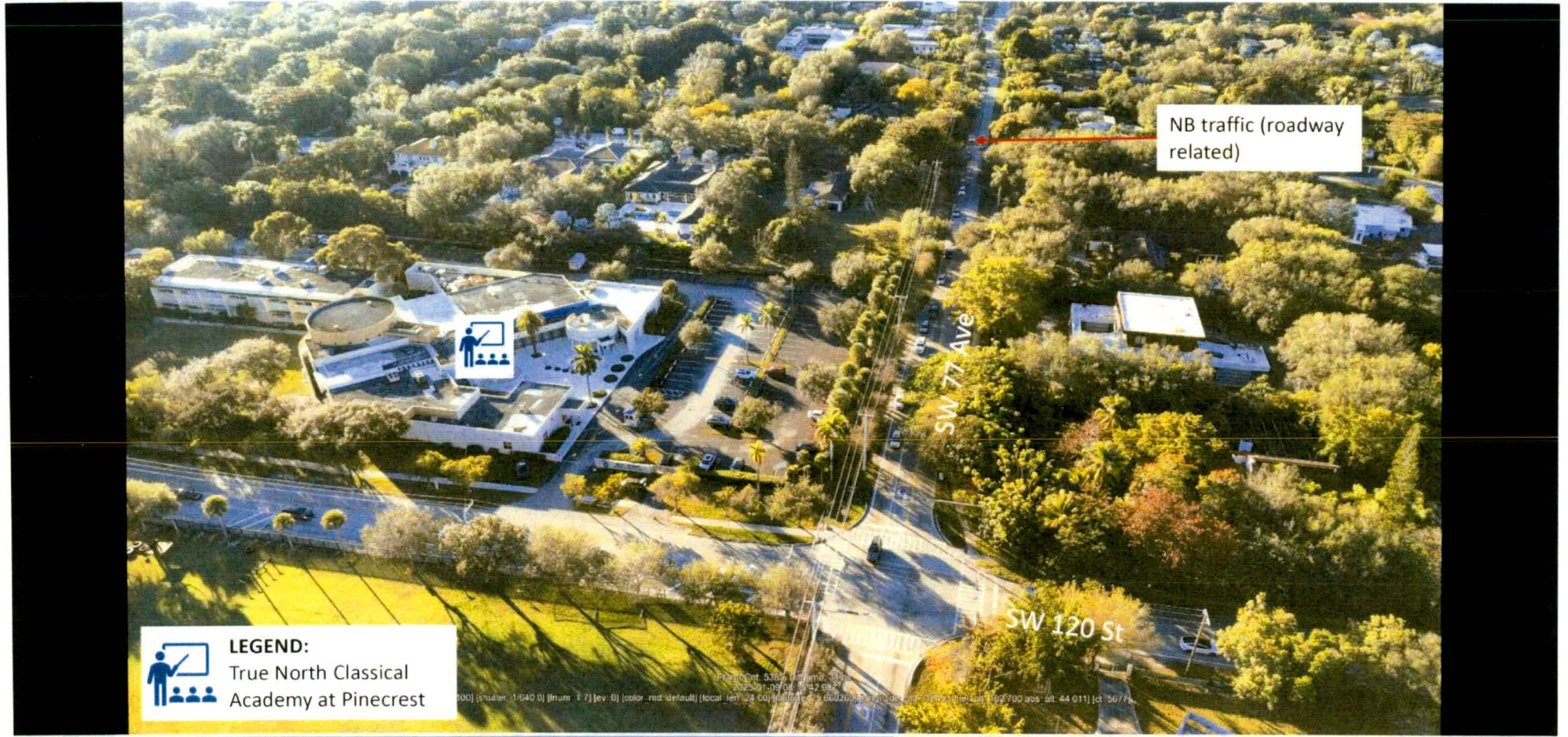
WB queue (subject school related)

Low to moderate roadway activity

 **LEGEND:**  
True North Classical Academy at Pinecrest

Drone Field Observations (Jan. 9, 2025) for True North Classical Academy Study

DJI\_20250109081243\_0027\_D.MP4 - VLC media player  
Media Playback Audio Video Subtitle Tools View Help



**LEGEND:**  
True North Classical  
Academy at Pinecrest

ProRes 5384 1000000 3000  
002:59 259 04 5:27 01:17  
000 | studio: 1:640 0 | (num: 1 7) | ev: 0 | color: md: default | local: len: 24 60 | latitude: 25.862269 | longitude: -80.216931 | altitude: 112 700 amsl: 44 011 | lat: 36/11

02:59  
▶ ⏪ ⏩ ⏸ ⏹

Approx. 8:16 AM

05:13  
📶 50%

# Drone Field Observations (Jan. 7, 2025) for True North Classical Academy Study



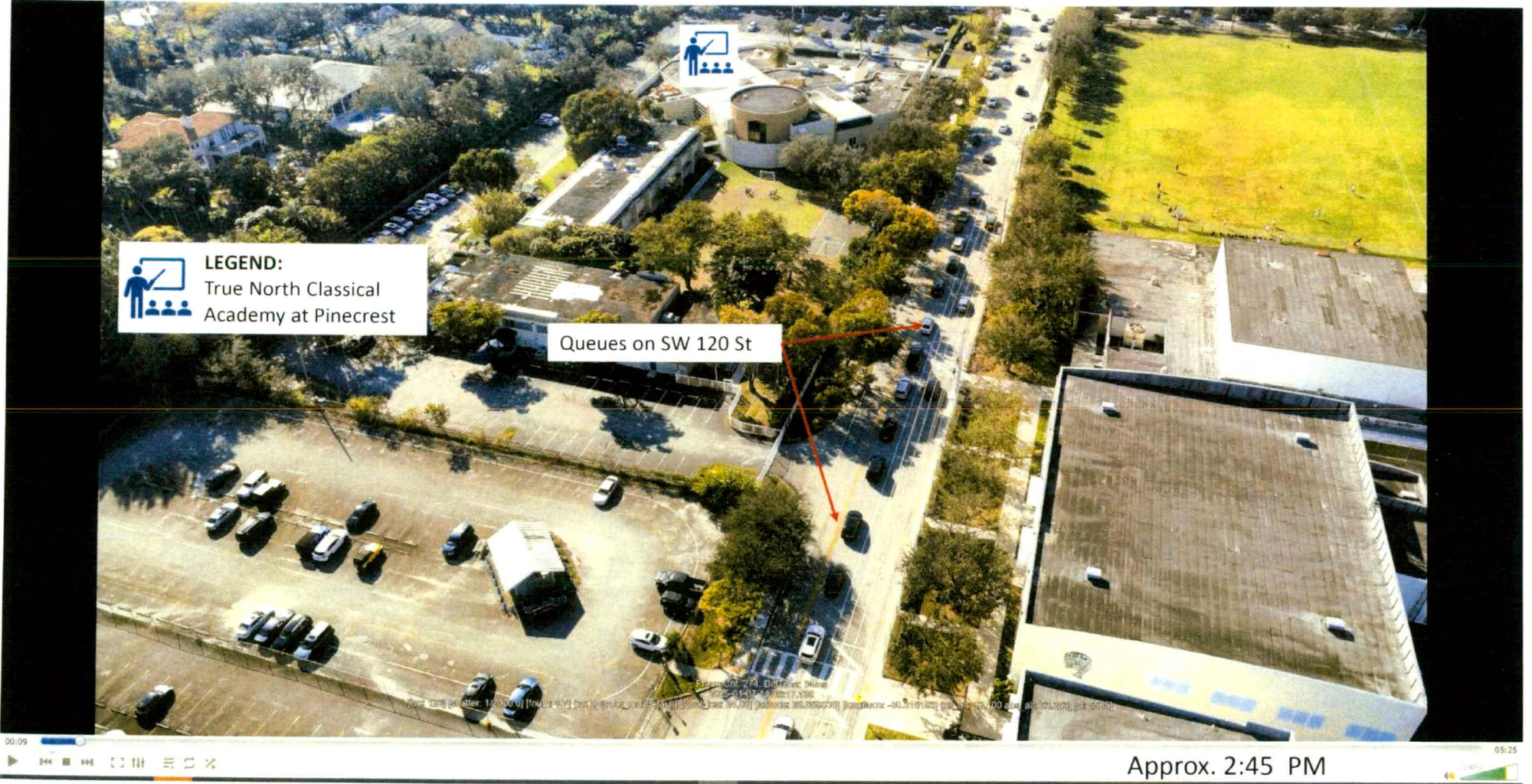
**LEGEND:**  
True North Classical  
Academy at Pinecrest

Vehicles parked on  
shoulder waiting for  
students

### Drone Field Observations (Jan. 7, 2025) for True North Classical Academy Study

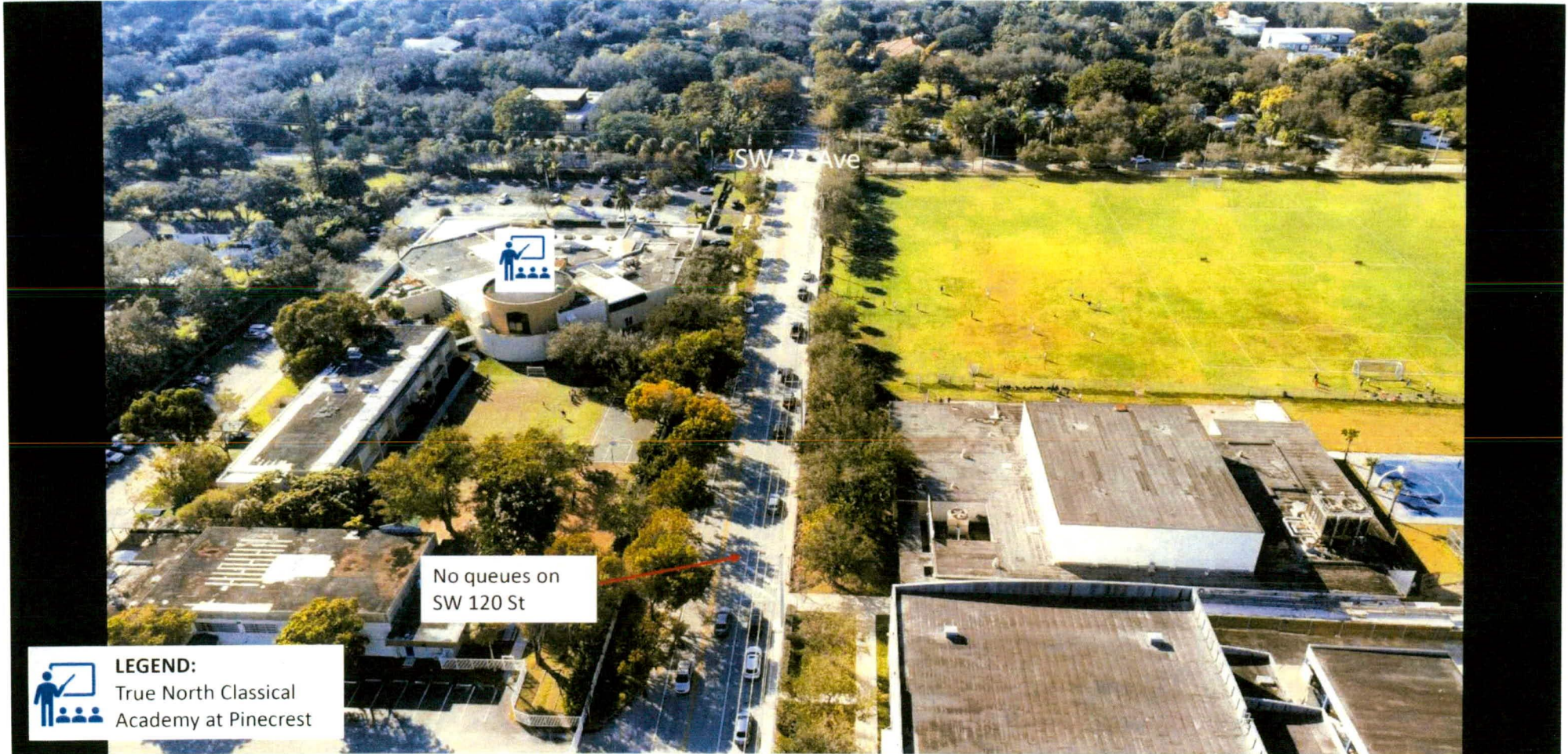


Drone Field Observations (Jan. 7, 2025) for True North Classical Academy Study




Approx. 2:45 PM

### Drone Field Observations (Jan. 7, 2025) for True North Classical Academy Study



No queues on  
SW 120 St

 **LEGEND:**  
True North Classical  
Academy at Pinecrest



Vehicles turning left to enter school



**LEGEND:**  
True North Classical Academy at Pinecrest

Drone Field Observations (Jan. 7, 2025) for True North Classical Academy Study



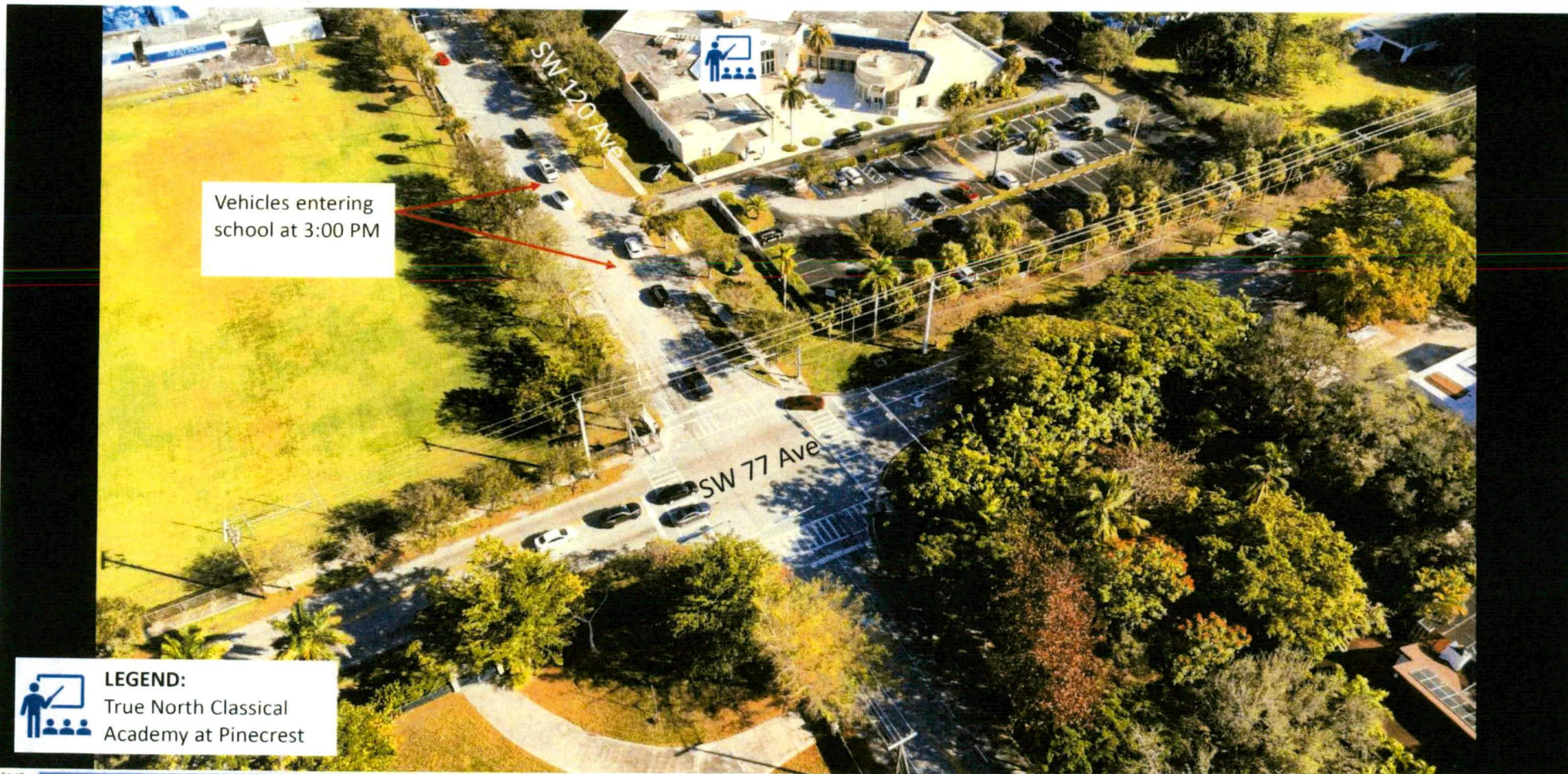
Vehicles entering school

**LEGEND:**  
True North Classical  
Academy at Pinecrest

Approx. 3:05 PM

02:04 05:25 100%

Drone Field Observations (Jan. 7, 2025) for True North Classical Academy Study



Vehicles entering school at 3:00 PM

**LEGEND:**  
True North Classical Academy at Pinecrest

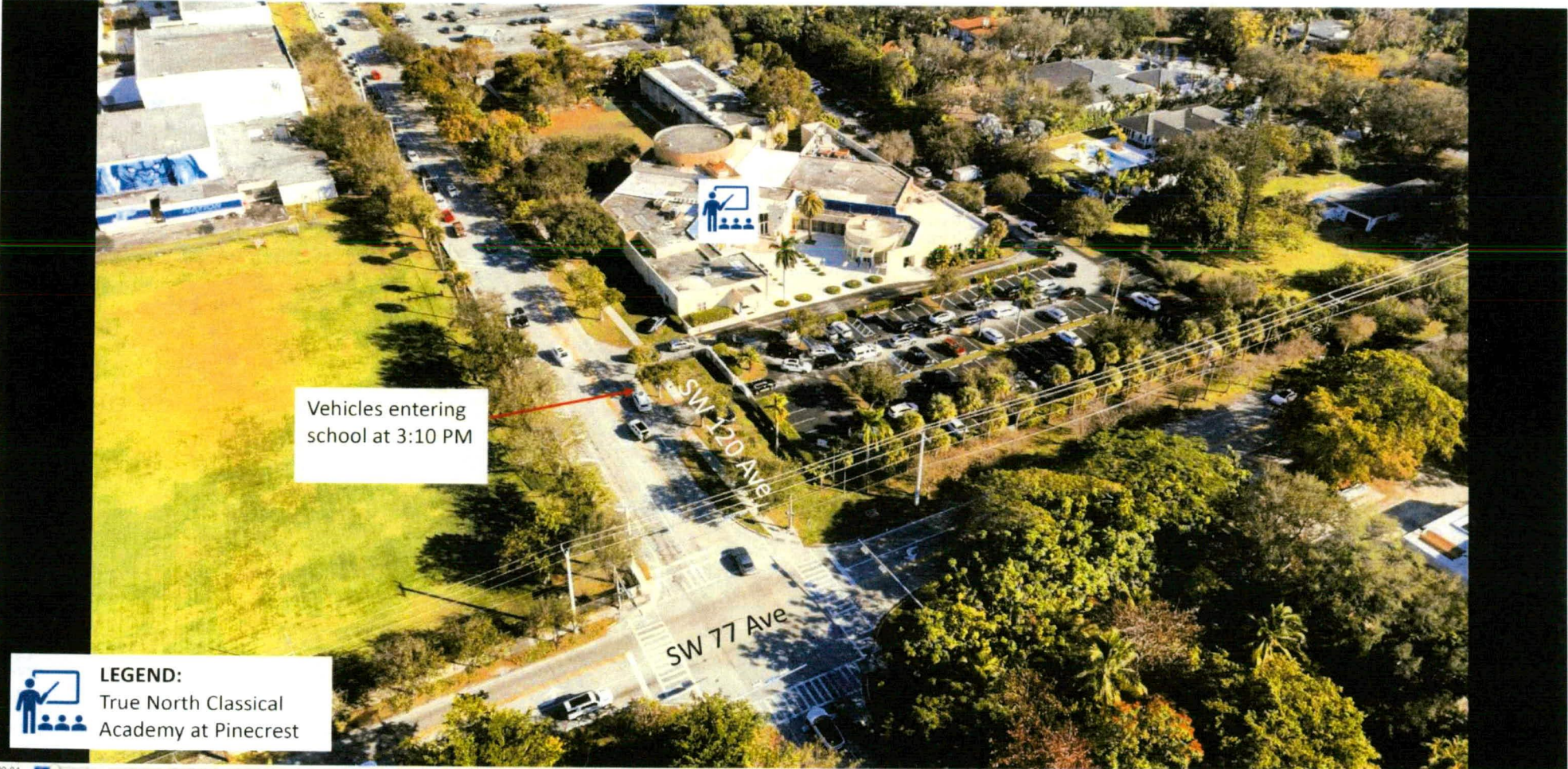
Approx. 3:05 PM

Drone Field Observations (Jan. 7, 2025) for True North Classical Academy Study



**LEGEND:**  
True North Classical  
Academy at Pinecrest

# Drone Field Observations (Jan. 7, 2025) for True North Classical Academy Study



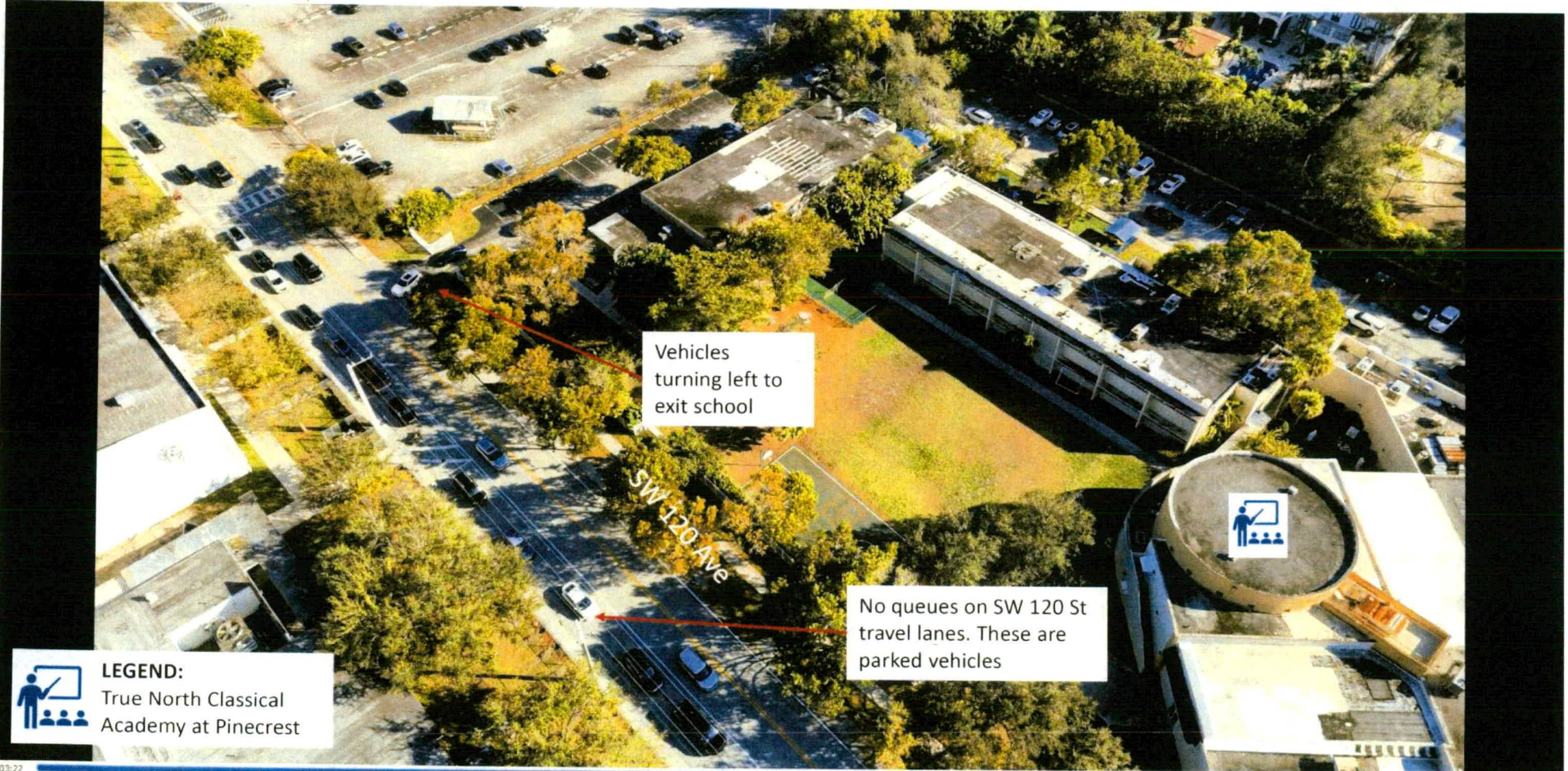
Vehicles entering school at 3:10 PM



**LEGEND:**  
True North Classical Academy at Pinecrest

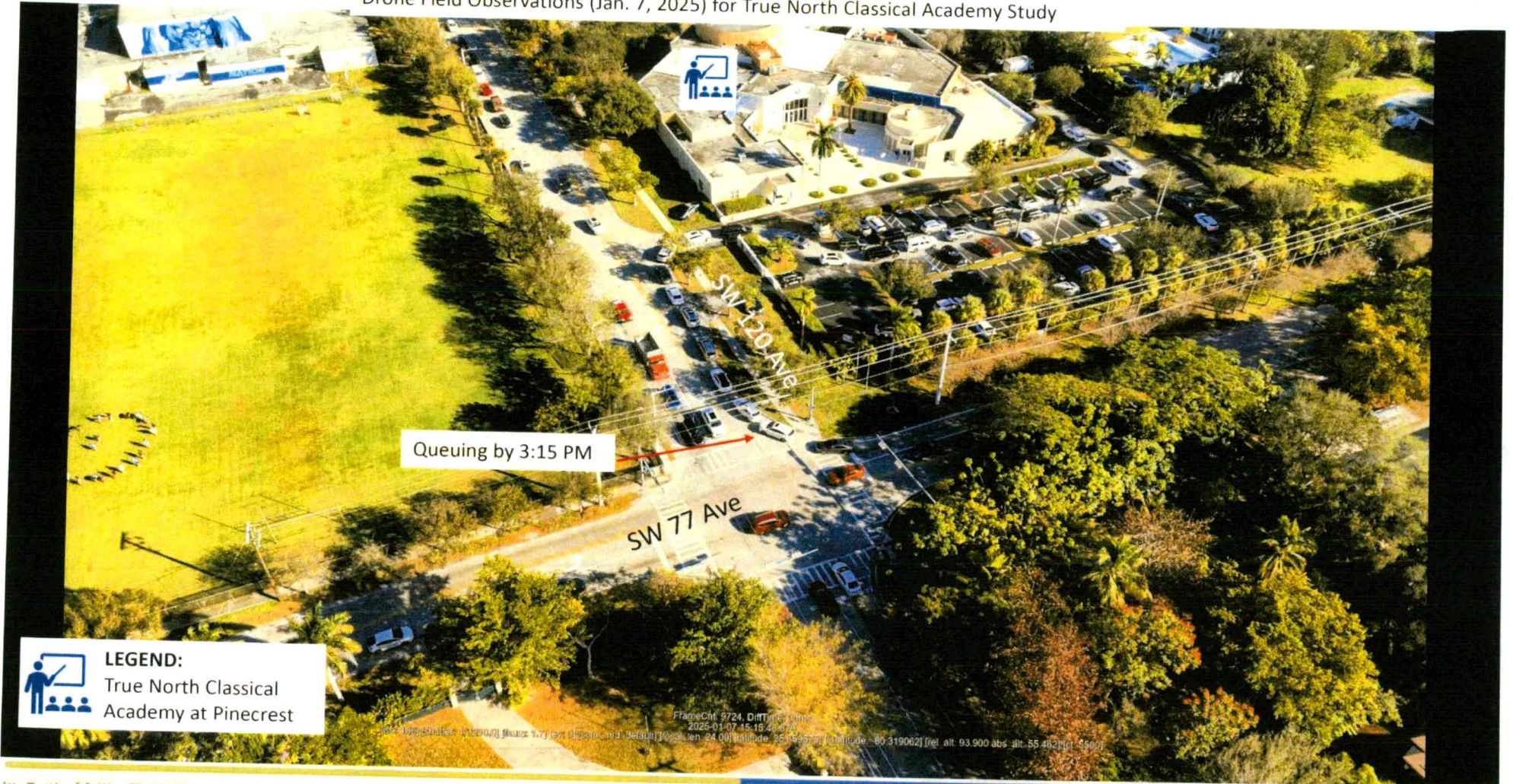


### Drone Field Observations (Jan. 7, 2025) for True North Classical Academy Study




Approx. 3:25 PM

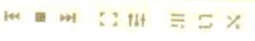
Drone Field Observations (Jan. 7, 2025) for True North Classical Academy Study



Queuing by 3:15 PM

 **LEGEND:**  
True North Classical  
Academy at Pinecrest

FrameCnt: 3724, DirTime: 1.000  
2025-01-07 15:15:46.819  
[Map: WGS84/EPS:31466/Scale: 1.7] [ex: 0.0, 0.0, 0.0] [0.0, 0.0, 24.00] [altitude: 25.5357, longitude: -80.319062] [ref. alt: 93.900 abs. alt: 55.462] [cr: 560]



Approx. 3:15 PM



### Drone Field Observations (Jan. 7, 2025) for True North Classical Academy Study

